

**CITY OF LEWISTON
PLANNING BOARD MEETING**
Monday, December 22, 2014 – 5:30 P.M.
City Council Chambers – First Floor
Lewiston City Building
27 Pine Street, Lewiston

AGENDA

I. ROLL CALL

II. ADJUSTMENTS TO THE AGENDA

III. CORRESPONDENCE

IV. PUBLIC HEARINGS:

An application submitted by Stoneybrook Consultants, Inc. on behalf of Grimmel's Car Crushing, LLC to add a 4,260 SF addition on the south end of the existing building located at 36 River Road.

V. OTHER BUSINESS:

a) Any other business Planning Board Members may have relating to the duties of the Lewiston Planning Board.

VI. READING OF THE MINUTES: Motion to adopt the December 8, 2014 draft minutes

VII. ADJOURNMENT



CITY OF LEWISTON



Department of Planning & Code Enforcement

TO: Planning Board
FROM: David Hediger, City Planner
DATE: December 19, 2014
RE: December 22, 2014 Planning Board Agenda Item IV(a)

An application submitted by Stoneybrook Consultants, Inc. on behalf of Grimmel's Car Crushing, LLC to add a 4,260 SF addition on the south end of the existing building located at 36 River Road.

Stoneybrook Consultants, Inc. on behalf of Grimmel's Car Crushing, LLC has submitted an application to add a 4,260 SF addition on the south end of the existing building located at 36 River Road. This property of approximately 4.5 acres is located in the Industrial (I) district in which junkyard, auto graveyard, recycling and reprocessing facilities are allowed as a conditional use. The property is currently used as such with the applicant proposing a building addition for purpose of processing material inside. Projects less than 5,000 SF are typically reviewed by the Staff Review Committee. However, because the project requires conditional use approval, the development review process is bumped to the Planning Board. Therefore, this project is subject to the conditional use criteria of Article X, Section 3 and Article XIII, Section 4 of the Zoning and Land Use Code.

Staff has been working closely with the applicant's representative to address concerns and questions. The applicant has since provided revised plans and documentation addressing staff comments. Staff notes the following with respect to the proposed development:

- The applicant has referenced all of the applicable sections of the Zoning and Land Use Code including Article X, Section 3 (conditional use) and Article XIII, Section 4 (development review).
- The property consists of approximately 4.5 acres. The site plans, surveying and engineering provided are limited to a 17,700 SF portion of the lot where said improvements are proposed. Most of the area where development is proposed is impervious. Once completed, there will be approximately 2,263 SF of additional vegetated area added to the site.
- The applicant has provided a stormwater design to the satisfaction of Public Works. However, the outlet pipe crosses a small triangle of city property before discharging into a drainage swale within the River Road right-of-way. This small triangle of land was identified by the applicant's surveyor as being deeded to the city from when River Road was relocated in 1982. Interestingly, the city tax maps show this triangle owned by Grimmel's. Unless the applicant reroutes the drain pipe as shown, the applicant will either need to request an easement or acquire this land from the city. Therefore, staff is recommending as a condition of approval that no building permits are issued and no site construction occur until the applicant either secures an easement or ownership the land

owned by the city identified in book 1567, page 178 as recorded at the Androscoggin Registry of deeds.

- The property is located adjacent to Hart Brook and within that watershed which has been designated as impaired by DEP. However, the proposed improvements fall outside of the shoreland area and site disturbance is proposed to be less than one acre. Therefore, any additional provisions are not applicable. The applicant has noted and provided details with respect to the appropriate installation and monitoring of erosion control measures.
- The applicant has requested a modification of front and side setbacks pursuant to Article IX, Section 3(9) such that building shall be 37.5' versus 50' from the front property line and 18.75 'versus 20' from the side property line. Staff has no concerns with the requested modifications.
- State law and city ordinance require junkyard facilities screen their material from view. The proposed addition will be constructed within an area presently used for exterior storage of scrap material with limited screening. The new building will be over 30' tall with outside storage of scrap material remaining behind the structure, thereby improving the screening of the site. Furthermore, the applicant has added a note to sheet 1 (#9) that there shall be no outside storage of material or equipment within the front setback area of the new building.
- Sheet 1, note 10 has been added per Lewiston Fire that no outside storage shall occur within 10' behind the new structure.
- The applicant is requesting the conditional use approval be valid for two years to begin and to diligently pursue improvements to completion. This would be consistent with the development review expiration of approval requirements of Article XIII, Section 11 which provides an applicant two years to start a project and five years to complete.

No other concerns have been raised by City staff. Approval is recommended pursuant to Article XIII, Section 4 Zoning and Land Use Code with the following conditions:

- That no building permits are issued and no site construction occur until the applicant either secures an easement from or ownership the land owned by the city identified in book 1567, page 178 as recorded at the Androscoggin Registry of deeds for purposes of the drainage improvements as approved by the Planning Board.
- The conditional use approval is valid for two years to begin and to diligently pursue improvements to completion as per Article X, Section 5 of the Zoning and Land Use Code.

ACTIONS NECESSARY

1. Make a motion to consider an application submitted by Stoneybrook Consultants, Inc. on behalf of Grimmel's Car Crushing, LLC to add a 4,260 SF addition on the south end of the existing building located at 36 River Road.
2. Obtain input on the application;
3. Make a determination that the application is complete;
4. Make finding that the application meets all of the necessary criteria contained in the Zoning and Land Use Code, including Article X, Section 3 and Article XIII, Section 4 of the Zoning and Land Use Code and to grant approval to Grimmel's Car Crushing, LLC to

add a 4,260 SF addition on the south end of the existing building located at 36 River Road (subject to any concerns raised by the Planning Board or staff).

From: [Mike Gotto](#)
To: [David Hediger](#)
Subject: RE: Comments on Grimmel's revisions
Date: Thursday, December 18, 2014 2:55:12 PM

David,
See my response to your comments below. Response to Ryan's comments are as follows:

1. The letter refers to an F Basin and the plans indicate a 4'Ø basin. Which will be used on this site? Given the large quantity of gravel surface adjacent to the catchbasin a 4'Ø basin may be more appropriate due to the additional silt storage capacity.
We show both on the plan. We will add a detail for the "F" structure to the detail sheet.
2. A snout shall be installed over the outlet pipe to minimize the floatables and silt from exiting the basin.
We show a 90 degree elbow on the outlet pipe to minimize floatables or silt exiting the basin.
3. An easement will be required to install and maintain the stormdrain outlet across the City's property.
I would suggest a condition of approval that the easement be secured prior to constructing the outlet or that we provide City staff a detail showing an acceptable outlet alternative.

Let me know if you have questions.

Mike Gotto
Stoneybrook Consultants, Inc.
456 Buckfield Road
Turner, Maine 04282

(207) 514-7491 Voice
(207) 514-7492 Fax
(207) 513-6123 Cell

From: David Hediger [mailto:DHediger@lewistonmaine.gov]
Sent: Thursday, December 18, 2014 10:05 AM
To: Mike Gotto
Subject: FW: Comments on Grimmel's revisions

Please review. Thanks.

David Hediger
City Planner/Deputy Director Planning and Code Enforcement
City of Lewiston

27 Pine Street
Lewiston, ME 04240-7201
Phone 207.513.3125 ext. 3223
TDD/TTY 207.513.3007
www.lewistonmaine.gov

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From: Ryan Barnes
Sent: Thursday, December 18, 2014 9:56 AM
To: David Hediger
Subject: RE: Comments on Grimmel's revisions

From: David Hediger
Sent: Wednesday, December 17, 2014 5:13 PM
To: Gotto, Mike
Cc: Paul Ouellette; Ryan Barnes
Subject: Comments on Grimmel's revisions

Mike:

I have reviewed the revised site plan and responses to staff's comments and offer the following:

1. Sheet 1: note should be added referencing the modifications of the setbacks being requested. We frequently receive calls from surveyors and banks questioning the location of buildings contrary to the space and bulk requirements noted in the ordinance. Adding a note helps everyone know that a modification was request and granted.

We will show the setbacks requested and note that the setback is reduced from the setback distance required on our revised plans.

2. Consideration should be given to vegetating the existing gravel area parallel to the south side of the structure. Given fire's requirement of maintaining at least 10' of separation, use of this side of the structure is limited. Vegetation would assisting with reducing impervious area and improve drainage.

We plan to use the area to access the back of the scrap pile or the rear of the building for maintenance.

3. Sheet 1: it would be helpful if the 10' area of separation referenced in note 10 were delineated on the plan.

We will show this line on our revised plan.

4. Sheet 2: you original submittal consisted of a sheet 2 with construction notes and details. The revised sheet 2 is a topographic site plan. This needs to corrected.

We submitted 3 revised sheets.

5. Construction notes and details sheet: detail should be added for referenced "F" catch basin structure.

We will add a detail for the "F" structure on the detail sheet.

6. Topographic site plan: outlet of catch basin crosses city property. Technically, an easement should be obtained from the city for any private utility crossing. That said, this triangle portion is city land may not be of interest to the city and could possibly be sold to the

applicant. Might this be directed elsewhere? I realize elevations are an issue.

I would suggest a condition of approval that the easement be secured prior to constructing this outlet or that we provide City Staff a detail showing an acceptable outlet alternative.

7. Reference to Article X, Section 3 (b): what are the hours of operation and what is typical of traffic at this facility?

I will provide more details at the Planning Board meeting.

Ryan and Paul:

Please provide Mike any additional comment ASAP, ideally tomorrow/Thursday, as both Mike and I need to understand the issues going into Monday's meeting. Ideally, I would reference them in my memo due Friday morning.

Thanks

David Hediger
City Planner/Deputy Director Planning and Code Enforcement
City of Lewiston
27 Pine Street
Lewiston, ME 04240-7201
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TDD/TTY 207.513.3007
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ExchangeDefender Message Security: [Check Authenticity](#)

From: [Paul Ouellette](#)
To: [David Hediger](#)
Cc: [David Chick](#); [Ryan Barnes](#); [Gildace Arsenault](#); [Bruce McKay](#)
Subject: 36 River Rd. Building Addition.
Date: Tuesday, November 18, 2014 10:45:57 AM

I have no major concerns regarding the building addition project.

Under NFPA 101 (2009) Life Safety Codes – Chapter 42 – Storage Requires no fire alarm or extinguishing systems.

However, my biggest concern that there will now be a large pile of metal and debris in close proximity to the new building. I have issues and concerns with LFD not having clear access to the new building addition in the event of a fire. I would need to see a yard floor plan in place that would provide an adequate 20' (foot) break between the outside pile of metal storage and the new building. This would leave us with access to both the outside pile and the building in the event of an emergency.

Insp. Ouellette

Paul Ouellette
NFPA - Certified Fire Inspector /
IAAI - Certified Fire Investigator
Lewiston Fire Department
Fire Prevention Bureau
2 College St.
Lewiston, ME 04240
Phone # (207) 513-3002 ext.3605
Fax # (207) 783-6138
TTY/TDD: (207) 513-3007
pouellette@lewistonme.gov

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Stoneybrook Consultants, Inc.

456 Buckfield Road
Turner, Maine 04282
(207) 514-7491 voice
(207) 514-7492 fax

November 12, 2014

David Hediger, City Planner
Department of Planning & Code Enforcement
City of Lewiston
27 Pine Street
Lewiston, ME 04240-7201

Re: *Grimmel's Car Crushing, LLC*
Building Addition - 36 River Road

Dear David:

On behalf of *Grimmel's Car Crushing LLC*, (*Grimmel*), I have prepared the attached information for modification to the approved site improvements located at 36 River Road. The property is shown on Tax Map 179 as Lot 34. There is about 660' of frontage on River Road and about 200' of frontage along Goddard Road. The property also abuts the Maine Turnpike and Hart Brook runs through the site. The parcel contains about 4.5 acres and is located in the Industrial District. Portions of the property along Hart Brook are also subject to requirements of the Shoreland Zoning Overlay District. While all of the improvements proposed lie within the Shoreland Zoning Overlay District, all of the improvements exceed the required 25' setback from the stream.

We have attached a Tax Map, USGS Map, Aquifer Map, Zoning Map, 2013 Aerial Photo and Flood Plain. We have also attached the application form, a copy of the current deed for the property, a copy of the site survey and design plans for the proposed improvements. Survey work was completed by Jones Associates, Inc. (*Jones*) and the design plans have been prepared by Summit Geoengineering Services, Inc. (*Summit*).

The applicant plans to add a 4,620 square foot addition on the south end of the existing building on site. These improvements will fit within a 17,700 square foot portion of this property which has been identified as "Limits of Project Area" on the site design plans. As we previously discussed, we have limited our field survey work and design work to this project area. Most of the application questions were answered as they relate to the project area only. There is a small area of woods containing about 3,160 square feet within the project area. Otherwise, the project area is currently impervious area. As you can see from the 2013 Aerial Photo, all of the land along Hart Brook and lying north of the brook is currently undeveloped. Therefore, this project will not exceed City requirements for maximum lot coverage (0.75) or maximum impervious coverage (0.75). Please also note that the site tabs show this project will increase the green space on the property.

The survey work by Jones was completed to define the River Road right of way limits and the boundary of the Maine Turnpike Authority. You will note that, with this survey information, we are requesting a 25% reduction in the front and side setback requirements for construction of this building. The front setback required is 50' and we hope to reduce that to 37.5', while the side setback is 25' and we hope to be at 18.75'.

The existing building floor is at elevation 172.71. The proposed building floor will be lower at elevation 169.4. The building foundation wall will be set 5' above the floor grade to help with grading changes at the site. Access between buildings will be via the existing building floor elevation with a door leading out of the building onto a concrete pad. From this outside slab, forklifts or pedestrian movements can proceed through an overhead door into the new building and down an interior ramp, or they can proceed down an exterior ramp to the new floor level and enter the new building through doors proposed in the front of the new structure.

Forklift access between buildings is important because the new building will be used to sort metal materials, bail them for shipping and for additional storage of the various metal products. This expansion is not expected to increase business. It will, instead, provide covered space to make the sorting and bailing process easier during winter and rainy weather. The new building will not be heated and the only utility service proposed is the extension of the existing electrical services

to the new structure for lighting. This will include two exterior lights at the proposed building access doors.

Again, operations at the site will not change and there will not need to be any new employees or parking spaces provided from the current operations. The large overhead door in the front of the structure will allow delivery trucks to enter the building with large containers full of metal products and dump the load on the floor. From there, the products will be separated, sorted and bailed for storage and future sale.

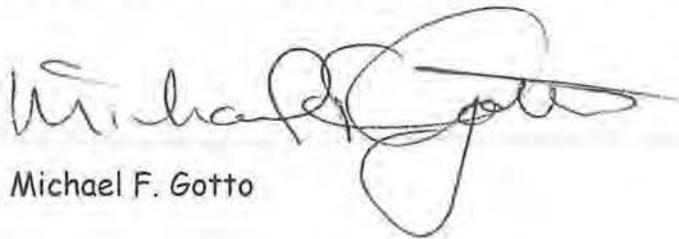
Total impervious area within the project area is currently at 14,540 square feet with an impervious ratio of 0.82. The proposed impervious area will be reduced to 12,290 square feet with an impervious ratio of 0.69. As noted, this ratio is within the project limits only and the site will still be well below the allowed impervious ratio of 0.75. The total building area on site will increase to 10,240 square feet with a total lot coverage of 0.05 for the entire property.

Since this project will decrease total impervious area on the property, no stormwater improvements are proposed or required. All site construction activities will be done in accordance with the Soil Erosion Control requirements outlined on the project plans.

Construction of the proposed improvements will begin in the Spring of 2015. The building will be completed by late Fall. The project is expected to cost \$250,000 which will be funded from cash on hand. I trust you will find this application complete for processing. Please let me know if you have any questions.

Respectfully Yours

STONEBROOK CONSULTANTS, INC.

A handwritten signature in black ink, appearing to read "Michael F. Gotto", written over a horizontal line.

Michael F. Gotto

cc: Gary Grimmel

PROJECT DATA

The following information is required where applicable, in order to complete the application

IMPERVIOUS SURFACE AREA/RATIO

Existing Total Impervious Area
 Proposed Total Paved Area
 Proposed Total Impervious Area
 Proposed Impervious Net Change
 Impervious surface ratio existing
 Impervious surface ratio proposed

* Within project area only	
	14,540 sq. ft.
	5,120 sq. ft.
	12,290 sq. ft.
	-2,250 sq. ft.
	82 % of lot area
	69 % of lot area

BUILDING AREA/LOT COVERAGE

Existing Building Footprint
 Proposed Building Footprint
 Proposed Building Footprint Net change
 Existing Total Building Floor Area
 Proposed Total Building Floor Area
 Proposed Building Floor Area Net Change
 New Building
 Building Area/Lot coverage existing
 Building Area/Lot coverage proposed

	5,620 sq. ft.
	10,240 sq. ft.
	4,620 sq. ft.
	5,620 sq. ft.
	10,240 sq. ft.
	4,620 sq. ft.
	Yes (yes or no)
	3 % of lot area
	5 % of lot area

ZONING

Existing
 Proposed, if applicable

	Industrial
	N/A

LAND USE

Existing
 Proposed

	Industrial
	Industrial

RESIDENTIAL, IF APPLICABLE

Existing Number of Residential Units
 Proposed Number of Residential Units
 Subdivision, Proposed Number of Lots

	N/A
	N/A
	N/A

PARKING SPACES

Existing Number of Parking Spaces
 Proposed Number of Parking Spaces
 Required Number of Parking Spaces
 Number of Handicapped Parking Spaces

ESTIMATED COST OF PROJECT

DELEGATED REVIEW AUTHORITY CHECKLIST

SITE LOCATION OF DEVELOPMENT AND STORMWATER MANAGEMENT

Existing Impervious Area 14,540 sq. ft.
 Proposed Disturbed Area 17,700 sq. ft.
 Proposed Impervious Area 12,290 sq. ft.

1. *If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with MDEP.*
2. *If the proposed impervious area is greater than one acre including any impervious area created since 11/16/05, then the applicant shall apply for a MDEP Stormwater Management Permit, Chapter 500, with the City.*
3. *If total impervious area (including structures, pavement, etc) is greater than 3 acres since 1971 but less than 7 acres, then the applicant shall apply for a Site Location of Development Permit with the City. If more than 7 acres then the application shall be made to MDEP unless determined otherwise.*
4. *If the development is a subdivision of more than 20 acres but less than 100 acres then the applicant shall apply for a Site Location of Development Permit with the City. If more than 100 acres then the application shall be made to MDEP unless determined otherwise.*

TRAFFIC ESTIMATE

Total traffic estimated in the peak hour-existing _____ passenger car equivalents (PCE)
 (Since July 1, 1997)

Total traffic estimated in the peak hour-proposed (Since July 1, 1997) _____ passenger car equivalents (PCE)
 If the proposed increase in traffic exceeds 100 one-way trips in the peak hour then a traffic movement permit will be required.

Zoning Summary

1. Property is located in the **Industrial** zoning district.
2. Parcel Area: **4.5** acres / _____ square feet(sf).

Regulations	Required/Allowed	Provided
Min Lot Area	40,000 s.f.	4.5 acres
Street Frontage	100'	860'
Min Front Yard	25'	N/A
Min Rear Yard	10'	N/A
Min Side Yard	10'	N/A
Max. Building Height	100'	/
Use Designation	Industrial	Industrial
Parking Requirement	1 space/ per _____ square feet of floor area	
Total Parking:	/	
Overlay zoning districts (if any):	Shoreland / /	
Urban impaired stream watershed?	<input checked="" type="radio"/> YES <input type="radio"/> NO If yes, watershed name Hart Brook	

DEVELOPMENT REVIEW APPLICATION SUBMISSION

Submission shall include payment of fee and fifteen (15) complete packets containing the following materials:

1. Full size plans containing the information found in the attached sample plan checklist.
2. Application form that is completed and signed.
3. Cover letter stating the nature of the project.
4. All written submittals including evidence of right, title and interest.
5. Copy of the checklist completed for the proposal listing the material contained in the submitted application.

Refer to the application checklist for a detailed list of submittal requirements.

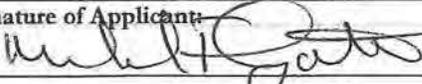
L/A's development review process and requirements have been made similar for convenience and to encourage development. Each City's ordinances are available online at their prospective websites:

Auburn: www.auburnmaine.org under City Departments/ Planning and Permitting/Land Use Division/Zoning Ordinance

Lewiston: <http://www.ci.lewiston.me.us/clerk/ordinances.htm>. Refer to Appendix A of the Code of Ordinances

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, I certify that the City's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for development review only; a Performance Guarantee, Inspection Fee, Building Permit Application and other associated fees and permits will be required prior to construction.

Signature of Applicant: 	Date: 11/12/14
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AG GATO

Development Review Checklist
 City of Auburn Planning and Permitting Department
 City of Lewiston Department of Planning and Code
 Enforcement



THE FOLLOWING INFORMATION IS REQUIRED WHERE APPLICABLE TO BE
 SUBMITTED FOR AN APPLICATION TO BE COMPLETE

PROJECT NAME: 36 River Road

PROPOSED DEVELOPMENT ADDRESS and PARCEL #: 36 River Road, 179-34

Required Information		Check Submitted		Applicable Ordinance	
		Applicant	Staff	Lewiston	Auburn
Site Plan					
	Owner's Names/Address	✓			
	Names of Development	✓			
	Professionally Prepared Plan	✓			
	Tax Map or Street/Parcel Number	✓			
	Zoning of Property	✓			
	Distance to Property Lines	✓			
	Boundaries of Abutting land	✓			
	Show Setbacks, Yards and Buffers	✓			
	Airport Area of Influence (Auburn only)	N/A			
	Parking Space Calcs	N/A			
	Drive Openings/Locations	✓			
	Subdivision Restrictions	N/A			
	Proposed Use	✓			
	PB/BOA/Other Restrictions	N/A			
	Fire Department Review				
	Open Space/Lot Coverage	✓			
	Lot Layout (Lewiston only)				
	Existing Building (s)	✓			
	Existing Streets, etc.	✓			
	Existing Driveways, etc.	✓			
	Proposed Building(s)	✓			
	Proposed Driveways	✓			
Landscape Plan					
	Greenspace Requirements				
	Setbacks to Parking	N/A			
	Buffer Requirements				
	Street Tree Requirements				
	Screened Dumpsters	N/A			
	Additional Design Guidelines				

	Planting Schedule				
Stormwater & Erosion Control Plan					
	Compliance w/ chapter 500	✓			
	Show Existing Surface Drainage	✓			
	Direction of Flow				
	Location of Catch Basins, etc.	N/A			
	Drainage Calculations	N/A			
	Erosion Control Measures	✓			
	Maine Construction General Permit Bonding and Inspection Fees	N/A			
	Post-Construction Stormwater Plan				
	Inspection/monitoring requirements				
	Third Party Inspections (Lewiston only)				
Lighting Plan					
	Full cut-off fixtures	✓			
	Meets Parking Lot Requirements				
Traffic Information		N/A			
	Access Management				
	Signage				
	PCE - Trips in Peak Hour				
	Vehicular Movements				
	Safety Concerns				
	Pedestrian Circulation				
	Police Traffic				
	Engineering Traffic				
Utility Plan					
	Water	N/A			
	Adequacy of Water Supply	N/A			
	Water main extension agreement	N/A			
	Sewer	N/A			
	Available city capacity				
	Electric	✓			
	Natural Gas	N/A			
	Cable/Phone	N/A			
Natural Resources					
	Shoreland Zone	✓			
	Flood Plain	✓			
	Wetlands or Streams	✓			
	Urban Impaired Stream	✓			
	Phosphorus Check	N/A			
	Aquifer/Groundwater Protection	N/A			
	Applicable State Permits	N/A			
	No Name Pond Watershed (Lewiston only)	N/A			

	Lake Auburn Watershed (Auburn only)				
	Taylor Pond Watershed (Auburn only)				
Right Title or Interest					
	Verify	✓			
	Document Existing Easements, Covenants, etc.	✓			
Technical & Financial Capacity					
	Cost Est./Financial Capacity	✓			
	Performance Guarantee				
State Subdivision Law		N/A			
	Verify/Check				
	Covenants/Deed Restrictions				
	Offers of Conveyance to City				
	Association Documents				
	Location of Proposed Streets & Sidewalks				
	Proposed Lot Lines, etc.				
	Data to Determine Lots, etc.				
	Subdivision Lots/Blocks				
	Specified Dedication of Land				
Additional Subdivision Standards		N/A			
	Single-Family Cluster (Lewiston only)				
	Multi-Unit Residential Development (Lewiston only)				
	Mobile Home Parks				
	Private Commercial or Industrial Subdivisions (Lewiston only)				
	PUD (Auburn only)				
A jpeg or pdf of the proposed site plan					
Final sets of the approved plans shall be submitted digitally to the City, on a CD or DVD, in AutoCAD format R 14 or greater, along with PDF images of the plans for archiving					

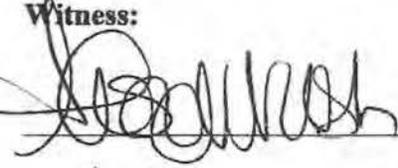
WARRANTY DEED

Gary Grimmel and **Betty Grimmel**, both with a mailing address at P.O. Box 246, Rensselaer, New York 12144, for consideration paid, grant to **Grimmel's Car Crushing, LLC**, a Maine Limited Liability Company with a mailing address at 80 Pejepscoot Village M, Topsham, Maine 04086, with **Warranty Covenants**, a certain lot or parcel of land, together with any buildings situated thereon, located in **Lewiston**, County of **Androscoggin**, and State of **Maine**, as more fully described in Exhibit "A" attached hereto and made a part hereof.

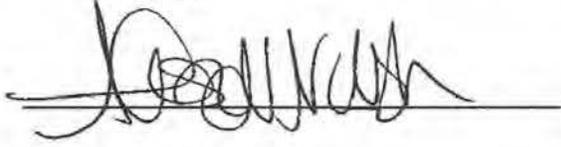
In Witness Whereof, the Grantors have hereunto set their hands and seals effective on this 23rd day of April, 2009.

NO MAINE R.E. TRANSFER TAX PAID

Witness:




_____ **Gary Grimmel**




_____ **Betty Grimmel**

STATE OF NEW YORK
COUNTY OF Rensselaer, SS.

On the 23rd day of April, in the year 2009, before me, the undersigned, a Notary Public in and for said State, personally appeared **Gary T. Grimmel** personally known to me or proved to me on the basis of satisfactory evidence to be the individual whose name is subscribed to the within instrument and that by his signature on the instrument, acknowledged to me that he executed the same.


Notary Public
Print Name: M. Alison Coe / Nash
My Commission Expires: 4-2-11

Title Not Searched, Description Not Verified

M. Alison Coe
Notary Public, State of New York
No. 01006056845
Residing in Albany County
Commission Expires 4-2-11

STATE OF NEW YORK
COUNTY OF Cattaraugus, SS.

On the 23rd day of April, in the year 2009, before me, the undersigned, a Notary Public in and for said State, personally appeared **Betty A. Grimmel** personally known to me or proved to me on the basis of satisfactory evidence to be the individual whose name is subscribed to the within instrument and that by her signature on the instrument, acknowledged to me that she executed the same.



Notary Public
Print Name: M. Allison Coe
My Commission Expires: 4-2-11

M. Allison Coe
Notary Public, State of New York
No. 01008056845
Residing in Albany County
Commission Expires 4-2-11

Title Not Searched, Description Not Verified

EXHIBIT A

A certain lot or parcel of land consisting of approximately 6.79 acres, more or less, situated in said Lewiston and bounded and described as follows: Beginning at a point on the southeasterly line of River Road, said point being the southwesterly corner of land now or formerly owned by Romeo and Theresa Robitaille and recorded in the Androscoggin County Registry of Deeds, Book 706, Page 46; thence in a southwesterly direction along the southeasterly line of the River Road one hundred twenty (120) feet more or less to an angle in said road; thence in a southwesterly direction along said southeasterly line of River Road 540'± to land now or formerly owned by Maine Turnpike Authority and recorded in Book 703, Page 253 in said Registry; thence in a southeasterly direction along said Turnpike 628.7 feet to a point; thence in a northeasterly direction along said Turnpike 255.3 feet to a point and land now or formerly of Roger and Yvette Roux and recorded in said Registry in Book 618, Page 379; thence in a westerly direction along said Roux land 180 feet more or less to a point; thence in a northerly direction along land now or formerly of said Roux, Omer Gagne, Albert and Simonne Gagne, Jean B. and Imelda Roux 250 feet more or less to a point; thence in an easterly direction along the northerly line of Jean B. Roux 200 feet more or less to a point on the westerly line of the Goddard Road; thence in a northerly direction along the westerly line of Goddard Road 200 feet more or less to land now or formerly of Romeo and Theresa Robitaille; thence in a northwesterly direction along land of said Robitaille 225 feet more or less to the point of beginning.

There is specifically excepted from the above-described premises those premises described in a warranty deed from Real O. Fournier to Marcel and Lucille Belanger dated May 20, 1986 and recorded in the Androscoggin County Registry of Deeds in Book 1929, Page 132 and those premises, including easement rights, taken by the Maine Department of Transportation pursuant to a Notice of Layout and Taking dated July 22, 1986 and recorded in said Registry of Deeds in Book 1964, Page 75.

Also including all rights which Grantor has to that portion of the Old River Road which abuts the above-described premises.

This conveyance is made subject to any rights, easements, privileges, and appurtenances of record.

Being a portion of the same premises conveyed to Real O. Fournier by warranty deed of Alpee Robitaille dated September 10, 1958 and recorded in the Androscoggin County Registry of Deeds in Book 791, Page 328 as corrected by warranty deed of Alpee Robitaille et al dated October 6, 1964 and recorded in said registry of Deeds in Book 926, Page 6.

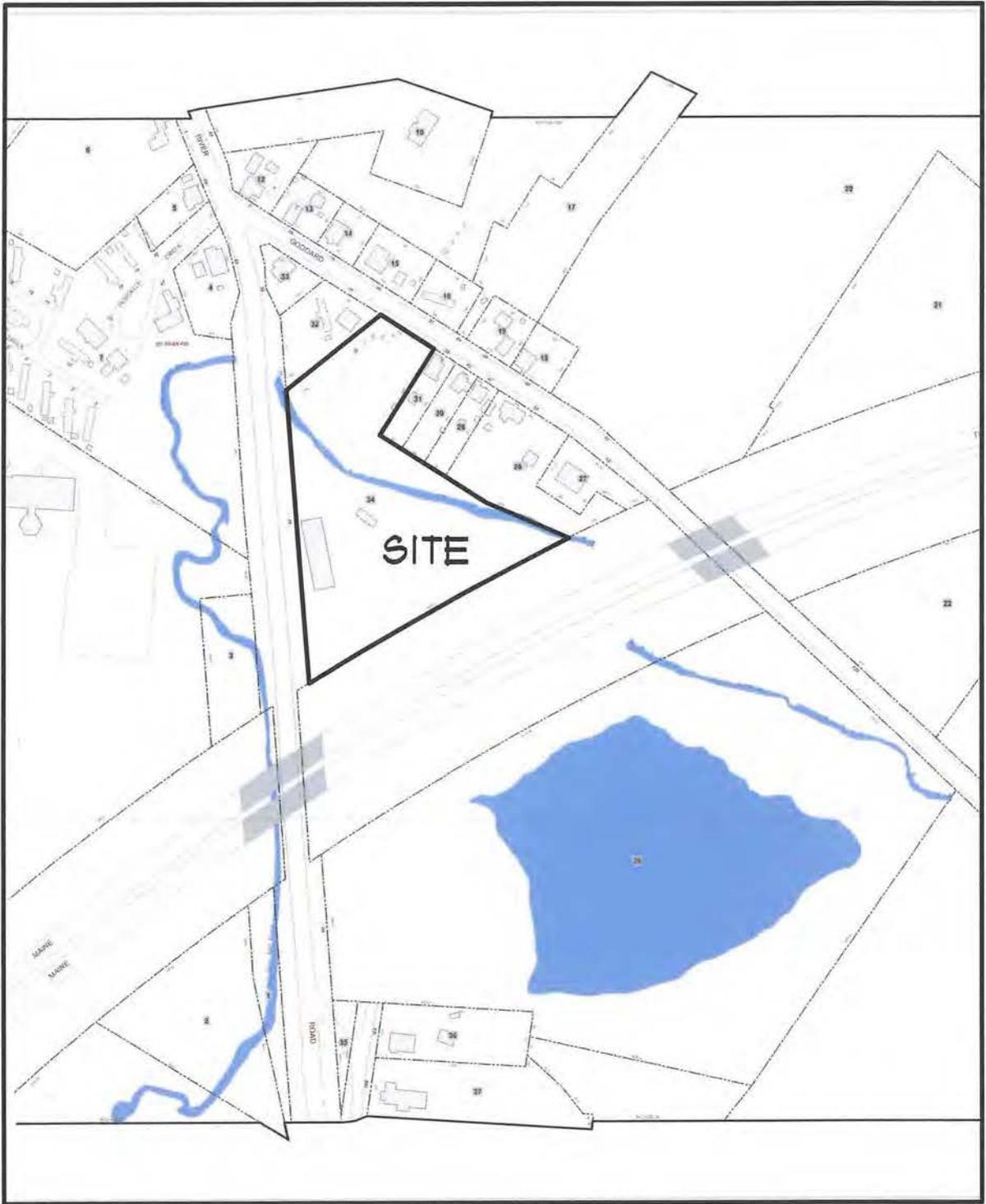
The above-described premises is subject to an easement to the City of Lewiston from Real O. Fournier as described in a document recorded in the Androscoggin County Registry of Deeds at Book 797, Page 179.

Title Not Searched, Description Not Verified

Also being the same premises described in a warranty deed from Real O. Fournier to Gary Grimmel and Betty Grimmel dated April 1, 1987 and recorded in the Androscoggin County Registry of Deeds in Book 2070, Page 110.

ANDROSCOGGIN COUNTY
Tha M. Chaunard
REGISTER OF DEEDS

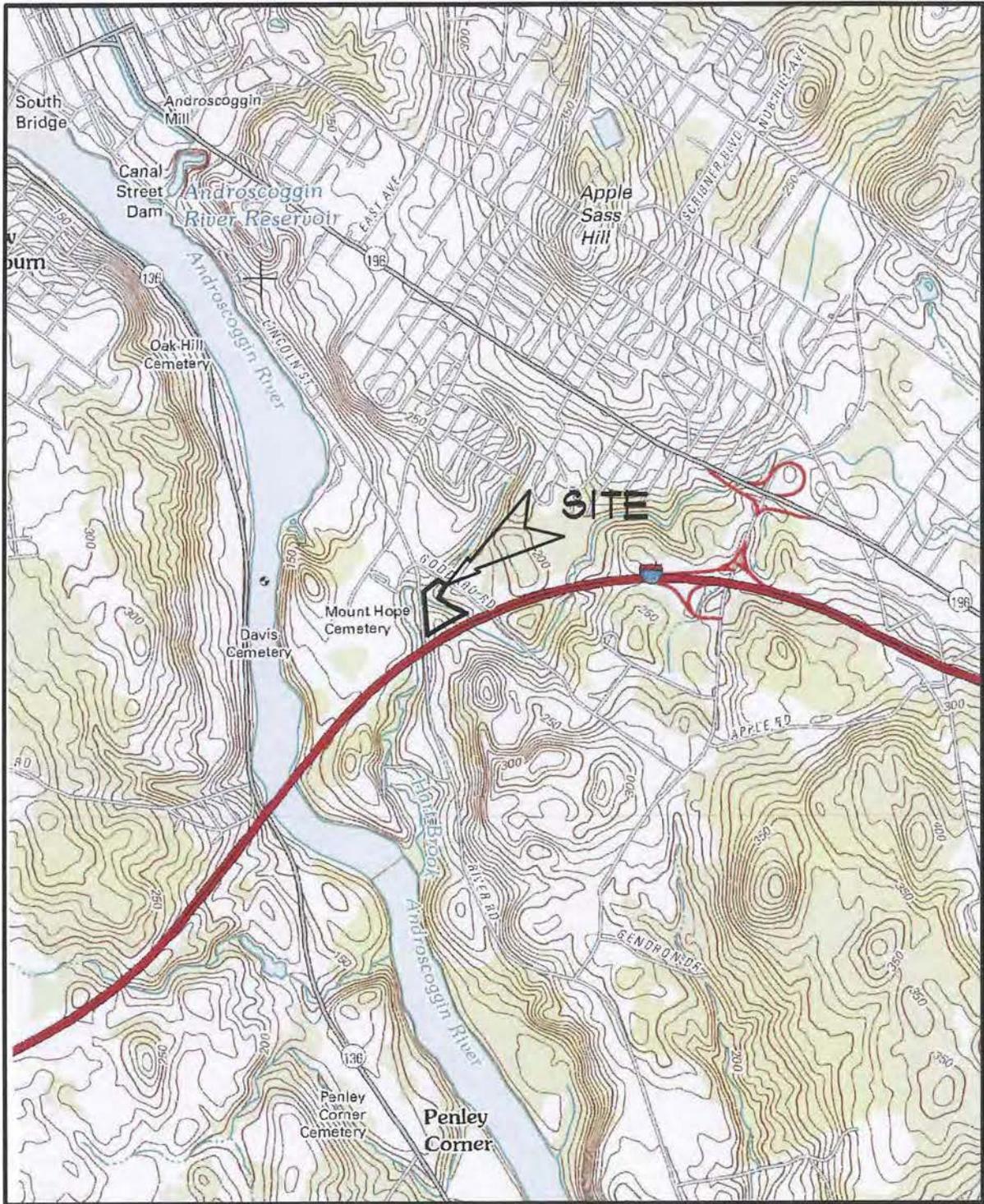
Title Not Searched, Description Not Verified



TAX MAP

36 RIVER ROAD, LEWISTON
OWNER: GRIMMEL'S CAR CRUSHING, LLC
SCALE: 1" = 300'±
DATE OF GRAPHIC: SEPTEMBER 4, 2014
SOURCE: CITY OF LEWISTON TAX MAP 179
PUBLICATION DATE: APRIL 1, 2014

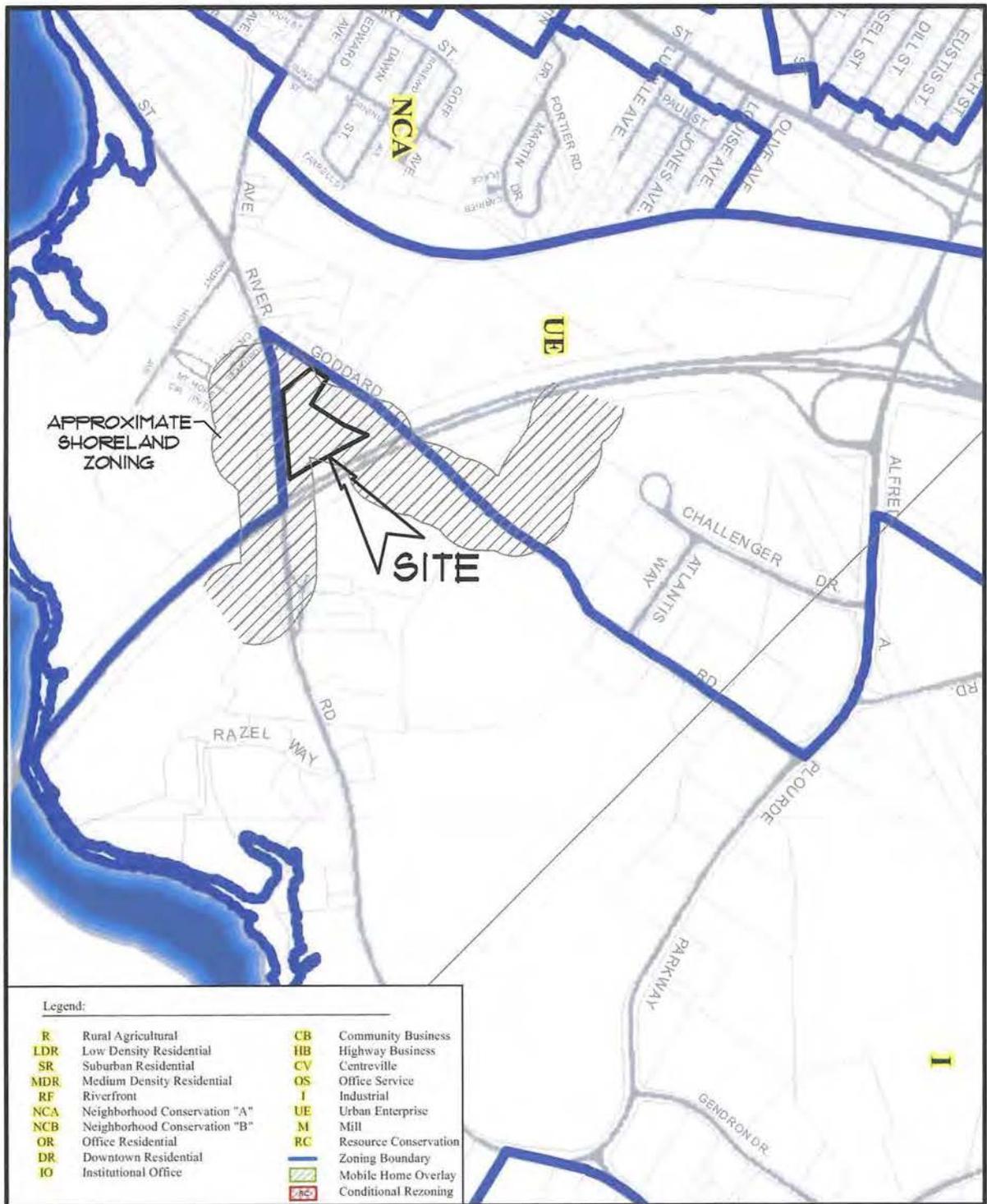
Stoneybrook
Consultants, Inc.



USGS LOCATION MAP

36 RIVER ROAD, LEWISTON
 OWNER: GRIMMEL'S CAR CRUSHING, LLC
 SCALE: 1" = 2,000'
 DATE OF GRAPHIC: SEPTEMBER 4, 2014
 SOURCE: MAINE OFFICE OF GIS
 ORIGINAL PUBLICATION DATE: 1967

Stoneybrook
 Consultants, Inc.



ZONING MAP

36 RIVER ROAD, LEWISTON
 OWNER: GRIMMEL'S CAR CRUSHING, LLC
 SCALE: 1" = 1,000'±
 DATE OF GRAPHIC: SEPTEMBER 4, 2014
 SOURCE: CITY OF LEWISTON ZONING MAP
 PUBLICATION DATE: 2014

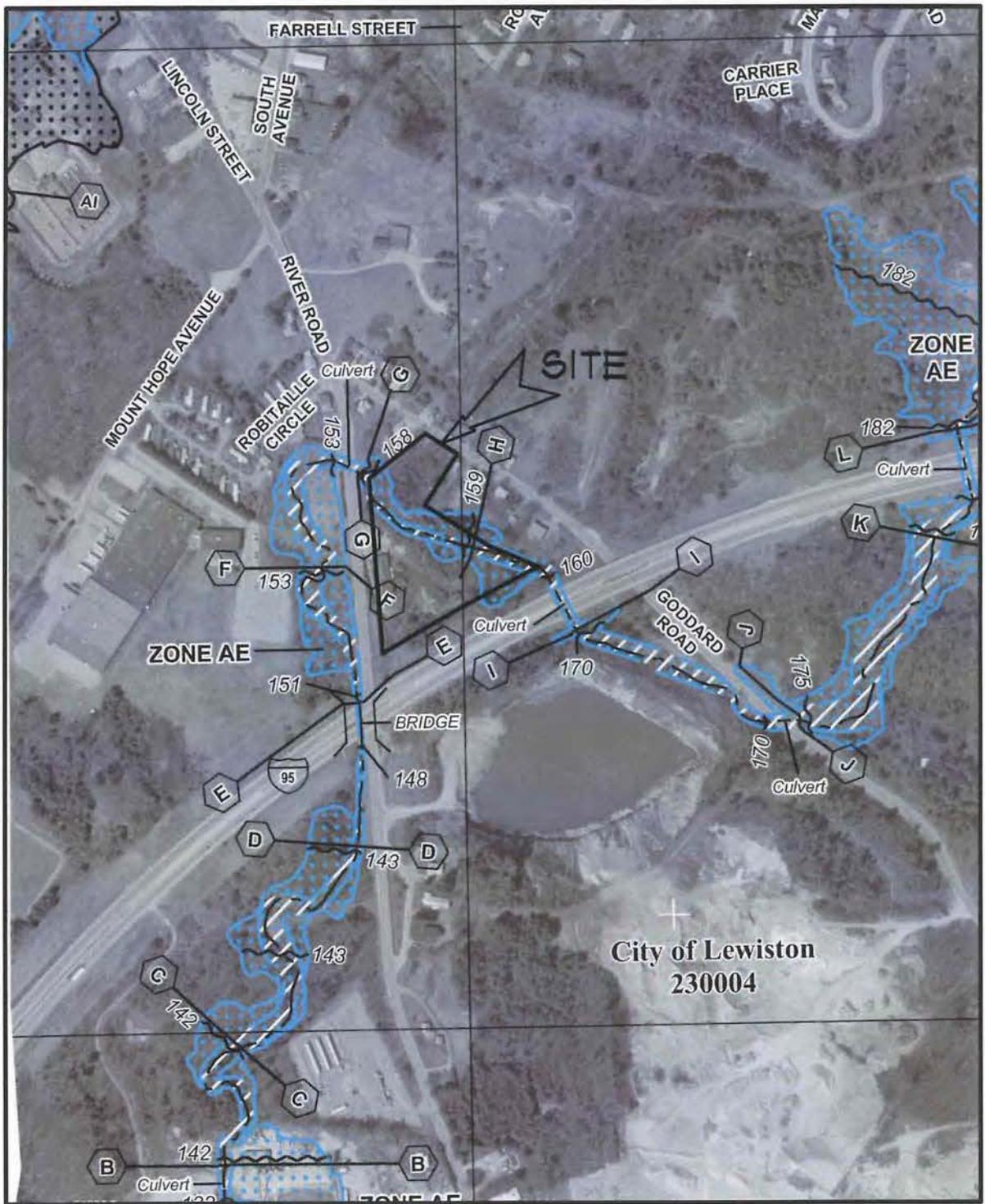
Stoneybrook
 Consultants, Inc.



2013 AERIAL PHOTO

36 RIVER ROAD, LEWISTON
OWNER: GRIMMEL'S CAR CRUSHING, LLC
SCALE: 1" = 100'
DATE OF GRAPHIC: SEPTEMBER 4, 2014
SOURCE: MAINE OFFICE OF GIS
PUBLICATION DATE: 2013

Stoneybrook
Consultants, Inc.



FLOOD MAP

36 RIVER ROAD, LEWISTON
 OWNER: GRIMMEL'S CAR CRUSHING, LLC
 SCALE: 1" = 500'±
 DATE OF GRAPHIC: SEPTEMBER 4, 2014
 SOURCE: FEMA FLOOD MAP
 PUBLICATION DATE: JULY 8, 2013

Stoneybrook
 Consultants, Inc.



Stoneybrook Consultants, Inc.

456 Buckfield Road
Turner, Maine 04282
(207) 514-7491 voice
(207) 514-7492 fax

December 16, 2014

David Hediger, City Planner
Department of Planning & Code Enforcement
City of Lewiston
27 Pine Street
Lewiston, ME 04240-7201

Re: *Grimmel's Car Crushing, LLC*
Building Addition - 36 River Road

Dear David:

On behalf of *Grimmel's Car Crushing, LLC* (*Grimmel*), please accept the following as our response to staff questions raised on the application we submitted. While preparing this response, we noted several issues that were not shown correctly on the previous plans. We have attempted to make the necessary corrections and have responded to staff comments as presented and also noted in those responses other items found where appropriate.

November 25, 2014 - Ryan Barns Memo:

1. Lewiston Public Works has the following comments at this time upon reviewing the application:

No response required

2. What size truck will be accessing the overhead doors? Please provide evidence that adequate turning radius is being provided for the design truck.

December 16, 2014

David Hediger

RE: Grimmel's Car Crushing, LLC

Page 2

We have attached plans showing truck movements for a 38' long straight truck with dumpster.

3. The fence currently located within the City Right of Way should be relocated to be on Private property to eliminate the shown encroachment.

We have shown the fence within the River Road right of way and along the side property line as being removed. We will reuse some of the fencing removed to install a fence in the new location shown on the revised plan.

4. How will the building's roof be drained? Concentrated flow from the roof could cause erosion problems to the proposed gravel.

The new roof will pitch to the rear of the proposed structure. Erosion of the proposed gravel area in front of the building will not be an issue.

5. What is the purpose of the gravel area between the proposed building and the 2:1 slope?

As we outlined in the cover letter, this gravel ramp will allow forklift or pedestrian movements between the existing building and the proposed building.

6. The site appears to be graded to pond water behind the building. The applicant should consider adding a swale or other drainage measures to prevent the water from ponding against the building.

If you review the survey plan prepared by Jones Associates, Inc. (Jones) provided with the application, you will note that the surveyor indicated that the ground in this area was obscured by piles of metal. No contours in this area were provided by the surveyor. During the design process, we added topographic information from the City GIS system to get some idea of what the contours may look like. Based upon the survey data from Jones and our onsite observations, the City GIS contour data is incorrect and most likely represents portions of the metal piles that existed when the maps were developed. Fully understanding the limits of the contour

information available to us, we proceeded with our site design. Our design work did consider drainage behind the building and included a 3' wide stone trench detail for backfilling along the rear of the building foundation to catch any drainage along the rear of the building. We also raised the foundation wall about 1.5' above the existing building floor elevation. The design engineer and the applicant are very comfortable with the proposed design. Given the problems with the City GIS contours and other information, we have removed them from the revised plan. We have shown a new "F" catch basin structure at the rear of the new addition to allow visual and physical access to the end of the drain system proposed. We have also noted that site grades along the rear of the building are to be determined in the field during construction after the metal piles have been removed.

7. The grading to the right of the building is concentrating drainage to sheet flow across the proposed gravel area, this will likely cause erosion, additional drainage features should be added to prevent the erosion.

We have revised the plans to show a catch basin structure at this location.

November 24, 2014 - David Hediger Memo

- The use is regulated as a conditional use. The application must speak to the conditional use criteria.

See attached ordinance requirement responses to Article X Sections 3 & 5.

- Application should address the specific development review criteria of Article XIII, Section 4.

See attached ordinance requirement responses.

December 16, 2014

David Hediger

RE: Grimmell's Car Crushing, LLC

Page 4

- Existing impervious ratios are questionable based upon aerial photos and site visits. Staff believes there may be more impervious area than shown on site plans. With that in mind, clarification of existing/proposed impervious areas is needed for new gravel area and modifications of front/side yard may be needed. Specific reference to modification criteria of Article XI, Section 3(9-11) needed.

Staff is correct; there is more impervious area on the property. As we discussed with staff in the very preliminary meetings, we believed that all areas used for metal storage yard, truck maneuvering, parking or equipment operations on this site are impervious. We limited our impervious calculations to the "Limits of Project Area" because this is the only location that we collected survey data based upon work completed by Jones at the site. Unfortunately, we did our calculations of green space based upon a tree line shown on the City GIS information, which was incorrect. We have revised the Jones survey plan to show the tree line located by survey and to show existing green space areas and storage yard areas which are impervious.

Based upon the survey data, we now know that the total green space area is 1,530 square feet, not 3,160 square feet as reported in our cover letter. Everything else within the project limits, including the area up to the fence that runs along the project frontage and land of the Turnpike Authority, is impervious area. There is no existing front or side yard area, except for the small triangular area located at the southwest corner of the property.

Currently, the impervious area in the front and side yards extend to the fence running along the street, side lot line and the Turnpike ownership. Site work proposed to raise the grades in the front of the building addition will create some additional green space within the front yard area where none currently exists. There will be no changes to green space within the side yard area. Therefore, no front yard or side yard reductions are required for this project.

December 16, 2014

David Hediger

RE: *Grimmel's Car Crushing, LLC*

Page 5

- Requested setback modifications need to make reference to the modification criteria of Article XI, Section 3(9-11) as applicable.

See attached ordinance requirements responses.

- They must be no outside storage of materials or equipment may occur on this side of the structure; this should be noted on the plan.

See note 9 on the revised plan.

- Contours behind the new structure suggest ponding may occur behind the structure. Pre/post grading behind the structure needed.

See the response to Ryan Barn's memo above.

- Drainage from the existing scrap yard area appears to drain across proposed gravel area. Grading should be revisited in this area.

See the response to Ryan Barn's memo above.

- Clearly delineate areas to be loamed and seeded.

We have noted areas to be loamed and seeded on the revised plans.

November 18, 2014 - Paul Ouellette e-mail

- However, my biggest concern that there will now be a large pile of metal and debris in close proximity to the new building. I have issues and concerns with LFD not having clear access to the new building addition in the event of a fire. I would need to see a yard floor plan in place that would provide an adequate 20' (foot) break between the outside pile of metal storage and the new building. This would leave us with access to both the outside pile and the building in the event of an emergency.

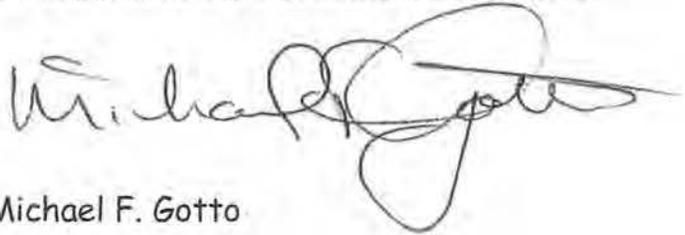
December 16, 2014
David Hediger
RE: Grimmel's Car Crushing, LLC
Page 6

See note 10 on the revised plan which requires the side setback area and 10' behind the new building shown to be kept clear for Fire Department access.

I trust you will find these responses acceptable to continue your review of this project. I will plan to attend the Planning Board meeting on December 22nd to address any new comments or concerns.

Respectfully Yours,

STONEBROOK CONSULTANTS, INC.

A handwritten signature in black ink, appearing to read "Michael F. Gotto". The signature is fluid and cursive, with a large loop at the end.

Michael F. Gotto

cc: Gary Grimmel

Ordinance Requirements

Article X, Section 3

(1) Value

- a. The size of the proposed use is comparable with existing commercial uses.
- b. Hours of operations will not change and there will be no increase in traffic generated at this site.
- c. there will be no increase in noise, dust, odor, vibration, glare, smoke, litter or other nuisances with this project.
- d. There will be no impact on the quality or quantity of groundwater with this project.
- e. There are no unusual physical characteristics of this site or this project that could aggravate impacts upon surrounding properties.

(2) Vehicular and pedestrian access

There are no changes to existing vehicular or pedestrian access to this property. Safe internal access movements have been provided in accordance with all City design standards.

(3) Municipal or other facilities

- a. There are no sewer or water services required for this project.
- b. Stormwater flow will be reduced by this project.
- c. Access to the site for the fire department will be improved by this project.

(4) Soils

The soils at this site have adequate capacity and stability to support the improvements proposed. This project will not create erosion. This site is not located in a flood plain and this project will increase vegetated areas on the property.

(5) Scale and design

This expansion will be similar in size and construction as the existing structure on site.

Article X, Section 5

With this submittal, we request that the conditional use requested be valid for a period of two years from the date of approval.

Article XIII, Section 4

(a) Utilization of the site - This is the only area of this property that can be used for the proposed building addition. Development is proposed within a previously used portion of the site to minimize impacts on environmental resources of the area.

(b) Traffic movements - This project will not impact traffic movements of any type in this area.

(c) Access to the site - The project will use the existing access to this lot.

(d) Internal vehicular circulation - This project will use the same internal vehicular movements that have been established and used for many years. Safe movement for pedestrians, passenger vehicles, delivery trucks and emergency vehicles will not be changed.

(e) Pedestrian circulation - Safe pedestrian movement between the buildings has been provided with these improvements.

- (f) Stormwater management - Not Applicable.
- (g) Erosion control - All improvements for this project will be constructed under the requirements of the erosion control plans provided. All erosion control measures proposed meet or exceed all local and State requirements.
- (h) Water supply - Not Applicable
- (i) Sewer disposal - Not Applicable
- (j) Utilities - This project will connect to the existing building electrical services. No other utility services are proposed.
- (k) Natural features - There are no natural features on this lot within the project limits.
- (l) Groundwater protection - There will be no impact to groundwater resources by this project.
- (m) Water and air pollution - There will be no water or air pollution with this project.
- (n) Exterior lighting - The exterior lighting for this project has been designed to meet the City ordinance requirements.
- (o) Waste disposal - There will be no new waste generated by this project.
- (p) Lot layout - Not Applicable
- (q) Landscaping - None proposed
- (r) Shoreland relationship - This project meets shoreland zone requirements for this area.

(s) Open space - This is not a residential project and there are no open space areas proposed with this project.

(t) Technical and financial capacity - The application and design plans have been prepared by professionals qualified to perform this work. The applicant has completed a number of projects in this State and has a proven track record for completing these types of projects.

(u) Buffering - Not Applicable

(v) Compliance with district regulations - Development of this lot as proposed meets all of the requirements of Article XI, Section 15.

(w) Design consistent with performance standards - The improvements proposed will comply with the performance standards of Article XII, insofar as they may be applicable.

Article IX, Section 3. (9)

Under Article IX, Section 3. (9), the Planning Board is allowed to modify these setback requirements by 25%. The side setback requirement is 25' and we have proposed to reduce this requirement by 25% to 18.75'. The front setback requirement is 50' and we propose to reduce this requirement by 25% to 37.5'.

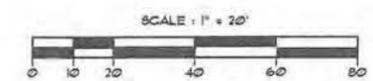
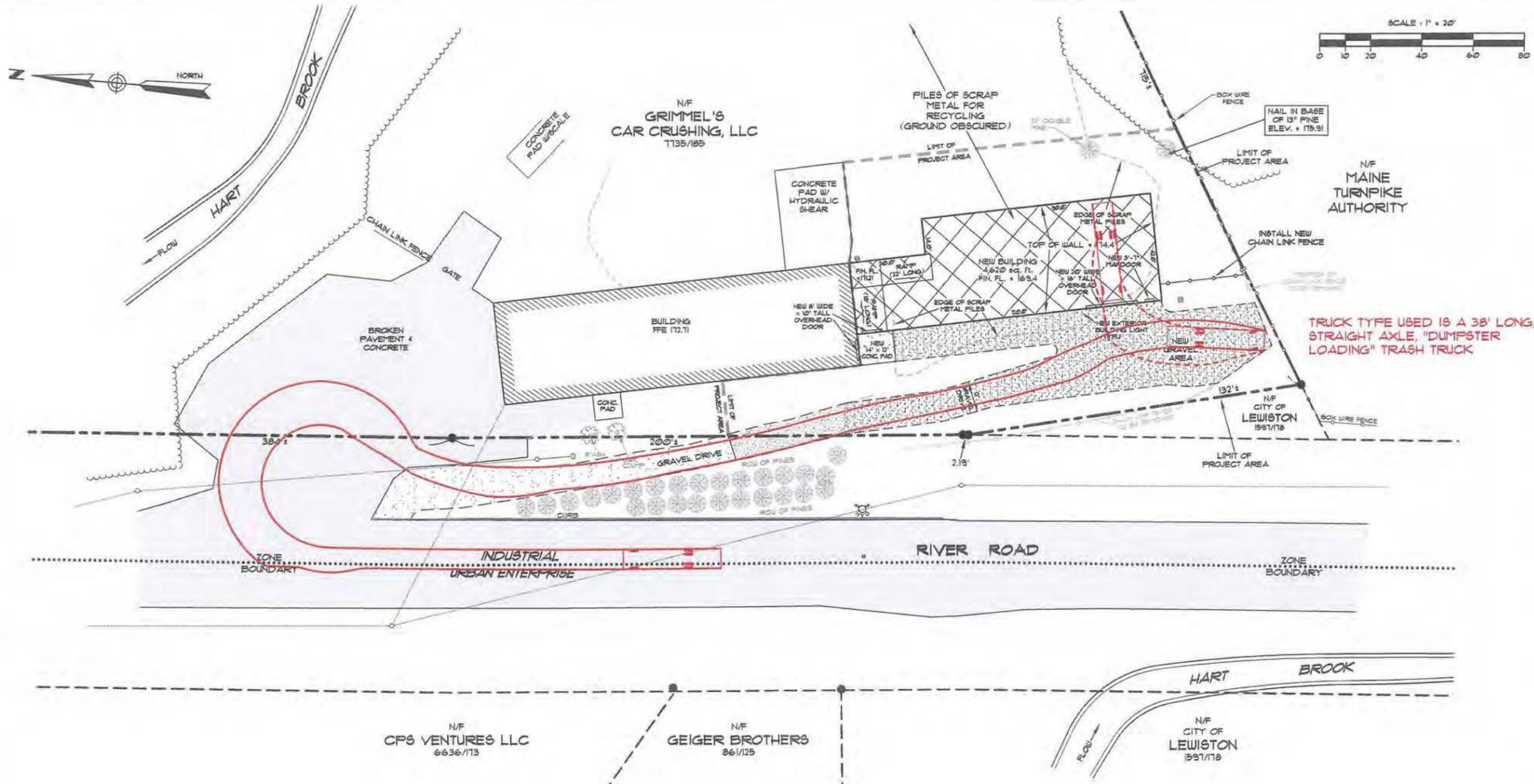
To support all of the modifications requested and in response to the requirements of Article IX, Section 3. (9) a.- d., we offer the following:

(a) The structures proposed on this site will be constructed in accordance with the current building code.

(b) The reductions requested will not result in undue impact to adjacent properties due to the location of the structure. Placement of the structure as shown will place the entrance and service areas farther from the adjacent buildings and the location will not block solar access.

(c) The modifications requested will not impede public safety services from reaching or providing service to this site or adjacent properties.

(d) This is not a residential project or an existing service station, so these requirements are not applicable to this project.

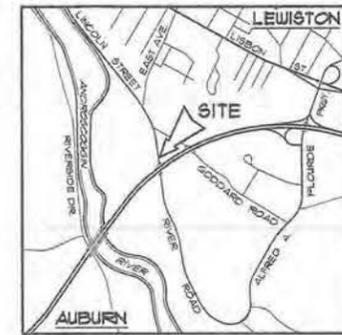


LEGEND

- BOUNDARY LINE (SUBJECT PARCEL)
- ZONING BOUNDARY
- - - - - BOUNDARY LINE (OTHER)
- IRON ROD OR PIPE FOUND
- N/F NOW OR FORMERLY
- 2356/09 BOOK AND PAGE NUMBER
- - - - - LIMIT OF EXISTING SCRAP METAL FILES
- UTILITY POLE WITH OVER-HEAD WIRES
- EXISTING HYDRANT
- EXISTING WATER SHUT OFF VALVE
- EXISTING CHAIN LINK FENCE
- EXISTING BOX WIRE FENCE
- ▨ NEW BUILDING
- ▨ EXISTING BUILDING
- ▨ EXISTING PAVEMENT
- ▨ NEW GRAVEL AREA
- ▨ NEW EROSION CONTROL BLANKET
- ▨ NEW 3' WIDE STONE TRENCH DRAIN

NOTES

- 1) FOR COMPLETE BOUNDARY AND TOPOGRAPHIC INFORMATION REFERENCE IS MADE TO A PLAN ENTITLED "PARTIAL BOUNDARY & TOPOGRAPHIC PLAN, GRIMMEL'S CAR CRUSHING, LLC", DATED AUGUST 1, 2014, PREPARED BY JONES ASSOCIATES, INC. TOPOGRAPHY WITHIN THE SCRAP METAL FILE AREA IS APPROXIMATE AND IS BASED UPON CITY OF LEWISTON G.I.S. CONTOURS.
- 2) ALL BOOK AND PAGE NUMBERS REFER TO THE CUMBERLAND COUNTY REGISTRY OF DEEDS.
- 3) OWNER OF RECORD - GRIMMEL'S CAR CRUSHING LLC DEED REFERENCE - BOOK T135, PAGE 199 TAX MAP IT3, LOT 34
- 4) PARCEL AREA = 4.41 ACRES.
- 5) THE PARCEL IS NOT LOCATED WITHIN A 100-YEAR FLOOD HAZARD AREA AS SHOWN ON THE FEMA FLOOD INSURANCE RATE MAP COMMUNITY PANEL #30001C 0329 E, DATED JULY 9, 2013.
- 6) THE PARCEL IS LOCATED IN THE "INDUSTRIAL" ZONING DISTRICT.
- 7) A PROJECT AREA HAS BEEN DEFINED ON THIS PLAN FOR THE PURPOSE OF DETERMINING AN ACCURATE SITE TABULATION COMPARISON BETWEEN EXISTING CONDITIONS AND POST-CONSTRUCTION CONDITIONS.
- 8) CONTRACTOR SHALL CONTACT DIGSAFE/ON-TARGET PRIOR TO EXCAVATION TO DETERMINE THE LOCATION, DEPTH, SIZE AND EXISTENCE OF ALL PUBLIC AND PRIVATE UTILITY LINES, TANKS AND/OR STRUCTURES WITHIN THE PROJECT AREA.



TRUCK TYPE USED IS A 38' LONG, STRAIGHT AXLE, "DUMPSTER LOADING" TRASH TRUCK

TRUCK ENTRANCE ROUTE

PROJECT: **36 RIVER ROAD**
LEWISTON, MAINE

CLIENT: **GRIMMEL'S CAR CRUSHING, LLC**
80 FLEERSCOT VILLAGE MAIN STREET - TOPSHAM, MAINE 04686

145 LISBON ST. - SUITE 408
ROCKLAND, ME 04840
Tel: (207) 576-3313

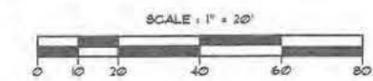
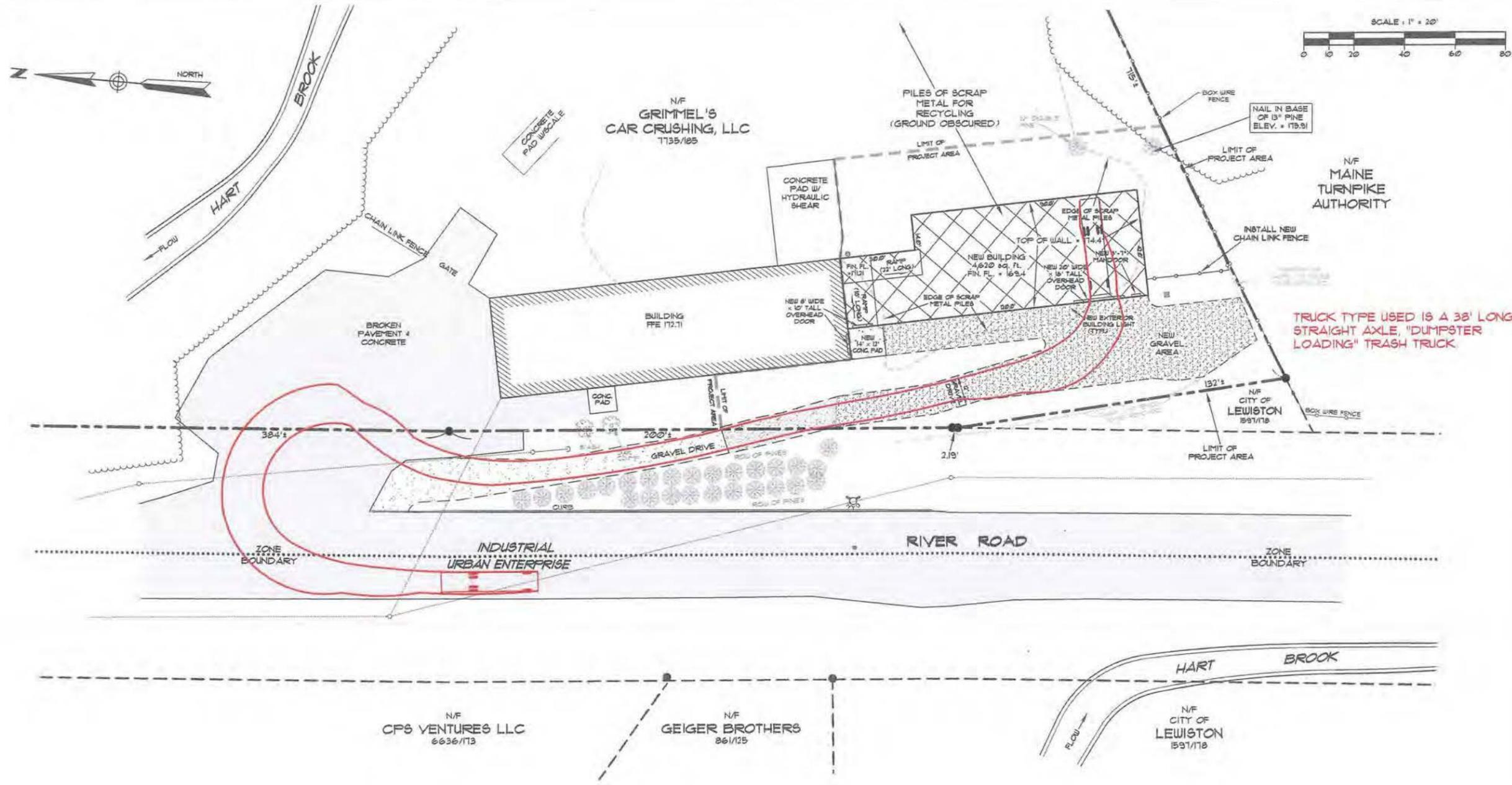
173 PLEASANT STREET
ROCKLAND, ME 04841
Tel: (207) 318-7161

SUMMIT
GEOENGINEERING SERVICES

JOB NO. - 14170
SHEET - 1

SHEET TITLE: **TRUCK ENTRANCE ROUTE**

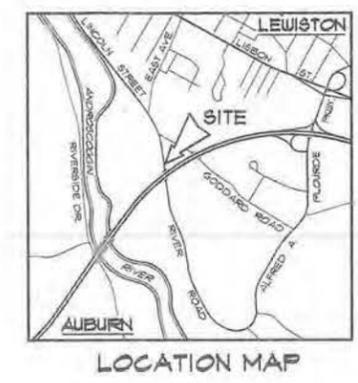
DATE: DEC. 11, 2014
SCALE: 1" = 20'
DRAWN BY: KRF
CHECKED BY: WMP



LEGEND

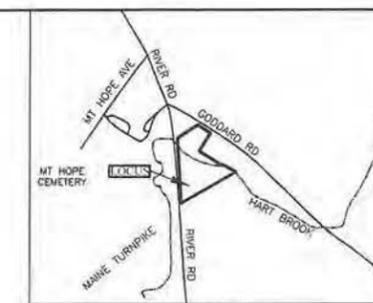
	BOUNDARY LINE (SUBJECT PARCEL)
	ZONING BOUNDARY
	BOUNDARY LINE (OTHER)
	IRON ROD OR PIPE FOUND NOW OR FORMERLY
	BOOK AND PAGE NUMBER
	LIMIT OF EXISTING SCRAP METAL FILES
	UTILITY POLE WITH OVER-HEAD WIRES
	EXISTING HYDRANT
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	EXISTING BOX WIRE FENCE
	NEW BUILDING
	EXISTING BUILDING
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	NEW GRAVEL AREA
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	NEW 3' WIDE STONE TRENCH DRAIN

- NOTES**
- 1) FOR COMPLETE BOUNDARY AND TOPOGRAPHIC INFORMATION REFERENCE IS MADE TO A PLAN ENTITLED "PARTIAL BOUNDARY & TOPOGRAPHIC PLAN, GRIMMEL'S CAR CRUSHING, LLC", DATED AUGUST 1, 2014, PREPARED BY JONES ASSOCIATES, INC. TOPOGRAPHY WITHIN THE SCRAP METAL FILE AREA IS APPROXIMATE AND IS BASED UPON CITY OF LEWISTON G.I.S. CONTOURS.
 - 2) ALL BOOK AND PAGE NUMBERS REFER TO THE CUMBERLAND COUNTY REGISTRY OF DEEDS.
 - 3) OWNER OF RECORD - GRIMMEL'S CAR CRUSHING LLC DEED REFERENCE - BOOK 1135, PAGE 183 TAX MAP 113, LOT 34
 - 4) PARCEL AREA = 4.44 ACRES.
 - 5) THE PARCEL IS NOT LOCATED WITHIN A 100-YEAR FLOOD HAZARD AREA AS SHOWN ON THE FEMA FLOOD INSURANCE RATE MAP COMMUNITY PANEL #22001C0323E, DATED JULY 8, 2013.
 - 6) THE PARCEL IS LOCATED IN THE "INDUSTRIAL" ZONING DISTRICT.
 - 7) A PROJECT AREA HAS BEEN DEFINED ON THIS PLAN FOR THE PURPOSE OF DETERMINING AN ACCURATE SITE TABULATION COMPARISON BETWEEN EXISTING CONDITIONS AND POST-CONSTRUCTION CONDITIONS.
 - 8) CONTRACTOR SHALL CONTACT DIGSAFE/ON-TARGET PRIOR TO EXCAVATION TO DETERMINE THE LOCATION, DEPTH, SIZE AND EXISTENCE OF ALL PUBLIC AND PRIVATE UTILITY LINES, TANKS AND/OR STRUCTURES WITHIN THE PROJECT AREA.

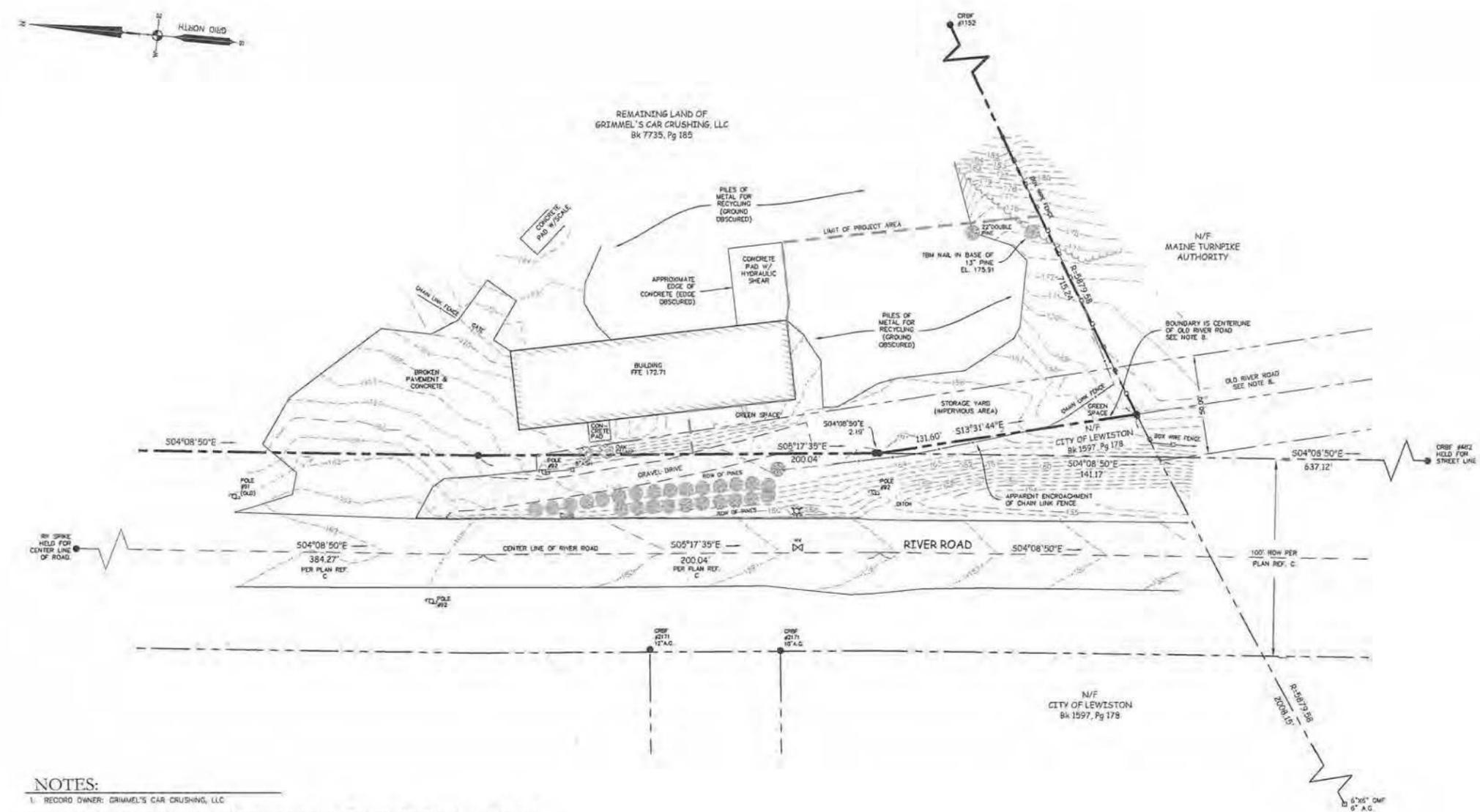


TRUCK TYPE USED IS A 38' LONG, STRAIGHT AXLE, "DUMPSTER LOADING" TRASH TRUCK.

<p>PROJECT:</p> <p>36 RIVER ROAD LEWISTON, MAINE</p>	<p>SHEET TITLE:</p> <p>TRUCK EXIT ROUTE</p>	<p>SCALE: 1" = 20'</p> <p>DATE: DEC. 11, 2014</p>	<p>DRAWN BY: KRF</p> <p>CHECKED BY: WMP</p>
	<p>CLIENT:</p> <p>GRIMMEL'S CAR CRUSHING, LLC 80 PEEPERSCOTT VILLAGE MAIN STREET • TOPSHAM, MAINE 04086</p>	<p>PROJECT:</p> <p>36 RIVER ROAD LEWISTON, MAINE</p>	<p>SCALE: 1" = 20'</p> <p>DATE: DEC. 11, 2014</p>
<p>Stoneybrook Consultants, Inc.</p> <p>155 Buckfield Road - Turner, Maine 04282 (207) 514-2444, Fax: (207) 514-2442, www.stoneybrook.com</p>			
<p>145 LEBRON ST. - SUITE 401 LEWISTON, ME 04203 Tel: (207) 514-3910</p> <p>SUMMIT GEOENGINEERING SERVICES</p>			
<p>JOB NO. - 14170</p> <p>SHEET 2</p>			

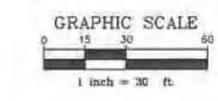


LOCATION PLAN



LEGEND

	SUBJECT BOUNDARY LINE
	ABUTTER OR RIGHT OF WAY LINE
	CENTER LINE OF RIVER ROAD
	TREELINE
	N/F OR FORMERLY
	DEED BOOK PAGE REFERENCE
	TAX MAP AND LOT NUMBER
	DECIDUOUS / CONIFEROUS TREE
	GRANITE MONUMENT FOUND
	IRON PIPE/ROD/REBAR FOUND
	5/8" REBAR W/ CAP OR MAG NAIL SET
	UTILITY POLE
	WATER VALVE
	GAS VALVE



NOTES:

1. RECORD OWNER: GRIMMEL'S CAR CRUSHING, LLC
2. PARCEL DEED REFERENCE: SEE DEED FROM GARY GRIMMEL AND BETTY GRIMMEL TO GRIMMEL'S CAR CRUSHING, LLC DATED APRIL 23, 2009, RECORDED AT THE ANDROSCOGG COUNTY REGISTRY OF DEEDS IN BOOK 7735, PAGE 185.
3. ALL BOOK AND PAGE REFERENCES REFER TO THE ANDROSCOGG COUNTY REGISTRY OF DEEDS.
4. PARCEL TAX MAP REFERENCE: CITY OF LEWISTON, MAP 179, LOT 34
5. ALL BEARINGS ARE REFERENCED TO GRID NORTH WA83 MAINE WEST
6. ELEVATIONS SHOWN ARE TIED TO NAVD83 PER RTK GPS OBSERVATIONS
7. THE LOCATION, SIZE, AND DEPTH OF UNDERGROUND UTILITY LINES, TANKS, AND DR STRUCTURES NOT DETERMINED BY THIS SURVEY
8. THE ORIGINAL LOCATION OF RIVER ROAD SHOWN AS OLD RIVER IS BASED ON PLAN REF. C AND BEST FIT OF PLAN REF. A & E. THIS PORTION OF RIVER ROAD WAS RELOCATED OCTOBER 5, 1982 BY CITY OF LEWISTON SEE LEWISTON BOOK OF PLANS BOOK 4, PAGE 45. IT WAS THEN WIDENED TO 100 FEET WIDE BASED ON PLAN REFERENCE C (SEE ALSO LAYOUT AND TAKING IN 1964/75). THE OLD RELOCATED PORTION OF RIVER ROAD HAS BEEN ABANDONED FOR MORE THAN 30 YEARS AND OWNERSHIP IS ASSUMED TO REVERT TO ABUTTERS.
9. PLAN REFERENCES:
 - A. STANDARD BOUNDARY SURVEY OF PROPERTY OWNED BY H.E. SARGENT, INC., CITY OF LEWISTON, 94 AND 152 RIVER ROAD PREPARED BY A.R.C.C. LAND SURVEYORS INC. DATED 5/27/99 RECORDED IN SAID REGISTRY OF DEEDS PLAN BOOK 40, PAGE 156.
 - B. GRIMMEL INDUSTRIES OFFICE BUILDING & TRUCK SCALE SITE PLAN PREPARED FOR GARY & BETTY GRIMMEL BY DAVID B. CHITTM DATED MAY 14, 1990 REVISED 4/18/91.
 - C. STATE OF MAINE DEPARTMENT OF TRANSPORTATION RIGHT OF WAY MAP RIVER ROAD, LEWISTON, FEDERAL AID PROJECT NO. M-0002(4) PART 4, D.O.T. FILE NO. 1-178, SHEET NO. 4 THROUGH 6 OF 6 SHEETS DATED JANUARY 1986 RECORDED IN SAID REGISTRY IN BOOK 33, PAGES 83 THROUGH 85.
 - D. PLAN OF PROPERTY DUNE GRAVEL PIT INC. PREPARED BY WRIGHT - PIERCE DATED SEPTEMBER 1984 RECORDED IN SAID REGISTRY OF DEEDS PLAN BOOK 31, PAGE 97.
 - E. MAINE TURNPIKE AUTHORITY MAINE TURNPIKE SECTION 2 - PORTLAND TO AUGUSTA, PROPERTY PLAN, SHEET NO 1 OF 5 SHEETS, DATED MARCH 1954, RECORDED IN SAID REGISTRY OF DEEDS PLAN BOOK 11, PAGE 555.
 - F. MAINE TURNPIKE AUTHORITY MAINE TURNPIKE SECTION 2 - PORTLAND TO AUGUSTA, PROPERTY PLAN, SUPPLEMENTAL SHEET NO. 2, DATED MARCH 1954, RECORDED IN SAID REGISTRY OF DEEDS PLAN BOOK 14, PAGE 734.
 - G. DEFINITION OF SOUTH RIVER ROAD, LEWISTON MAINE SOUTH AVENUE TO GARCELONS FERRY BY THE ANDROSCOGG COUNTY COMMISSIONERS PREPARED BY GEO. BARRON, ENGINEER, DATED MAY 9, 1938 RECORDED IN SAID REGISTRY OF DEEDS IN THE ANDROSCOGG COUNTY COMMISSIONERS PLAN VOLUME 2, PAGE 59.

CERTIFICATION:
 I HEREBY CERTIFY THAT THIS BOUNDARY SURVEY CONFORMS TO THE STATE OF MAINE BOARD OF LICENSURE FOR PROFESSIONAL LAND SURVEYORS, STANDARDS OF PRACTICE (CHAPTER 90, SECTIONS 1 & 2), EXCEPTION: ONLY PARTIAL BOUNDARY SURVEY PERFORMED

Michael A. Hartman
 MICHAEL A. HARTMAN, P.L.S. #2433
 FOR JONES ASSOCIATES INC.

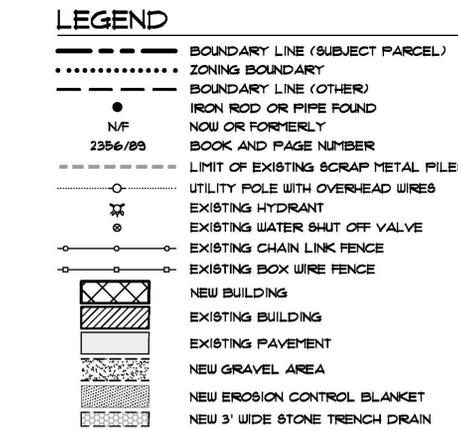
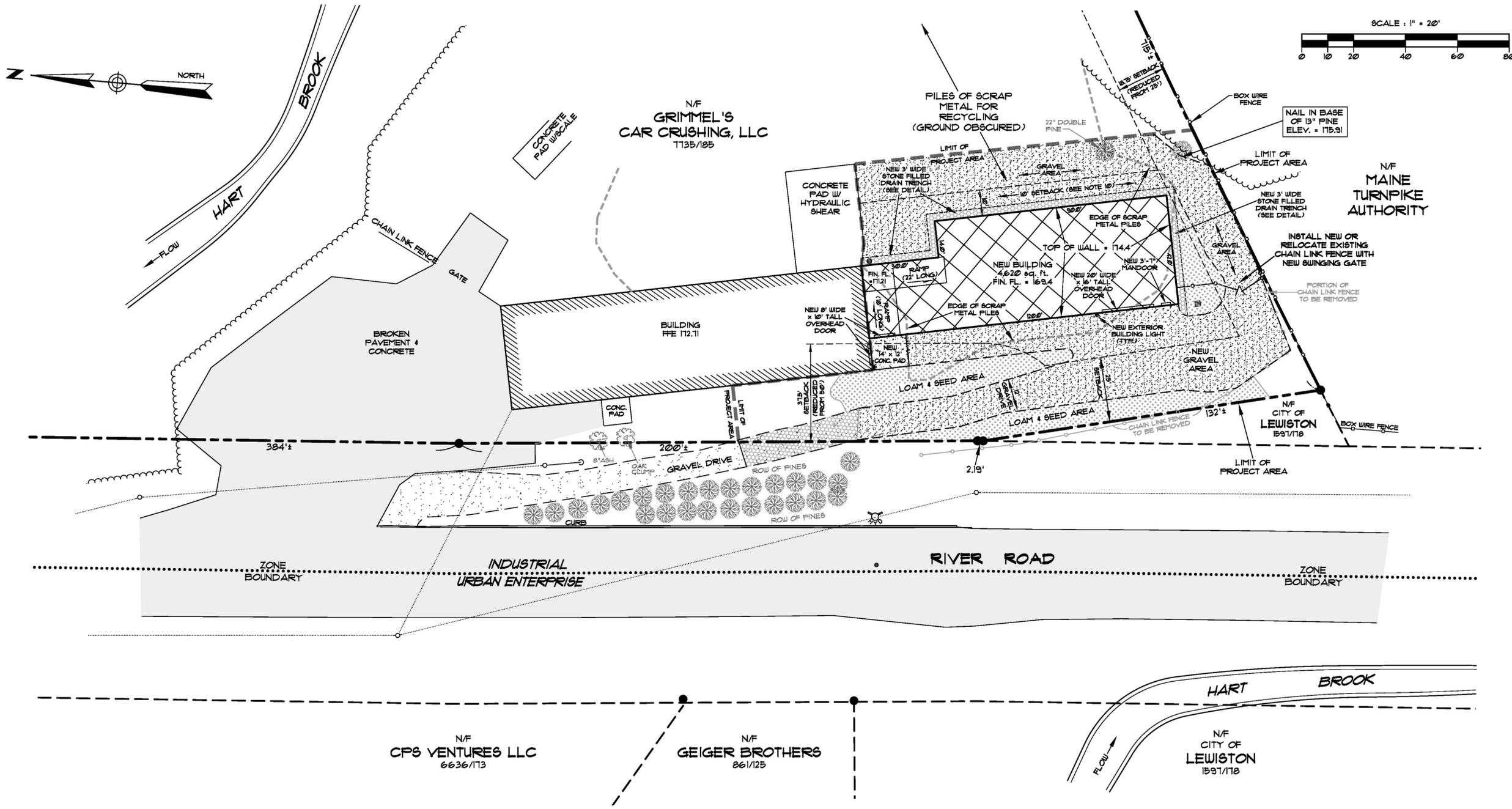


REVISIONS			
NO.	DATE	DESCRIPTION	BY
01	12/15/2014	ADDED TREELINE, PROJECT LIMIT AREA, ETC.	MH

PARTIAL BOUNDARY AND TOPOGRAPHIC PLAN
GRIMMEL'S CAR CRUSHING, LLC
 36 RIVER ROAD
 LEWISTON, MAINE

PREPARED FOR:
 GRIMMEL'S CAR CRUSHING, LLC
 80 PEJEPSCOT VILLAGE MAIN STREET
 TOPSHAM, ME 04086

PREPARED BY: JONES ASSOCIATES INC. Valuations, Surveying, Land Environmental Remediation <small>200 PLAINFIELD SPRING ROAD, SUITE 403, LEWISTON, MAINE 04240 Phone: (207) 524-0335; Website: jonesassoc.com</small>	RECORD OWNERS: GRIMMEL'S CAR CRUSHING, LLC 80 PEJEPSCOT VILLAGE MAIN ST. TOPSHAM, ME 04086
	PLAN DATE: 8/1/2014 SCALE: 1"=30' PROJ. #: 14-032LE



- NOTES**
- FOR COMPLETE BOUNDARY AND TOPOGRAPHIC INFORMATION REFERENCE IS MADE TO A PLAN ENTITLED "PARTIAL BOUNDARY & TOPOGRAPHIC PLAN, GRIMMEL'S CAR CRUSHING, LLC", DATED AUGUST 1, 2014, PREPARED BY JONES ASSOCIATES, INC. TOPOGRAPHY WITHIN THE SCRAP METAL FILE AREA IS APPROXIMATE AND IS BASED UPON CITY OF LEWISTON G.I.S. CONTOURS.
 - ALL BOOK AND PAGE NUMBERS REFER TO THE ANDROSCOGGIN COUNTY REGISTRY OF DEEDS.
 - OWNER OF RECORD - GRIMMEL'S CAR CRUSHING LLC
DEED REFERENCE - BOOK 1135, PAGE 185
TAX MAP 119, LOT 34
 - PARCEL AREA = 4.4± ACRES.
 - THE PARCEL IS NOT LOCATED WITHIN A 100-YEAR FLOOD HAZARD AREA AS SHOWN ON THE FEMA FLOOD INSURANCE RATE MAP COMMUNITY PANEL 23001 C 0323 E, DATED JULY 8, 2013.
 - THE PARCEL IS LOCATED IN THE "INDUSTRIAL" ZONING DISTRICT.
 - A PROJECT AREA HAS BEEN DEFINED ON THIS PLAN FOR THE PURPOSE OF DETERMINING AN ACCURATE SITE TABULATION COMPARISON BETWEEN EXISTING CONDITIONS AND POST-CONSTRUCTION CONDITIONS.
 - CONTRACTOR SHALL CONTACT DIGSAFE/ON-TARGET PRIOR TO EXCAVATION TO DETERMINE THE LOCATION, DEPTH, SIZE AND EXISTENCE OF ALL PUBLIC AND PRIVATE UTILITY LINES, TANKS AND/OR STRUCTURES WITHIN THE PROJECT AREA.
 - THERE SHALL BE NO OUTSIDE STORAGE OF MATERIALS OR EQUIPMENT WITHIN THE FRONT SETBACK AREA OF THE NEW BUILDING SHOWN ON THIS PLAN.
 - THE SIDE SETBACK AREA AND 10' BEHIND THE NEW BUILDING SHOWN ON THIS PLAN SHALL BE KEPT CLEAR WITH NO OUTSIDE STORAGE OF MATERIALS FOR FIRE DEPARTMENT ACCESS TO THE BUILDING.

SITE TABULATIONS

EXISTING CONDITIONS WITHIN PROJECT AREA	
GRAVEL DRIVE	460 sq. ft.
SCRAP YARD	15,710 sq. ft.
TOTAL IMPERVIOUS AREA	16,170 sq. ft. (82.1%)
GREEN SPACE	15,930 sq. ft. (11.9%)
TOTAL AREA	11,700 sq. ft. (100%)

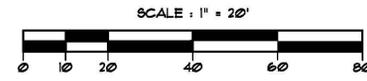
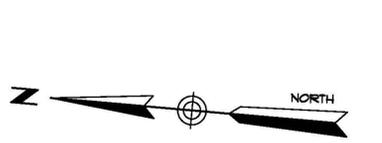
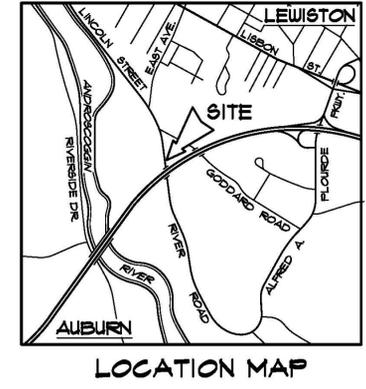
PROPOSED CONDITIONS WITHIN PROJECT AREA	
GRAVEL/CONCRETE/SCRAP YARD	9,281 sq. ft.
BUILDING	4,620 sq. ft.
TOTAL IMPERVIOUS AREA	13,901 sq. ft. (78.6%)
GREEN SPACE	3,793 sq. ft. (21.4%)
TOTAL AREA	11,700 sq. ft. (100%)

APPROVAL

APPROVED BY THE CITY OF LEWISTON PLANNING BOARD

CHAIRMAN _____ DATE _____

IF DEVELOPMENT HAS NOT OCCURRED AS DEFINED WITHIN THE SCOPE OF THE CITY OF LEWISTON CODE OF ORDINANCES, WITHIN 24 MONTHS OF THE DATE OF APPROVAL, DEVELOPMENT REVIEW APPROVAL SHALL EXPIRE. THE APPLICANT MAY NOT BEGIN CONSTRUCTION OR OPERATION OF THE DEVELOPMENT UNTIL A NEW APPROVAL IS GRANTED (ARTICLE XIII, SECTION 11).



REVISED: DECEMBER 18, 2014 - ADD 10' SETBACK & GATE
REVISED: DECEMBER 16, 2014 - SHOW LOAN & SEED AREAS

SHEET TITLE: **SITE PLAN**

PROJECT: **36 RIVER ROAD**
LEWISTON, MAINE

CLIENT: **GRIMMEL'S CAR CRUSHING, LLC**
80 FLEETWOOD VILLAGE MAIN STREET - TOPSHAM, MAINE 04086

SCALE: 1" = 20'
DATE: NOV. 11, 2014

DRAWN BY: KRF
CHECKED BY: WMP

Stoneybrook Consultants, Inc.
476 Buckfield Road - Turner, Maine 04282
(207) 514-7491 Voice / (207) 514-7492 Fax

119 PLEASANT STREET
ROCKLAND, ME 04841
Tel: (207) 318-1161

45 LEBRON ST. SUITE 601
LEWISTON, ME 04240
Tel: (207) 516-3313

SUMMIT
ENGINEERING SERVICES

STATE OF MAINE
WILLIAM M. PETERLIN
5787
LICENSED PROFESSIONAL ENGINEER

JOB NO. - 14170
SHEET 1

N/F
GRIMMEL'S
CAR CRUSHING, LLC
1135/105

NOTES

- FOR COMPLETE BOUNDARY AND TOPOGRAPHIC INFORMATION REFERENCE IS MADE TO A PLAN ENTITLED "PARTIAL BOUNDARY & TOPOGRAPHIC PLAN, GRIMMEL'S CAR CRUSHING, LLC", DATED AUGUST 1, 2014, PREPARED BY JONES ASSOCIATES, INC. TOPOGRAPHY WITHIN THE SCRAP METAL PILE AREA IS APPROXIMATE AND IS BASED UPON CITY OF LEWISTON G.I.S. CONTOURS.
- CONTRACTOR SHALL CONTACT DIGSAFE/ON-TARGET PRIOR TO EXCAVATION TO DETERMINE THE LOCATION, DEPTH, SIZE AND EXISTENCE OF ALL PUBLIC AND PRIVATE UTILITY LINES, TANKS AND/OR STRUCTURES WITHIN THE PROJECT AREA.

REVISED: DEC. 16, 2014 - ADD GATE TO FENCE
REVISED: DEC. 16, 2014 - CHANGE SCALE TO 1" = 10'
SHEET TITLE:

**TOPOGRAPHIC
SITE PLAN**

PROJECT: **36 RIVER ROAD**
LEWISTON, MAINE
CLIENT: **GRIMMEL'S CAR CRUSHING, LLC**
80 FLEETBOLT VILLAGE MAIN STREET - TOPSHAM, MAINE 04086

Stoneybrook Consultants, Inc.
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(207) 514-7491 Voice / (207) 514-7492 Fax

113 PLEASANT STREET
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145 LISBON ST. - SUITE 601
LEWISTON, ME 04240
Tel: (207) 516-3313

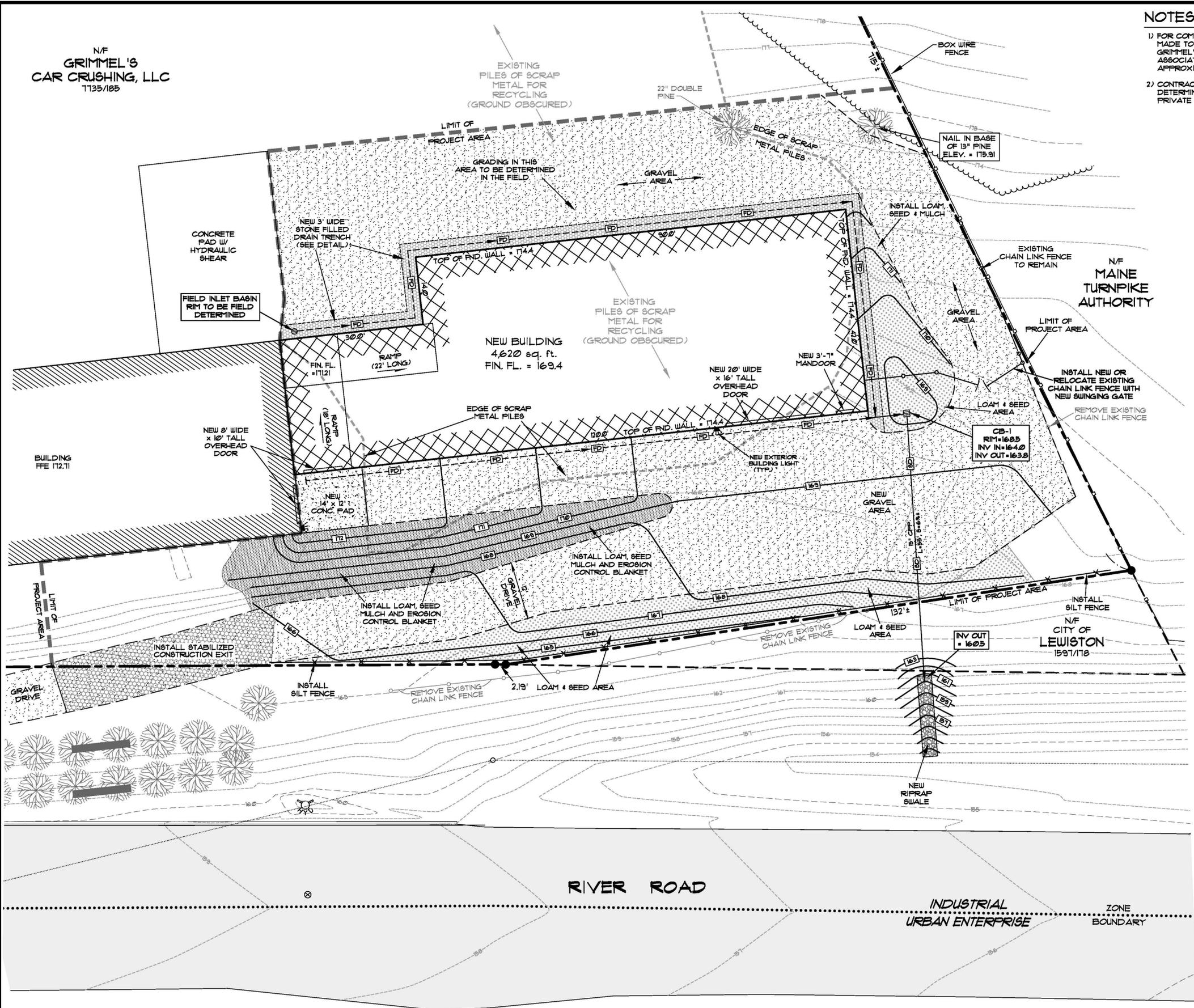
WILLIAM M. PETERLIN
5787
LICENSED PROFESSIONAL ENGINEER

JOB NO. - 14170

SHEET **2**

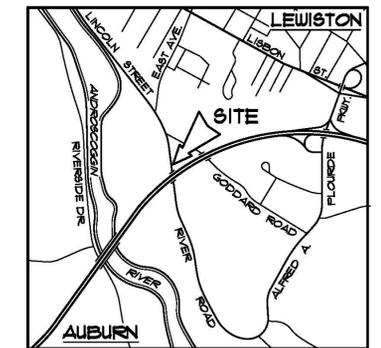
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SCALE: 1" = 10'
DATE: NOV. 11, 2014



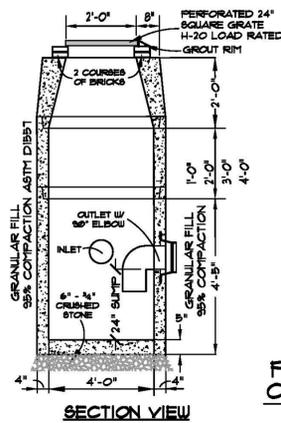
LEGEND

- BOUNDARY LINE (SUBJECT PARCEL)
- ZONING BOUNDARY
- BOUNDARY LINE (OTHER)
- IRON ROD OR PIPE FOUND NOW OR FORMERLY
- NF 2356/03 BOOK AND PAGE NUMBER
- - - - - EXISTING CONTOUR
- - - - - NEW CONTOUR
- - - - - NEW 6" PERF. PVC UNDERDRAIN PIPE
- UTILITY POLE WITH OVERHEAD WIRES
- ⊕ EXISTING HYDRANT
- ⊕ EXISTING WATER SHUT OFF VALVE
- CHAIN LINK FENCE TO BE REMOVED
- EXISTING BOX WIRE FENCE
- NEW CHAIN LINK FENCE
- ▨ NEW BUILDING
- ▨ EXISTING BUILDING
- ▨ EXISTING PAVEMENT
- ▨ NEW GRAVEL AREA
- ▨ NEW EROSION CONTROL BLANKET
- ▨ NEW 3' WIDE STONE TRENCH DRAIN
- ▨ NEW EROSION CONTROL BLANKET
- ▨ NEW SILT FENCE
- LIMIT OF EXISTING SCRAP METAL PILES (TOPOGRAPHY IN THIS AREA IS APPROXIMATE)



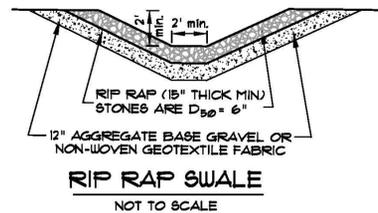
LOCATION MAP



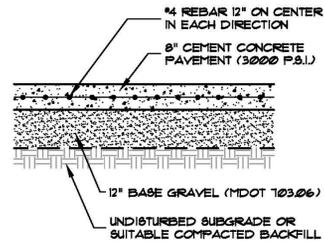


- STRUCTURES AND COVERS TO MEET CITY OF LEWISTON STANDARDS**
- NOTES:**
1. CONCRETE: 4000 PSI AFTER 28 DAYS.
 2. REINFORCING: H-20 LOADING, 4X4/4X4 W/MT.
 3. 3" SHIPLAP JOINTS SEALED WITH ONE STRIP OF BUTYL RUBBER SEALANT.
 4. EACH CASTING TO HAVE LIFTING HOLES CAST IN.
 5. PIPE OPENINGS CAST IN AS REQUIRED.
 6. 6" THICK H-20 SLAB TOP AVAILABLE.

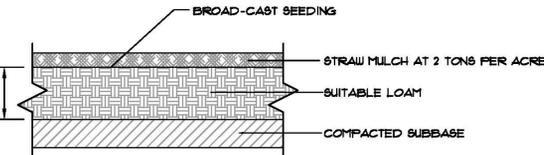
PRECAST CONCRETE CATCH BASIN DETAIL
NOT TO SCALE



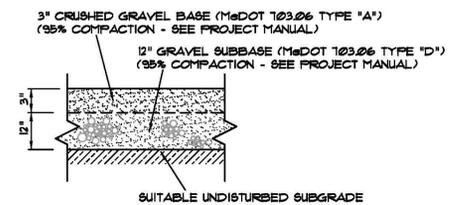
RIP RAP SWALE
NOT TO SCALE



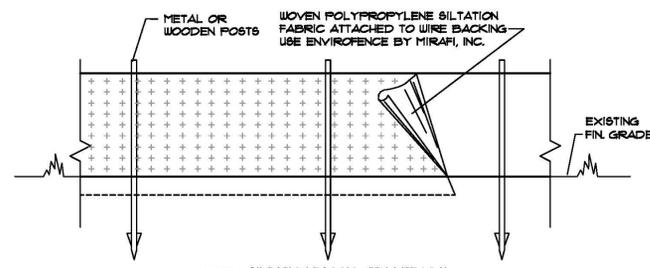
CROSS SECTION CONCRETE PAD
NOT TO SCALE



LOAM & SEED DETAIL
NOT TO SCALE

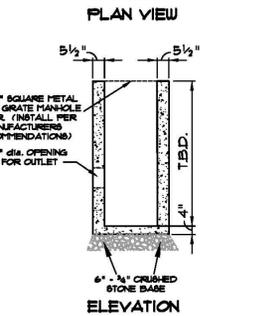
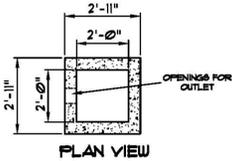


DRIVEWAY GRAVEL DETAIL
NOT TO SCALE

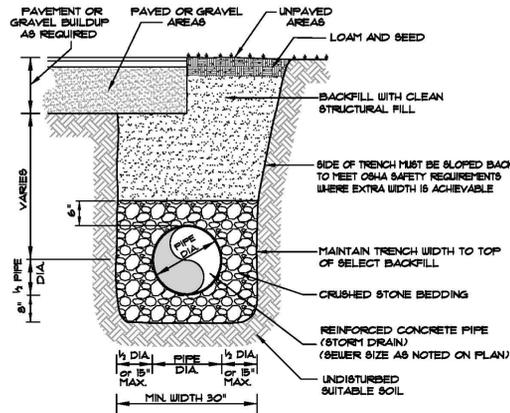


SILT FENCE DETAIL
NOT TO SCALE

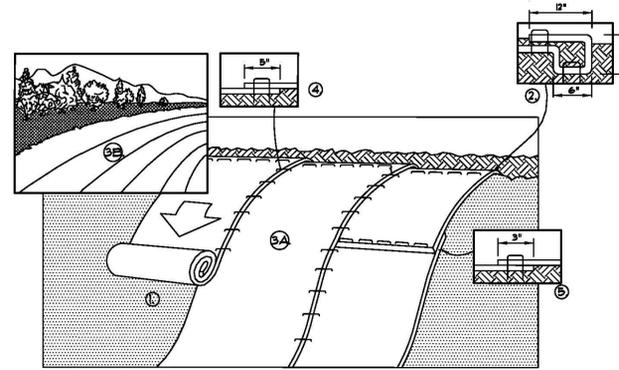
NOTE
REFERENCE IS MADE TO THE BEST MANAGEMENT PRACTICE FOR EROSION AND SEDIMENT CONTROL: B-1 SEDIMENT BARRIERS



PRECAST CONCRETE FIELD INLET
NOT TO SCALE

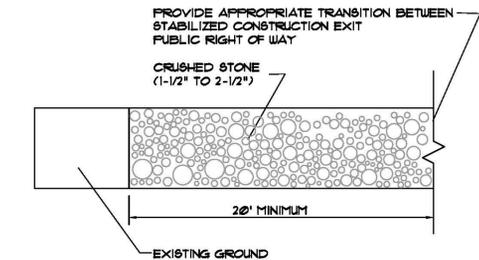


STORM DRAIN TRENCH DETAIL
NOT TO SCALE



1. PREPARE SOIL BEFORE INSTALLING BLANKETS, INCLUDING ANY NECESSARY APPLICATION OF LINE, FERTILIZER, AND SEED.
2. BEGIN AT THE TOP OF THE SLOPE BY ANCHORING THE BLANKET IN A 6" DEEP X 6" WIDE TRENCH WITH APPROXIMATELY 12" OF BLANKET EXTENDED BEYOND THE UP-SLOPE PORTION OF THE TRENCH. ANCHOR THE BLANKET WITH A ROW OF STAPLES/STAKES APPROXIMATELY 12" APART IN THE BOTTOM OF THE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING. APPLY SEED TO COMPACTED SOIL AND FOLD REMAINING 12" PORTION OF BLANKET BACK OVER SEED AND COMPACTED SOIL. SECURE BLANKET OVER COMPACTED SOIL WITH A ROW OF STAPLES/STAKES SPACED APPROXIMATELY 12" APART ACROSS THE WIDTH OF THE BLANKET.
3. ROLL THE BLANKETS (A) DOWN OR (B) HORIZONTALLY ACROSS THE SLOPE. BLANKETS WILL UNROLL WITH APPROPRIATE TENSION AGAINST THE SOIL SURFACE. ALL BLANKETS MUST BE SECURELY FASTENED TO SOIL SURFACE BY PLACING STAPLES/STAKES IN APPROPRIATE LOCATIONS AS SHOWN IN THE STAPLE PATTERN GUIDE.
4. THE EDGES OF PARALLEL BLANKETS MUST BE STAPLED WITH APPROXIMATELY 2"-5" OVERLAP DEPENDING ON BLANKET TYPE. TO ENSURE PROPER SEAM ALIGNMENT, PLACE THE EDGE OF THE OVERLAPPING BLANKET (BLANKET BEING INSTALLED ON TOP) EVEN WITH THE COLORED SEAM STITCH ON THE PREVIOUSLY INSTALLED BLANKET.
5. CONSECUTIVE BLANKETS APPLIED DOWN THE SLOPE MUST BE PLACED END OVER END (SHINGLE STYLE) WITH AN APPROXIMATE 3" OVERLAP. STAPLE THROUGH OVERLAPPED AREA APPROXIMATELY 12" APART ACROSS ENTIRE BLANKET WIDTH. NOTE: IN LOOSE SOIL CONDITIONS, THE USE OF STAPLE OR STAKE LENGTHS GREATER THAN 6" MAY BE NECESSARY TO PROPERLY SECURE THE BLANKETS.

EROSION CONTROL BLANKET DETAIL
NOT TO SCALE



1. STONE SIZE - AASHTO DESIGNATION M 43, SIZE #2 (2 1/2" - 1 1/2") USE CRUSHED STONE
2. LENGTH - AS EFFECTIVE BUT NOT LESS THAN 50'
3. THICKNESS - NOT LESS THAN 6"
4. WIDTH - NOT LESS THAN FULL WIDTH OF ALL POINTS OF INGRESS OR EGRESS
5. WASHING - WHEN NECESSARY, WHEELS SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE ONTO PUBLIC RIGHT OF WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE WHICH DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN. ALL SEDIMENT SHALL BE PREVENTED FROM ENTERING ANY STORM DRAIN, DITCH, OR WATERCOURSE THROUGH USE OF SAND BAGS, GRAVEL, BOARDS, OR OTHER APPROVED METHODS.
6. MAINTENANCE - THE STABILIZED CONSTRUCTION EXIT SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS OF WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEANOUT OF ANY MEASURED USES TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED, OR TRACKED ONTO PUBLIC RIGHT OF WAYS MUST BE REMOVED IMMEDIATELY.

STABILIZED CONSTRUCTION EXIT DETAIL
NOT TO SCALE

GENERAL CONSTRUCTION NOTES

- THE CONTRACT WORK TO BE PERFORMED ON THIS PROJECT CONSISTS OF FURNISHING ALL REQUIRED LABOR, MATERIALS, EQUIPMENT, IMPLEMENTS, PARTS AND SUPPLIES NECESSARY FOR OR APPURTENANT TO, THE INSTALLATION OF CONSTRUCTION IMPROVEMENTS IN ACCORDANCE WITH THESE DRAWINGS AND AS FURTHER ELABORATED IN ANY ACCOMPANYING SPECIFICATIONS.
2. THE WORK SHALL BE PERFORMED IN A THOROUGH WORKMANLIKE MANNER. ALL CONTRACTORS TO CONFORM TO ALL APPLICABLE OSHA STANDARDS. ANY REFERENCE TO A SPECIFICATION OR DESIGNATION OF THE AMERICAN SOCIETY FOR TESTING MATERIALS, FEDERAL SPECIFICATIONS, OR OTHER STANDARDS, CODES OR ORDERS, REFERS TO THE MOST RECENT OR LATEST SPECIFICATION OR DESIGNATION.
 3. ALL CONSTRUCTION WITHIN THE CITY OF LEWISTON AND/OR STATE OF MAINE RIGHT OF WAY SHALL COMPLY WITH CITY PUBLIC WORKS AND/OR MDOT STANDARDS.
 4. THE OWNER IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS REQUIRED BY THE CITY OF LEWISTON PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS FROM THE CITY OF LEWISTON REQUIRED TO PERFORM ALL THE WORK (STREET OPENINGS, BUILDING PERMIT, ETC.). THE CONTRACTOR SHALL POST ALL BONDS AS REQUIRED, PAY ALL FEES, PROVIDE PROOF OF INSURANCE AND PROVIDE TRAFFIC CONTROL NECESSARY FOR THIS WORK.
 5. PRIOR TO CONSTRUCTION, THE SITE CONTRACTOR IS TO INFORM ALL AREA UTILITY COMPANIES AND GOVERNMENTAL AGENCIES OF PLANNED CONSTRUCTION. THE SITE CONTRACTOR IS REQUIRED TO CONTACT DIG-SAFE (1-800-225-4971) AT LEAST 3 BUSINESS DAYS PRIOR TO ANY EXCAVATION TO VERIFY ALL UNDERGROUND AND OVERHEAD UTILITY LOCATIONS.
 6. THE PROJECT DRAWINGS ARE GENERALLY SCHEMATIC AND INDICATE THE POSSIBLE LOCATION OF EXISTING UNDERGROUND UTILITIES. INFORMATION ON EXISTING UTILITIES HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING MUNICIPAL RECORD MAPS, AND FIELD SURVEY. IT IS NOT GUARANTEED TO BE CORRECT OR COMPLETE. UTILITIES ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF THE EXISTING UTILITIES AS APPROPRIATE. THE CONTRACTOR IS TO PROVIDE ADEQUATE MEANS OF SUPPORT AND PROTECTION DURING THE EXCAVATING AND BACKFILLING OPERATIONS. SHOULD ANY UNCHARTED OR INCORRECTLY CHARTED UTILITIES BE FOUND, THE CONTRACTOR SHALL CONTACT THE DESIGN ENGINEER IMMEDIATELY FOR DIRECTIONS BEFORE PROCEEDING FURTHER WITH THE WORK IN THIS AREA.
 7. OSHA REGULATIONS MAKE IT UNLAWFUL TO OPERATE CRANES, BOOMS, HOISTS, ETC. WITHIN TEN FEET (10') OF ANY ELECTRIC LINE. IF THE CONTRACTOR MUST OPERATE CLOSER THAN 10', THE CONTRACTOR MUST CONTACT THE POWER COMPANY TO MAKE ARRANGEMENTS FOR PROPER SAFEGUARDS BEFORE ENCROACHING ON THIS REQUIREMENT.
 8. IT IS THE CONTRACTOR'S RESPONSIBILITY TO EXAMINE ALL PLANS, APPROVALS, AND DETAILS FOR ADDITIONAL INFORMATION. THE CONTRACTOR SHALL VERIFY ALL THE SITE CONDITIONS IN THE FIELD AND CONTACT THE DESIGN ENGINEER IF THERE ARE ANY DISCREPANCIES REGARDING THE CONSTRUCTION DOCUMENTS AND/OR FIELD CONDITIONS SO THAT AN APPROPRIATE REVISION CAN BE MADE PRIOR TO BIDDING.
 9. ALTERNATIVE METHODS AND PRODUCTS OTHER THAN THOSE SPECIFIED MAY BE USED IF REVIEWED AND APPROVED IN WRITING BY THE OWNER, DESIGN ENGINEER, AND APPROPRIATE GOVERNMENTAL AGENCY PRIOR TO INSTALLATION.
 10. ALL EXCAVATION SHALL BE BACKFILLED TO EXISTING GRADE BEFORE THE END OF THE DAY OR ADEQUATELY PROTECTED FROM DANGER TO HUMANS AND ANIMALS.
 11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ALL FIELD LAYOUT. THE OWNER WILL PROVIDE A BENCH MARK AT THE CONSTRUCTION SITE FROM WHICH TO BEGIN LAYOUT.
 12. THE CONTRACTOR SHALL FURNISH ELECTRICAL POWER, WATER, AND SANITARY FACILITIES FOR HIS EXCLUSIVE USE AT THE CONSTRUCTION SITE SHOULD THE CONTRACTOR DEEM THIS ESSENTIAL FOR THE PROPER PERFORMANCE OF THE CONTRACT.
 13. WORK MAY PROGRESS MONDAY THROUGH FRIDAY 7:00 AM TO 5:00 PM. WORK AT OTHER TIMES MAY PROCEED UPON WRITTEN APPROVAL BY THE OWNER AND THE CITY OF LEWISTON.
 14. THE CONTRACTOR SHALL GUARANTEE THE FAITHFUL REMEDY OF ANY DEFECTS DUE TO FAULTY MATERIALS OR WORKMANSHIP AND GUARANTEE PAYMENT FOR ANY RESULTING DAMAGE WHICH SHALL APPEAR WITHIN A PERIOD OF ONE (1) YEAR FROM THE DATE OF SUBSTANTIAL COMPLETION OF THE PROJECT.
 15. THE CONTRACTOR SHALL PROVIDE AS-BUILT RECORDS OF ALL CONSTRUCTION (INCLUDING UNDERGROUND UTILITIES) TO THE OWNER AT THE END OF CONSTRUCTION.
 16. PROPER IMPLEMENTATION AND MAINTENANCE OF EROSION CONTROL MEASURES ARE OF PARAMOUNT IMPORTANCE FOR THIS PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH ALL EROSION CONTROL MEASURES SHOWN ON THE PLANS. ADDITIONAL EROSION CONTROL MEASURES SHALL BE INSTALLED IF DEEMED NECESSARY BY ON-SITE INSPECTIONS BY THE OWNER, THEIR REPRESENTATIVES, OR STATE/LOCAL/FEDERAL INSPECTORS AT NO ADDITIONAL COST TO THE OWNER.
 17. ALL UTILITY CONNECTIONS, INSTALLATION, SIZES AND MATERIALS SHALL COMPLY WITH THE CITY OF LEWISTON AND THE APPROPRIATE UTILITY COMPANY CONSTRUCTION STANDARDS.

EROSION & SEDIMENTATION CONTROL NOTES

1. THE CONTRACTOR SHALL INSPECT EROSION & SEDIMENT CONTROL MEASURES WEEKLY AND AFTER HEAVY RAINFALLS THROUGHOUT THE DURATION OF THE PROJECT INCLUDING WEEKENDS AND HOLIDAYS. ALL EROSION AND SEDIMENT CONTROLS SHALL BE MAINTAINED DURING THE ENTIRE CONSTRUCTION PROCESS.
2. THE CONTRACTOR IS RESPONSIBLE FOR CONSTRUCTING THE EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH THE "MAINE EROSION AND SEDIMENT CONTROL HANDBOOK FOR CONSTRUCTION: BEST MANAGEMENT PRACTICES", DEPARTMENT OF ENVIRONMENTAL PROTECTION, DATED MARCH 2009 (DEPLW 989). ADDITIONAL MEASURES SHALL BE TAKEN AS REQUIRED BY THE RATE OF INSPECTIONS BY THE OWNER, THEIR REPRESENTATIVES OR STATE/LOCAL/FEDERAL INSPECTORS AT NO ADDITIONAL COST TO THE OWNER.
3. AREAS DISTURBED DURING CONSTRUCTION SHALL BE MINIMIZED. AREAS SHALL BE TEMPORARILY STABILIZED WITH MULCH OR SEEDS UNLESS OTHERWISE SPECIFIED. NOT BE WORKED FOR MORE THAN 1 DAY'S. STABILIZE AREAS WITHIN 48 HOURS OF THE INITIAL DISTURBANCE OR PRIOR TO ANY STORM EVENT. PERMANENT SEEDING SHALL TAKE PLACE WITHIN 1 DAY'S OF FINAL GRADING.
4. PRIOR TO CONSTRUCTION, PROPERLY INSTALL SEDIMENT BARRIERS AT THE DOWN GRADIENT EDGE OF THE DISTURBED AREA AND ADJACENT TO DRAINAGE CHANNELS WITHIN THIS AREA.
5. SILT FENCE LOCATIONS SHOWN ARE APPROXIMATE. INSTALL WHERE APPROPRIATE TO CONTROL SEDIMENTATION ON AND OFF SITE. SILT FENCE SHALL BE REMOVED AFTER THE SITE IS STABILIZED WITH AT LEAST 75% VEGETATED GROWTH.
6. NO SLOPES, EITHER PERMANENT OR TEMPORARY, SHALL BE STEEPER THAN TWO TO ONE (2 TO 1).
7. PERMANENT SEEDING OF THE DISTURBED AREAS IS NOT COMPLETED BEFORE SEPTEMBER USE TEMPORARY MULCHING (DORMANT SEEDING MAY BE ATTEMPTED AS WELL) TO PROTECT THE SITE AND DELAY SEEDING UNTIL THE NEXT RECOMMENDED SEEDING PERIOD.
8. THE FOLLOWING METHODS MAY BE USED TO PERFORM A DORMANT SEEDING:
 - A) WHEN SOIL CONDITIONS PERMIT, BETWEEN OCTOBER 15th AND BEFORE SNOW FALL, PREPARE THE SEEDS, LINE AND FERTILIZE. APPLY THE SELECTED SEED MIXTURE, AND MULCH AND ANCHOR DOUBLE THE REGULAR SEEDING RATES FOR THIS TYPE OF SEEDING.
 - B) DORMANT SEEDS NEED TO BE ANCHORED EXTREMELY WELL ON SLOPES, BUT SHOULD NOT BE USED IN DITCH BASINS AND AREAS OF CONCENTRATED FLOWS. DORMANT SEEDING SHALL NOT BE USED IN WATERSHEDS SENSITIVE TO WATER QUALITY IMPACTS (FISHERIES, PHOSPHORUS SENSITIVE LAKES AND PONDS, ETC.). INSTEAD, CONSTRUCTION SITES NEXT TO SENSITIVE AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT SEEDING BY SEPTEMBER 15th.
 - C) DORMANT SEEDING REQUIRES INSPECTION IN THE SPRING. ALL AREAS WHERE COVER IS INADEQUATE MUST BE IMMEDIATELY RESEEDED AND MULCHED AS SOON AS POSSIBLE.
9. REVEGETATION MEASURES WILL COMPEL UPON COMPLETION OF CONSTRUCTION EXCEPT AS NOTED ABOVE. ALL DISTURBED AREAS NOT OTHERWISE STABILIZED WILL BE GRADED, SMOOTHED AND PREPARED FOR FINAL SEEDING AS FOLLOWS:
 - A) 4" OF LOAM WILL BE SPREAD OVER DISTURBED AREAS AND SMOOTHED TO A UNIFORM SURFACE.
 - B) APPLY LIMESTONE AND FERTILIZER ACCORDING TO SOIL TEST. IF SOIL TESTING IS NOT FEASIBLE ON SMALL OR VARIABLE SITES, OR WHERE TESTING IS CRITICAL, FERTILIZER MAY BE APPLIED AT THE RATE OF 200 POUNDS PER ACRE OR 20 POUNDS PER 1000 s.f. SQUARE FEET USING 10-10-2 (N-P2O5-K2O) OR EQUIVALENT. APPLY GROUND LIMESTONE (EQUIVALENT TO 50% CALCIUM PLUS MAGNESIUM OXIDE) AT A RATE OF 3.3 TONS PER ACRE (150 lbs. PER 1000 s.f.)
 - C) FOLLowing SEED BED PREPARATION, DITCHES AND BARK SLOPES WILL BE SEED TO A MIXTURE OF 4% CREEPING RED FESCUE, 5% REDTOP AND 48% TALL FESCUE. THE LAWN AREAS WILL BE SEED TO A PREMIUM TURF MIXTURE OF 44% KENTUCKY BLUEGRASS, 44% CREEPING RED FESCUE AND 12% PERENNIAL RYEGRASS. SEEDING RATE IS ONE POUND PER 1000 s.f. LAWN QUALITY SOY HAY MAY BE SUBSTITUTE FOR SEED. SEED MIX SHALL CONTAIN 10% ANNUAL RYEGRASS.
 - D) HAY MULCH AT THE RATE OF 10-30 lbs PER 1000 s.f. OR A HYDRO-APPLICATION OF ASPHALT, WOOD OR PAPER FIBER SHALL BE APPLIED FOLLOWING SEEDING. FOLLOWING SEEDING, A SUITABLE BINDER SUCH AS CURABOL OR RTB PLUS WILL BE USED ON HAY MULCH FOR WIND CONTROL.

REVISIONS: DEC. 19, 2014 - ADD FIELD INLET STRUCTURE
REVISED: DEC. 16, 2014 - ADD CB, TRENCH AND RIP RAP DETAILS

SHEET TITLE: **CONSTRUCTION NOTES & DETAILS**

PROJECT: **36 RIVER ROAD**
LEWISTON, MAINE

CLIENT: **GRIMMEL'S CAR CRUSHING, LLC**
80 PEEPS-COT VILLAGE MAIN STREET - TOPSHAM, MAINE 04086

DESIGNER: **Stoneybrook Consultants, Inc.**
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DRAWN BY: KRF
CHECKED BY: WMP

SCALE: 1" = 20'
DATE: NOV. 11, 2014

JOB NO. - 14110
SHEET **3**

STATE OF MAINE
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