

CITY OF LEWISTON
PLANNING BOARD MEETING
Monday, July 28, 2014 – 5:30 P.M.
City Council Chambers – First Floor
Lewiston City Building
27 Pine Street, Lewiston

AGENDA

I. ROLL CALL

II. ADJUSTMENTS TO THE AGENDA

III. CORRESPONDENCE

IV. PUBLIC HEARINGS:

- a) An application submitted by Taylor Engineering Associates on behalf of Chelsea Market Realty, LLC for the construction of a 6,387 sf restaurant to be built at 750 Sabattus Street.
- b) An application submitted by AnnBeha Architects on behalf of Bates College for the construction of two new residence halls with a campus store and mail facility at 55 & 65 Campus Avenue.

V. OTHER BUSINESS:

- a) Request for recommendation on the acquisition of 114-118 Bartlett Street.
- b) Executive Session to discuss real estate negotiations, of which the premature disclosure of the information would prejudice the competitive bargaining position of the City pursuant to 1 M.R.S.A. § 405(6)(C).
- c) Any other business Planning Board Members may have relating to the duties of the Lewiston Planning Board.

VI. READING OF THE MINUTES: Motion to adopt the June 9, 2014 draft minutes.

VII. ADJOURNMENT



CITY OF LEWISTON

Department of Planning & Code Enforcement



TO: Planning Board
FROM: David Hediger, City Planner
DATE: July 24, 2014
RE: July 28, 2014 Planning Board Agenda Item IV(a)

An application submitted by Taylor Engineering Associates on behalf of Chelsea Market Realty, LLC for the construction of a 6,387 sf restaurant to be built on the existing foundation, including a 826 sf addition at 750 Sabattus Street.

Taylor Engineering on behalf of Chelsea Market Realty, LLC has submitted an application for the construction of a 6,387 sf restaurant to be built on the existing foundation with an 826 sf addition at 750 Sabattus Street. This property of 1.05 acres consists of a 5,561 sf restaurant (in the process of being demolished) located in the Highway Business (HB) district in which said use is permitted. The applicant is demolishing most, if not all, of the entire structure with the exception of the foundation. They will be rebuilding on the existing foundation adding 810 sf to the structure.

Overall, site improvements to the site are minimal given the redevelopment and existing conditions of the site. Staff notes the following with respect to the proposed development:

- The applicant has referenced all of the applicable sections of the Zoning and Land Use Code including Article XIII, Section 4.
- In effort to improve access management to the site, the existing curb cut in front of the building shall be eliminated and the easterly entrance will become an “entrance only”.
- The overall impervious area will decrease approximately 2,608 sf with new landscaping and street trees being installed along the Sabattus Street side of the building.
- No new parking is being proposed. Article XII, Section 17(d) requires one space per three seats for restaurants. Based on the floor plan provided to staff, the restaurant will have seating for 180 customers, requiring 60 spaces. The applicant is proposing 58 spaces. Existing spaces in front of the building are being eliminated to improve access management and onsite circulation. The existing lot will be restriped for greater efficiency. Article XII, Section 17(d) provides the applicant the ability to demonstrate a differing parking need (either fewer or greater) than otherwise required. The code states that “. . . sufficient spaces to accommodate the normal parking demand of the use (must be provided) without requiring on-street parking. The number of required spaces shall be determined by the planning board for major project development review. . .’ Staff believes this request for two fewer parking spaces is reasonable in that the overall safety of accessing and utilizing the site is being improved despite losing some existing parking spaces in front of the building.

No other concerns have been raised by city staff. Therefore, approval is recommended pursuant to Article XIII, Section 4 Zoning and Land Use Code with the following condition:

- All site improvements must be completed in accordance with the approved plan prior the issuance of a certificate of occupancy.

ACTIONS NECESSARY

1. Make a motion to consider an application submitted by Taylor Engineering Associates on behalf of Chelsea Market Realty, LLC for the construction of a 6,387 sf restaurant to be built on the existing foundation, including an 826 sf addition at 750 Sabattus Street.
2. Obtain input on the application;
3. Make a determination that the application is complete;
4. Make finding that the application meets all of the necessary criteria contained in the Zoning and Land Use Code, including Article XIII, Section 4 of the Zoning and Land Use Code and to grant approval to Chelsea Market Realty, LLC for the construction of a 6,387 sf restaurant at 750 Sabattus Street (subject to any concerns raised by the Planning Board or staff).

July 16, 2014

RE: Restaurant
750 Sabattus Street
TEA Project No.: 2014-032

David Hediger
City Planner
City of Lewiston
27 Pine Street
Lewiston, Maine 04240

Dear David:

Enclosed please find the following submittals:

- ▶ One original and 14 copies of this letter
- ▶ 15 copies of the development review application and checklist
- ▶ 15 copies of an 11x17 site plan
- ▶ Check for the \$700 application fee

We have been retained to prepare a site plan for the property located at 750 Sabattus Street. There is an existing 120 seat restaurant on the site with plans to demolish it and build a new 180 seat restaurant on the existing foundation. There will also be an 810 sf addition to the west end of the building and the entrance to the building on the north will be relocated.

The current layout of the site has three, two way curb cuts on Sabattus Street and one on Stanley Street. The parking lot is striped for approximately 75 cars, however many of the spaces are undersized at 16' long and aisles are only about 10 feet wide in the rear. There are also 7 spaces located in front of the building, between it and Sabattus Street.

We are proposing to reuse most of the existing parking lot as it is currently paved, patching potholes and re-striping. The curb cut in front of the entrance will be closed and most of the area in front of the building be converted to green space, with lawn, some landscaping, two street trees and a walkway to the sidewalk. Also, the easternmost Sabattus Street curb cut will be relocated, reduced to 20' wide and signed as "NO EXIT". The existing sidewalk along Sabattus Street will be reconstructed to reflect these changes. No parking will be allowed within the 15' front yard of Sabattus Street, however we are proposing to use the existing pavement within the yard area for parking along the common lot line with the bar to the west, within the 15' front yard of Stanley Street, along the existing fence next to the Stanley Street lots and within the paper street right-of-way near Sabattus Street.

With the elimination of 6 spaces on the west end of the building due to the expansion and

restriping of the parking lot to meet the standards, the proposed parking lot will have spaces for 58 cars. The requirement for a 180 seat restaurant at one space per three seats would be 60 spaces. Aisle widths meet the standards in most cases, except at the rear of the building the existing two way aisle is 20.7' instead of the required 22'. We are requesting that the Planning Board approve the number of spaces and parking lot as proposed.

With the increase from 120 seats to 180 seats we are also projecting an increase of 52 trips for the peak hour. We do not think this will be an issue with the traffic light at the easternmost curb cut and the elimination of the exit onto Sabattus Street at the westernmost curb cut.

The project is located in the Jepson Brook Urban Impaired Stream watershed. However, the elimination of pavement in front of the building has reduced the impervious surface on the property by approximately 2,608 square feet.

Under Article XIII, Section 4, Approval Criteria, we have the following comments:

- (a) *Utilization of the site:* The project as proposed is primarily using the existing layout of the site. The building foundation will be reused in its current location with an expansion on the west end. The existing paved area will be reused as a parking lot, except for the area between the building and Sabattus Street. No additional unpaved areas will be impacted.
- (b) *Traffic Movement into and out of the development area:* Three of the four existing curb cuts will be reused in this plan, with the center curb cut on Sabattus Street completely closed and the easternmost curb cut reduced to an enter only. The westernmost Sabattus Street curb cut is signalized and greatly enhance traffic movement into and out of the site. The curb cut on Stanley Street will primarily be used as an entrance and exit for west bound Sabattus Street traffic as patrons become familiar with the location.
- (c) *Access into the site:* The existing curb cuts to be reused and reconfigured will provide safe and convenient access to the site.
 - (1) The grades and sight distances meet the City's street policy.
- (d) *Internal Vehicular Circulation:* The layout of the parking lot will provide for the safe movement of vehicles through the site.
 - (1) A WB-40 delivery vehicle will be able to enter the site from the easternmost Sabattus Street curb cut, make a delivery to the kitchen located at the northeast corner of the building and exit the site at either of the two other curb cuts.
 - (2) There will be clear routes throughout the site for emergency vehicles.
 - (3) The layout of the parking lot provides adequate space for safe and convenient circulation of vehicles. No vehicle is required to back onto a street.
 - (4) No new streets are part of the proposal.
 - (5) All abutting streets to this site meet city standards.
 - (6) This is not a residential project and marginal access streets, or other requirements, do not apply.
- (e) *Pedestrian circulation:* The reconfigured sidewalks on Sabattus Street will provide for safe and convenient access to the parking lot and building entrances.
- (f) *Stormwater Management:* This project will only disturb approximately 4,200 sf of the site. Impervious surface will be reduced by about 2,600 sf by removal of pavement from the front of the building.

- (g) *Erosion Control*: There is little need for erosion control as much of the site is already graded and paved. When the pavement is removed from the front of the building it will be immediately graded and stabilized with loam, seed and landscaping areas.
- (h) *Water Supply*: This project will use water provided by the City of Lewiston. Usage will be similar to the previous restaurant on the site.
- (i) *Sewage disposal*: Sewage will be discharge to the sewer system through the existing sanitary service. A grease trap will be installed to prevent fats and oils from entering the sewer system.
- (j) *Utilities*: Electric, phone and cable services will be available at the nearby utility pole.
- (k) *Natural features*: No natural features will be impacted by this plan.
- (l) *Groundwater protection*: This plan will not adversely impact groundwater.
- (m) *Water and air pollution*: This plan will not adversely impact surface waters or the air.
- (n) *Exterior lighting*: This plan proposes to reuse the existing exterior lighting on the three poles in the parking lot.
- (o) *Waste disposal*: An fenced enclosure will be provided for a trash dumpster and waste fry oil container. Waste will be disposed of at a licensed facility. No hazardous waste will be generated.
- (p) *Lot layout*: No new lots will be created by this plan.
- (q) *Landscaping*: This plan proposes to create a lawn area with landscaping and two street trees out of the paved parking lot that exists in front of existing building.
- (r) *Shoreland relationship*: This project is not adjacent to any water bodies.
- (s) *Open space*: This plan is not required to provide any open space.
- (t) *Technical and financial capacity*: The developer will provide the City adequate documentation of technical and financial capacity prior to beginning construction.
- (u) *Buffering*: There are existing stockade fences between the parking lot and adjacent residential properties.
- (v) *Compliance with district regulations*: The proposed project is a permitted use in the HB District. All dimensional requirements have been met except for parking spaces are proposed to be located within the 15' minimum front and side yards as they were for the previous restaurant. Also, impervious coverage will slightly exceed the maximum allowed, however it will be less than the existing condition.
- (w) *Design consistent with performance standards*: The project has been designed to be in compliance with the performance standards, except as noted in the letter and on the plans.

Please let me know if you have any additional questions or comments.

Very truly yours,

Taylor Engineering Associates



Norman G. Chamberlain II, PE
Vice President

PROJECT DATA

The following information is required where applicable, in order to complete the application

IMPERVIOUS SURFACE AREA/RATIO

Existing Total Impervious Area	37,273	sq. ft.
Proposed Total Paved Area	28,294	sq. ft.
Proposed Total Impervious Area	34,665	sq. ft.
Proposed Impervious Net Change	-2,608	sq. ft.
Impervious surface ratio existing	80.9	% of lot area
Impervious surface ratio proposed	75.2	% of lot area

BUILDING AREA/LOT

COVERAGE

Existing Building Footprint	5,561	sq. ft.
Proposed Building Footprint	6,371	sq. ft.
Proposed Building Footprint Net change	810	sq. ft.
Existing Total Building Floor Area	5,561	sq. ft.
Proposed Total Building Floor Area	6,371	sq. ft.
Proposed Building Floor Area Net Change	810	sq. ft.
New Building	Yes	(yes or no)
Building Area/Lot coverage existing	12.1	% of lot area
Building Area/Lot coverage proposed	13.8	% of lot area

ZONING

Existing _____
Proposed, if applicable _____
Highway Business (HB)

LAND USE

Existing _____ Restaurant
Proposed _____ Restaurant

RESIDENTIAL, IF APPLICABLE

Existing Number of Residential Units _____ NA
Proposed Number of Residential Units _____
Subdivision, Proposed Number of Lots _____

PARKING SPACES

Existing Number of Parking Spaces _____ 68
Proposed Number of Parking Spaces _____ 58
Required Number of Parking Spaces _____ 60
Number of Handicapped Parking Spaces _____ 3

ESTIMATED COST OF PROJECT

_____ \$1,000,000

DELEGATED REVIEW AUTHORITY CHECKLIST

SITE LOCATION OF DEVELOPMENT AND STORMWATER MANAGEMENT

Existing Impervious Area _____ 37,273 sq. ft.
Proposed Disturbed Area _____ 4,200 sq. ft.
Proposed Impervious Area _____ 34,665 sq. ft.

1. *If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with MDEP.*
2. *If the proposed impervious area is greater than one acre including any impervious area created since 11/16/05, then the applicant shall apply for a MDEP Stormwater Management Permit, Chapter 500, with the City.*
3. *If total impervious area (including structures, pavement, etc) is greater than 3 acres since 1971 but less than 7 acres, then the applicant shall apply for a Site Location of Development Permit with the City. If more than 7 acres then the application shall be made to MDEP unless determined otherwise.*
4. *If the development is a subdivision of more than 20 acres but less than 100 acres then the applicant shall apply for a Site Location of Development Permit with the City. If more than 100 acres then the application shall be made to MDEP unless determined otherwise.*

TRAFFIC ESTIMATE

Total traffic estimated in the peak hour-existing _____ 106 _____ passenger car equivalents (PCE)
(Since July 1, 1997) _____ (Calculated at 0.88 trips per seat)

Total traffic estimated in the peak hour-proposed (Since July 1, 1997) _____ 158 _____ passenger car equivalents (PCE)
If the proposed increase in traffic exceeds 100 one-way trips in the peak hour then a traffic movement permit will be required.

Zoning Summary

1. Property is located in the _____ zoning district.
2. Parcel Area: _____ acres / _____ square feet(sf).

Regulations	<u>Required/Allowed</u>	<u>Provided</u>
Min Lot Area	None	/ 46,100± sf
Street Frontage	150 ft.	/ 176± ft.
Min Front Yard	15 ft.	/ 15 ft.
Min Rear Yard	10 ft.	/ 0 ft. (existing)
Min Side Yard	10 ft.	/ 0 ft. (existing)
Max. Building Height	65 ft.	/ 24 ft.
Use Designation	/	
Parking Requirement	1 space/ per 3	space/foot of floor area Seats
Total Parking:	60	/ 58
Overlay zoning districts (if any):	/	
Urban impaired stream watershed?	YES/ XX If yes, watershed name <u>Jepson Brook</u>	

DEVELOPMENT REVIEW APPLICATION SUBMISSION

Submission shall include payment of fee and fifteen (15) complete packets containing the following materials:

1. Full size plans containing the information found in the attached sample plan checklist.
2. Application form that is completed and signed.
3. Cover letter stating the nature of the project.
4. All written submittals including evidence of right, title and interest.
5. Copy of the checklist completed for the proposal listing the material contained in the submitted application.

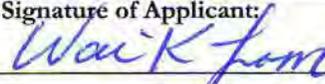
Refer to the application checklist for a detailed list of submittal requirements.

L/A's development review process and requirements have been made similar for convenience and to encourage development. Each City's ordinances are available online at their prospective websites:

Auburn: www.auburnmaine.org under City Departments/ Planning and Permitting/Land Use Division/Zoning Ordinance
Lewiston: <http://www.ci.lewiston.me.us/clerk/ordinances.htm>. Refer to Appendix A of the Code of Ordinances

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, I certify that the City's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for development review only; a Performance Guarantee, Inspection Fee, Building Permit Application and other associated fees and permits will be required prior to construction.

Signature of Applicant: 	Date: 
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Development Review Checklist
 City of Auburn Planning and Permitting Department
 City of Lewiston Department of Planning and Code
 Enforcement



**THE FOLLOWING INFORMATION IS REQUIRED WHERE APPLICABLE TO BE
 SUBMITTED FOR AN APPLICATION TO BE COMPLETE**

PROJECT NAME: Restaurant 750 Sabattus Street

PROPOSED DEVELOPMENT ADDRESS and PARCEL #: RE00012848

Required Information		Check Submitted		Applicable Ordinance	
		Applicant	Staff	Lewiston	Auburn
Site Plan					
	Owner's Names/Address	X			
	Names of Development	X			
	Professionally Prepared Plan	X			
	Tax Map or Street/Parcel Number	X			
	Zoning of Property	X			
	Distance to Property Lines	X			
	Boundaries of Abutting land	X			
	Show Setbacks, Yards and Buffers	X			
	Airport Area of Influence (Auburn only)	NA			
	Parking Space Calcs	X			
	Drive Openings/Locations	X			
	Subdivision Restrictions	NA			
	Proposed Use	X			
	PB/BOA/Other Restrictions	NA			
	Fire Department Review				
	Open Space/Lot Coverage	X			
	Lot Layout (Lewiston only)				
	Existing Building (s)	X			
	Existing Streets, etc.	X			
	Existing Driveways, etc.	X			
	Proposed Building(s)	X			
	Proposed Driveways	X			
Landscape Plan					
	Greenspace Requirements				
	Setbacks to Parking	X			
	Buffer Requirements				
	Street Tree Requirements	X			
	Screened Dumpsters	X			
	Additional Design Guidelines	NA			

	Planting Schedule				
Stormwater & Erosion Control Plan					
	Compliance w/ chapter 500	X			
	Show Existing Surface Drainage	X			
	Direction of Flow				
	Location of Catch Basins, etc.	X			
	Drainage Calculations	NA			
	Erosion Control Measures	NA			
	Maine Construction General Permit	NA			
	Bonding and Inspection Fees				
	Post-Construction Stormwater Plan	NA			
	Inspection/monitoring requirements	NA			
	Third Party Inspections (Lewiston only)	NA			
Lighting Plan					
	Full cut-off fixtures	Existing			
	Meets Parking Lot Requirements				
Traffic Information					
	Access Management				
	Signage	X			
	PCE - Trips in Peak Hour	X			
	Vehicular Movements	X			
	Safety Concerns				
	Pedestrian Circulation	X			
	Police Traffic				
	Engineering Traffic				
Utility Plan					
	Water	X			
	Adequacy of Water Supply				
	Water main extension agreement	NA			
	Sewer	X			
	Available city capacity				
	Electric	X			
	Natural Gas	X			
	Cable/Phone	X			
Natural Resources					
	Shoreland Zone	NA			
	Flood Plain	NA			
	Wetlands or Streams	NA			
	Urban Impaired Stream	X			
	Phosphorus Check	NA			
	Aquifer/Groundwater Protection	NA			
	Applicable State Permits	NA			
	No Name Pond Watershed (Lewiston only)	NA			

	Lake Auburn Watershed (Auburn only)				
	Taylor Pond Watershed (Auburn only)				
Right Title or Interest		X			
	Verify				
	Document Existing Easements, Covenants, etc.	NA			
Technical & Financial Capacity					
	Cost Est./Financial Capacity				
	Performance Guarantee				
State Subdivision Law		NA			
	Verify/Check				
	Covenants/Deed Restrictions				
	Offers of Conveyance to City				
	Association Documents				
	Location of Proposed Streets & Sidewalks				
	Proposed Lot Lines, etc.				
	Data to Determine Lots, etc.				
	Subdivision Lots/Blocks				
	Specified Dedication of Land				
Additional Subdivision Standards		NA			
	Single-Family Cluster (Lewiston only)				
	Multi-Unit Residential Development (Lewiston only)				
	Mobile Home Parks				
	Private Commercial or Industrial Subdivisions (Lewiston only)				
	PUD (Auburn only)				
A jpeg or pdf of the proposed site plan		X			
Final sets of the approved plans shall be submitted digitally to the City, on a CD or DVD, in AutoCAD format R 14 or greater, along with PDF images of the plans for archiving					

**QUIT-CLAIM DEED
RELEASE DEED**

Yan Xian Xie, also known as Yan X. Lam, of Skowhegan, Somerset County, Maine, for consideration paid, release to **Chelsea Market Realty, L.L.C.**, a limited liability corporation having an office in Skowhegan, Somerset County, Maine, the land in Lewiston, Androscoggin County and State of Maine, bounded and described as follows, to-wit:

PARCEL ONE

Real estate located at the southeasterly side of Garcelon Street in Lewiston, Androscoggin County, Maine, and more particularly described in a deed from Janice L. Sites to Wai K. Lam dated June 19, 2003 and recorded in the Androscoggin County Registry of Deeds, Book 5477, Page 247. See also Abstract of Divorce Decree between Wai K. Lam and Yan X. Xie dated May 5, 2009 and recorded in Somerset County Registry of Deeds in Book 5155, Page 4131.

PARCEL TWO

Real estate located at Thorn's Corner Park, in Lewiston, Androscoggin County, Maine, more particularly described in a deed from The Lam Family Real Estate Partnership to Wai Kwong Lam and Yan Xian Xie dated December 31, 2002 and recorded in the Androscoggin County Registry of Deeds, Book 5247, Page 45. See also Abstract of Divorce Decree between Wai K. Lam and Yan X. Xie dated May 5, 2009 and recorded in Somerset County Registry of Deeds in Book 5155, Page 4131.

WITNESS my hand this 18th day of March, 2010.

Ronald E. Chapman
Witness

Yan Xian Xie
Yan Xian Xie

**State of Maine
Kennebec, ss**

March 18, 2010

Then personally appeared the above named **Yan Xian Xie** and acknowledged the foregoing instrument to be her free act and deed.

Before me,

Robert J. Ringer, Jr.
Robert J. Ringer, Jr.
Notary Public

ANDROSCOGGIN COUNTY
TINA M. CHOUINARD
REGISTER OF DEEDS

NO MAINE R.E. TRANSFER TAX PAID
JABAR, BAYTEN, RINGER & LAIBERRY - ATTORNEYS AT LAW - ONE CENTER STREET - WATERVILLE, MAINE 04901



CITY OF LEWISTON

Department of Planning & Code Enforcement



TO: Planning Board
FROM: David Hediger, City Planner
DATE: July 24, 2014
RE: July 28, 2014 Planning Board Agenda Item IV(b)

An application submitted by AnnBeha Architects on behalf of Bates College for the construction of two new residence halls with a campus store and mail facility at 55 & 65 Campus Avenue.

AnnBeha Architects has submitted an application on behalf of Bates College for the construction of two new residence halls with a campus store and mail facility at 55 & 65 Campus Avenue. The development will consist of approximately 3.3 acres in the Institutional Office (IO) district in which student housing and academic uses are allowed as a permitted use.

The two 4-story residence halls will provide 250-beds, the campus store and mail/print facilities. These beds are replacement beds for the college as a result of reducing density in other residential buildings and do not represent any increase in the student, faculty or staff populations on campus. 55 Campus Avenue is proposed to be approximately 40,626 gross square feet. 65 Campus Avenue is proposed to approximately 43,013 gross square feet, of which the first floor and basement will have retail space including the relocated Campus Store and Office Services mail, package and print/copy services. Ten existing structures owned by Bates College on Campus Avenue, Bardwell Street, Franklin Street, and Central Avenue are being demolished to support this development.

Staff has been working closely with the applicant's representative to address concerns and questions. The applicant has since provided revised plans and documentation referencing most of staff comments (see application Article 4, Exhibit A). However, staff notes the following with respect to the proposed development:

- The applicant has referenced all of the applicable sections of the Zoning and Land Use Code including Article XIII, Section 4, which may be found in Article 3 of the application.
- The development will result in approximately 1.9 acres of impervious area. The applicant has submitted a performance guarantee for post construction stormwater management pursuant to Article XIII, Section 15e(3). Staff recommends as a condition of approval that prior to a certificate of occupancy being issued that evidence of said documentation recorded in the Androscoggin Registry of Deeds is provided regarding the maintenance and upkeep of the stormwater system pursuant to Lewiston MS4 stormwater requirements.
- The stormwater design for this project is very complicated with numerous improvements to Bates' and City infrastructure. The applicant and Public Works are working together to address staff's comments with respect to stormwater. An update will be provided at the

meeting. If not resolved by the time of the meeting, staff is recommending as a condition of approval that no building permits be issued or site activity commence until the stormwater design has been reviewed to staff's satisfaction.

- The applicant has noted on C1.0 that no certificate of occupancy will be issued for this development until written verification by a professional engineer is provided to the city that all stormwater improvements have been completed in accordance with the approved plan.
- In effort to heighten pedestrian safety crossing Campus Avenue, new crosswalks and traffic neckdowns are being proposed at the intersection of Campus and Franklin and Campus and Bardwell. Bates will be responsible for these improvements, as well as the initial crosswalk striping and pedestrian signs.
- Sidewalks will be reconstructed along Central and Campus providing an esplanade for tree planting and addition snow storage. City staff supports these improvements; however, the City will need an easement from Bates for these sidewalks as they will be located on Bates property outside the right-of-way. The responsibility and maintenance of these sidewalks is being discussed with Public Works. Acceptance of these easements will require City Council approval. Therefore, staff recommends this be a condition of approval. A new sidewalk on Bardwell will be located within the City right-of-way for which no easement is needed.
- Bates is proposing to close approximately 127' of Franklin Street from Campus Ave, with the right-of-way remaining open only to pedestrians and emergency vehicles. Closing this section results in Franklin Street becoming a dead end. Bates is looking to improve the closed portion by narrowing the travel way, installing concrete pavers, and landscaping. The city currently has utilities within this portion of Franklin. Bates is requesting easement from the City to close this section of the street and make these improvements. The responsibility and maintenance of this closed section would be Bates College. Related to this, Bates is proposing to provide the City an easement within their Franklin Street parking lot to allow plow trucks and other vehicles to turnaround. Article 4, Exhibit C of the application highlights the easements being requested. The applicant is in discussions with Public Works about the details of this proposal and the necessary easements. Acceptance of these easements will require City Council approval. Therefore, staff recommends this be a condition of approval.
- The property of Selma Nelson at 101 Franklin Street is not owned by Bates College and is being surrounded by the proposed improvements. Remaining a residence, frontage on a City accepted street must be maintained. Therefore, the portion of Franklin Street in front of this property will remain a city accepted street maintained by the City.
- Bates has secured grading easement from the property owner at 101 Franklin Street to place fill and grade the property in part to conform to the construction activity proposed on Bates property (Article 4, Exhibit E). Bates is also providing extensive landscaping around the perimeter of Ms. Nelson's property (sheet L 1.2).
- As the Board will recall from their June 9, 2014 discussion, Bates was instructed to come up a plan with respect to parking on and around campus. This concern was shared by the City Council at their June 10th meeting. At this time, Bates proposal consists of the following:
 - The proposed project will not generate additional parking demand campus wide since the purpose of the project is to replace existing student housing at other locations on campus. However, to avoid increase pressure on parking in the

neighborhood due to the redistribution of student housing, the overall parking inventory in this quadrant will increase a total of 10 spaces: 72 off-street spaces and 62 on-street (Article 4, Exhibit D).

- Plans and details are being updated to include signage and striping on Campus Avenue and Central Avenue. At this time, it's not clear what striping is being proposed for bike/ped purposes. Bates will be responsible for the signs and striping in coordination with the city. Striping and approval to eliminate and post no parking signs in select areas along city streets will need City Council approval. Therefore, staff recommends this be a condition of approval.
- Bates recognizes that there may be issues that arise following the completion of the project which will need to be addressed. To facilitate this, Bates is forming a transportation committee which will be in place by the fall 2014 semester. This committee will be charged with promoting and monitoring initiatives that minimize the number of cars on campus and therefore demand for new spaces; to monitor the parking and transportation issues in and around this project; and to identify more sustainable transportation practices for official college business. The committee will include faculty and staff and students and a representative from the City. The committee will hold meetings with the neighborhood on an annual basis to gain feedback on what is working and what can be improved. The committee will also continue discussions with city staff on a regular basis to monitor progress. Staff applauds this effort and believes this will assist Bates, the City, and abutting neighborhoods in addressing parking related concerns.
- Parking policy changes to provide clarity and to promote increased usage of off street student parking will be implemented by the beginning of the 2014 fall semester. These policies will include:
 - 1) Requiring all students that are issued parking permits to park in College lots only or risk forfeiture of parking privileges. The College will also work with the City and neighbors to identify which streets are not appropriate for faculty or staff parking. This will be a topic for the transportation committee to discuss.
 - 2) Any student bringing a vehicle to Bates College regardless of whether or not they have obtained a Bates College Parking Permit must register with security for the fall 2014 semester. This will help maintain better relations with neighbors and allow Bates to contact owners of vehicles when warranted.
 - 3) Starting with the fall semester of 2014, the Bates parking office will work with the Lewiston Police Department to identify students who park on street in violation of the policy discussed above who are not tenants of off campus housing.
 - 4) Update the parking section of the Campus Security Webpage to consolidate Parking Policy and Information. This is an ongoing effort which should be in place for the fall 2014 semester.
 - 5) Creation of a dedicated parking map that is easily accessible on Bates.edu. The map will be compatible with smartphone and tablet devices. This is now in place and will be updated prior to the start of the fall semester to reflect realigned parking restrictions in on-campus parking lots.

- 6) Prior to the start of the fall 2014 semester student parking will be restricted in the Smith, Olin and Bardwell lots where it is currently allowed.
 - The transportation assessment in Article 4, Exhibit h of the application indicates that on-site parking on the campus is proposed to be restricted to faculty and staff (p.2 section 8(b)). It is not clear what “restricted” student parking means or where faculty and staff will be directed to park. Staff recommends as a condition that additional information and evidence is provided clearly showing staff, staff, and faculty parking lots.
 - Bates will be completing a report on the parking conditions in the vicinity of this project which will be submitted to City staff within 12 months of the opening of the project. Staff recommends this be a condition of approval tied to the date of issuance of the certificate of occupancy and that the applicant will submit and report their findings City staff and the Planning Board.
 - This project is subject to a DEP site law permit being considered as part of the campus as a whole, exceeding the City’s delegated review authority. Therefore, prior to any building permits being issued, staff recommends as a condition of approval the approval from DEP is provided to the City.

Approval is recommended pursuant to Article XIII, Section 4 Zoning and Land Use Code with the following conditions:

1. Prior to a certificate of occupancy being issued, documentation regarding the maintenance and upkeep of the stormwater system pursuant to Lewiston MS4 stormwater requirement contained in Article XII, Section 15e(3) must be recorded in the Androscoggin Registry of Deeds.
2. If not resolved by the time of the meeting, no building permits will be issued and no site activity shall commence until the stormwater design has been reviewed to staff’s satisfaction.
3. No certificate of occupancy shall be issued for this development until written verification by a professional engineer is provided to the city that all stormwater improvements have been completed in accordance with the approved plan.
4. No building permits will be issued and no site activity shall commence until the City and Bates secure easements for utilities, sidewalks, and street closings.
5. No building permits will be issued and no site activity shall commence until the City Council approves of the proposed bike/pedestrian striping and approval to eliminate on street parking and post no parking signs in select areas along city streets.
6. Within 12 months of the date of issuance of the certificate of occupancy, Bates College will submit and report their findings on the parking conditions in the vicinity of this project to City staff and the Planning Board.
7. Bates must demonstrate to the City staff by October 31, 2014 their recommended parking policy changes have been implemented, including:
 - a) Requiring all students that are issued parking permits to park in College lots only or risk forfeiture of parking privileges.
 - b) Any student bringing a vehicle to Bates College regardless of whether or not they have obtained a Bates College Parking Permit must register with security for the fall 2014 semester.

- c) Work with the Lewiston Police Department to identify students who park on street in violation of the policy discussed above who are not tenants of off campus housing.
 - d) Update the parking section of the Campus Security Webpage to consolidate Parking Policy and Information.
 - e) Creation of a dedicated parking map that is easily accessible on Bates.edu
 - f) Student parking will be restricted in the Smith, Olin and Bardwell lots where it is currently allowed.
 - g) Additional information and evidence is provided clearly showing student, staff, and faculty parking lots.
 - h) Evidence is provided that the transportation committee has been created.
8. If development has not occurred as defined within the scope of the Zoning and Land Use Code within two years, development review approval shall expire, pursuant to Article XIII, Section 11. This must be noted on the site plan.

ACTIONS NECESSARY

1. Make a motion to consider an application submitted by AnnBeha Architects on behalf of Bates College for the construction of two new residence halls with a campus store and mail facility at 55 & 65 Campus Avenue.
2. Obtain input on the application;
3. Make a determination that the application is complete;
4. Make finding that the application meets all of the necessary criteria contained in the Zoning and Land Use Code, including Article XIII, Section 4 of the Zoning and Land Use Code and to grant approval to Bates College for the construction of two 4-story residence halls that will provide 250-beds, the campus store and mail/print facilities at 55 and 65 Campus Avenue (subject to any concerns raised by the Planning Board or staff).



POLICE DEPARTMENT

Sgt. David K. Chick
Inspector of Police



REVIEW OF REPORTED COMPLAINTS CONCERNING PARKING (BATES COLLEGE NEIGHBORHOOD)

Period of review was (June 25, 2013 – June 25, 2014)

Synopsis: There were NO REPORTED INCIDENCES of parking violations occurring on Benson St, Abbott St, Bardwell St, or Franklin St during the period of review. Complaint received on Ware St, Mountain Ave, and Wood St were only concerning snow parking ban issues. White St and Davis St complaints were of narrowed streets due to parking congestion (not all citable offenses). There were only (2) reported incidences of student owned vehicles blocking non-Bates premise driveways; these were on Frye St (block between Oak and Main) and on Nichols St, occurring several blocks away from the campus.

The single greatest issue of complaint pertains to parking so as to infringe upon the corner at the intersection of Frye/Oak. A significant number of the complaints which were not in any way related to snow parking bans occurred within the first month of the student school year.

Ware St

20140305 LA911 14-18082: 3 vehicles towed in conjunction with snow parking ban

Mountain Ave

20140216 LA911 14-12915: 2 vehicles towed in conjunction with snow parking ban

Benson St

Abbott St

Frye St

20130808 LA911 13-65287: Vehicle blocking driveway (*Bates origination* & Bates student)

20130905 LA911 13-74129: Too near corner of Oak St; ticketed parking violation

20130909 LA911 13-75264: Too near corner of Oak St; parking enforcement action

20130913 LA911 13-76495: Vehicle blocking driveway (*Bates origination* & Bates student)

20130918 LA911 13-77895: Student vehicle infringing on non-Bates driveway; ticketed

20130929 LA911 13-80980: Request locate owner move vehicle for construction access

20131022 LA911 13-87369: Too near corner of Oak St w/construction; *no citable violation*



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POLICE DEPARTMENT

Sgt. David K. Chick
Inspector of Police



20131025 LA911 13-88338: Too near corner of Oak St; parking enforcement action

20140115 LA911 14-4223: Too near corner of Oak St; parking enforcement action

20140131 LA911 14-8561: Too near corner of Oak St; no citable violation

20140205 LA911 14-10195: Too near corner of Oak St; ticketed parking violation

20140205 LA911 14-10198: Too near corner of Oak St; ticketed parking violation (duplicated)

20140205 LA911 14-10199: Vehicle towed in conjunction with snow parking ban

20140205 LA911 14-10213: Vehicle towed in conjunction with snow parking ban

20140219 LA911 14-13835: Vehicle moved in conjunction with snow activity

20140310 LA911 14-19454: 4 vehicles impeding snow removal; owners contacted to move

White St

20131230 LA911 13-106127: Vehicle towed in conjunction with snow parking ban

20140115 LA911 14-4209: Vehicles parking congestion; no citable violation

20140228 LA911 14-16696: Vehicles parking congestion; no citable violation

Davis St

20140217 LA911 14-13244: 2 vehicles parking wrong side of street; ticketed parking violations

Wood St

20140205 LA911 14-10151: Vehicles moved/towed in conjunction with snow parking ban

20140205 LA911 14-10181: Vehicle towed in conjunction with snow parking ban

20140219 LA911 14-13836: Vehicle moved in conjunction with snow activity

Nichols St

20130921 LA911 13-78663: Student vehicle infringing on non-Bates driveway; ticketed/moved

Bardwell St

Franklin St

Sgt. David Chick



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CITY OF LEWISTON
DEVELOPMENT REVIEW APPLICATION
FOR
BATES COLLEGE
55 & 65 CAMPUS AVENUE HOUSING



LEWISTON, MAINE

JULY 2014

ARTICLE 0
SITE DEVELOPMENT REVIEW

21 July 2014

Mr. David Hediger, City Planner
City of Lewiston
27 Pine Street
Lewiston, Maine 04240

Re: **Bates College Campus Housing Project – Development Review**

Dear Mr. Hediger,

Bates College proposes to build two student dormitory buildings on 3.3 acres of land located south of Campus Avenue between Bardwell Street and Central Avenue. The project is for a ~ 250-bed mixed use residential community, which is part of an overall campus upgrade of all residential facilities, the campus store and mail/print facilities. These beds are replacement beds for the college as a result of decanting other residential buildings, and do not represent any increase in the student, faculty or staff populations on campus.

55 Campus Avenue is proposed to be 4-stories above grade and approximately 40,626 gross square feet. The building will have student sleeping rooms, and shared spaces including a student kitchen, game room, lounges, study rooms and living rooms.

65 Campus Avenue is proposed to be 4-stories above grade with a basement and approximately 43,013 gross square feet. The first floor and basement will have retail space including the relocated Campus Store and Office Services mail, package and print/copy services. Floors 2 through 4 will have student sleeping rooms, and shared spaces including a student kitchen, game room, lounges, study rooms and living rooms.

The College's master plan envisions an academic core with residential communities at the periphery, and this new development will link to the core campus via a new pedestrian crossing at Franklin Street. Proposed site improvements include the closing and narrowing of the northern portion of Franklin Street to normal vehicular traffic, traffic-calming crosswalks across Campus Avenue at Bardwell Street and Franklin Street, as well as the creation of green space and parking.

The following materials are attached:

- 1- Development Review Application
- 2- Development Review Checklist
- 3- Approval Criteria Article XIII Sec. 4
- 4- Application Attachment: Additional Information
 - A- Responses to City Comments
 - B- Performance Guarantee
 - C- Easement Plan
 - D- Parking Diagram
 - E- "Agreement for the Purchase and Sale of Real Estate" as evidence of right, title

ARTICLE 1-2
SITE DEVELOPMENT REVIEW
CHECK LIST FORM



Development Review Application

City of Auburn Planning and Permitting Department
City of Lewiston Department of Planning and Code Enforcement



PROJECT NAME: Campus Avenue Residence Halls

PROPOSED DEVELOPMENT ADDRESS: 55 Campus Avenue, 65 Campus Avenue

PARCEL ID#: Map 173, Lots: 5, 9, 6, 7, 8, 10 / Map 194, Lots: 113, 114, 136, 137, 138, 139, 140, 167, 168

REVIEW TYPE: **Site Plan/Special Exception** **Site Plan Amendment**
 Subdivision **Subdivision Amendment**

PROJECT DESCRIPTION: Construction of two new residence halls with a campus store and mail facility.

CONTACT INFORMATION:

Applicant

Name: Bates College - Chris Streifel
Address: 147 Russell Street, Lewiston
Zip Code 04240
Work #: (207) 786-6211
Cell #:
Fax #:
Home #:
Email: cstreifel@bates.edu

Property Owner

Name: President & Trustees of Bates College
Address: 2 Andrews Road, Lewiston
Zip Code 04240
Work #: (207) 786-6211
Cell #:
Fax #:
Home #:
Email:

Project Representative

Name: AnnBeha Architects, Philip Chen
Address: 33 Kingston Street, Boston, MA
Zip Code 02111
Work #: (617) 226-1638
Cell #:
Fax #:
Home #:
Email: pchen@annbeha.com

Other professional representatives for the project (surveyors, engineers, etc.),

Name: Wright-Pierce, Jan Wiegman, PE
Address: 99 Main Street, Topsham, ME
Zip Code 04086
Work #: (207) 725-8721
Cell #:
Fax #:
Home #:
Email: jan.wiegman@wright-pierce.com

Zoning Summary

1. Property is located in the Institutional Office zoning district.
2. Parcel Area: 3.3 acres / 145,479 square feet(sf).

Regulations	Required/Allowed	Provided
Min Lot Area	<u>10,000 SF</u>	<u>> 70,000 each site</u>
Street Frontage	<u>100 FT</u>	<u>> 100 FT</u>
Min Front Yard	<u>10 FT</u>	<u>10 FT</u>
Min Rear Yard	<u>10 FT</u>	<u>>10 FT</u>
Min Side Yard	<u>10 FT</u>	<u>> 10 FT</u>
Max. Building Height	<u>75 FT</u>	<u>55 FT</u>
Use Designation	<u>Institutional</u>	<u>Institutional</u>
Parking Requirement	<u>ONE PARKING SPACE PER 7 SEATS IN CLASSROOM</u> City of Lewiston Zoning and Land Use Code, Appendix A, Article XII	
Total Parking:	<u>MAINTAIN EXISTING: 50 OFF-STREET+70-ON STREET= 120</u> / <u>PROVIDED 134 (72 OFF-STREET+62-ON STREET)</u>	
Overlay zoning districts (if any):	<u>N/A</u>	<u>/</u>
Urban impaired stream watershed?	<u>YES/NO If yes, watershed name</u> <u>NO</u>	

DEVELOPMENT REVIEW APPLICATION SUBMISSION

Submission shall include payment of fee and fifteen (15) complete packets containing the following materials:

1. Full size plans containing the information found in the attached sample plan checklist.
2. Application form that is completed and signed.
3. Cover letter stating the nature of the project.
4. All written submittals including evidence of right, title and interest.
5. Copy of the checklist completed for the proposal listing the material contained in the submitted application.

Refer to the application checklist for a detailed list of submittal requirements.

L/A's development review process and requirements have been made similar for convenience and to encourage development. Each City's ordinances are available online at their prospective websites:

Auburn: www.auburnmaine.org under City Departments/ Planning and Permitting/Land Use Division/Zoning Ordinance

Lewiston: <http://www.ci.lewiston.me.us/clerk/ordinances.htm>. Refer to Appendix A of the Code of Ordinances

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, I certify that the City's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for development review only; a Performance Guarantee, Inspection Fee, Building Permit Application and other associated fees and permits will be required prior to construction.

Signature of App: 

Date:

7/22/2014

	Planting Schedule	L1.0			
Stormwater & Erosion Control Plan					
	Compliance w/ chapter 500	Attachment			
	Show Existing Surface Drainage	C4.0			
	Direction of Flow	C4.1			
	Location of Catch Basins, etc.	C4.1			
	Drainage Calculations	Attachment			
	Erosion Control Measures	C2.3			
	Maine Construction General Permit	Applied For			
	Bonding and Inspection Fees	Attachment			
	Post-Construction Stormwater Plan	C4.1			
	Inspection/monitoring requirements	I, M & H Plan			
	Third Party Inspections (Lewiston only)	C1.0			
Lighting Plan					
	Full cut-off fixtures	L1.2			
	Meets Parking Lot Requirements	L1.2			
Traffic Information					
	Access Management	Attachment			
	Signage	C2.0			
	PCE - Trips in Peak Hour	Attachment			
	Vehicular Movements	Attachment			
	Safety Concerns	Attachment			
	Pedestrian Circulation	Attachment			
	Police Traffic	Attachment			
	Engineering Traffic	Attachment			
Utility Plan					
	Water	C2.2			
	Adequacy of Water Supply	PENDING			
	Water main extension agreement	N/A			
	Sewer	C2.2			
	Available city capacity	PENDING			
	Electric	C2.2			
	Natural Gas	C2.2			
	Cable/Phone	C2.2			
Natural Resources					
	Shoreland Zone	N/A			
	Flood Plain	Attachment			
	Wetlands or Streams	C1.2			
	Urban Impaired Stream	N/A			
	Phosphorus Check	N/A			
	Aquifer/Groundwater Protection	Attachment			
	Applicable State Permits	Pending			
	No Name Pond Watershed (Lewiston only)	N/A			

ARTICLE 3
ARTICLE 13, SECTION 4
DEVELOPMENT REVIEW
APPROVAL CRITERIA

21 July 2014

Mr. David Hediger, City Planner
City of Lewiston
27 Pine Street
Lewiston, Maine 04240

Re: **Bates College Campus Housing Project – Article 13, Section 4 Development Review
Approval Criteria**

Dear Mr. Hediger,

The following are acknowledgements of compliance with the City of Lewiston Approval Criteria under Article 13, Section 4:

Approval Criteria: Article 13, Section 4

(a) *Utilization of the site.*

The site has been laid out to take into account the Bates Campus and the uses proposed for the buildings. In addition the green spaces were developed to create a larger open area between the buildings to host outdoor activities. The proposed building area is similar to the existing structure density on the site. There is somewhat more paved surfaces to accommodate the increased pedestrian activity and parking accommodations. The building and impervious lot coverage of the sites are well below the maximums for the district.

The proposed project will construct two new buildings for student housing and a new campus store and mail facility. The project will primarily serve to modernize and update the current housing stock at the campus. The project site is part of the Bates College campus and is currently occupied by a number of houses that are being used for a variety of administrative and student housing purposes. The site is separated by two public streets, Franklin and Bardwell Streets and bordered by Campus Avenue and Central Avenue on two sides. Opposite Campus Avenue from the site are several academic and student activities buildings as well as the main body of the campus.

The existing site is currently developed and is part of an older residential neighborhood with 1/8 to 1/2 acre lots. Many of the properties are owned by Bates and the houses are used for a variety of functions related to the college. The site includes an existing parking lot that will be reconfigured to accommodate the building program.

The proposed building at 55 Campus Avenue will be a four story student housing building with a footprint of approximately 11,542 s.f. The building at 65 Campus Avenue will have the campus store and mail facility on the first floor, and three stories of student housing with a footprint of 12,280 s.f. This building will also have a partial basement level that will house the student services copy/print center, communications infrastructure for several campus buildings, and heating infrastructure for the two new buildings. An existing parking lot will be reconfigured and a loading

area is proposed for the campus store and mail/package facility to accommodate deliveries and shipments.

(b) *Traffic movement into and out of the development area.*

A Transportation and Parking Assessment study was prepared for the project by Gorrill-Palmer Consulting Engineers, Inc. Refer to Exhibit H attachment for the report. The report determined that there would be no significant impacts from the project. The project is relocating existing functions from several locations on campus and will not increase enrollment. Vehicular and pedestrian trips are not anticipated to increase and will reflect a redistribution of internal trips. To enhance safety for pedestrians the college is requesting the City approve partial discontinuance of Franklin Street and curb extensions at all four quadrants of the intersections of Campus Ave at both Bardwell and Franklin Streets. The college will install all necessary improvements including sidewalks, striping, signs, lighting, and accessibility ramps to improve safety as part of the project. The college will work with city staff to develop easement language regarding the College's access to maintain hardscape, softscape and other improvements within the Franklin Street ROW.

The proposed project will not generate additional parking demand campus wide since the purpose of the project is to replace existing student housing at other locations on campus. However, to avoid increase pressure on parking in the neighborhood due to the redistribution of student housing, the overall parking inventory in this quadrant will increase as a result of the project. The College also proposes development and implementation of a comprehensive parking management plan overseen by a newly formed college Transportation Committee.

(c) *Access into the site.*

Vehicular access into the parking lot of 55 Campus Avenue can be made from both Bardwell Street and Franklin Street. The northern end of Franklin Street is proposed to be closed to normal vehicular traffic in the interest of pedestrian safety. Access to the 65 Campus Avenue parking lot can be made from Central Avenue. On-street parking within close proximity of the site is available on Campus Avenue, Bardwell Street, Franklin Street, and Central Avenue. Grades, intersections, access and sight distances shall be in accordance with the City of Lewiston's Policy for the Design and Construction of Streets and Sidewalks.

(d) *Internal vehicular circulation.*

The layout of the site will provide for the safe movement of passenger, service and emergency vehicles through the site. At the 65 Campus Ave loading dock, the project will provide a clear route for delivery vehicles with appropriate geometric design to allow turning and backing for WB-40 vehicles. Clear routes of access will be provided and maintained for emergency vehicles to all portions of the site including the portion of Franklin Street closed to normal vehicular traffic, and will be posted with appropriate signage. The layout and design of the parking areas including the 55 Campus Avenue parking lot and the Central Avenue parking lot will provide for safe and convenient circulation of vehicles throughout the lot and will prohibit vehicles from backing out onto a street.

(e) *Pedestrian circulation.*

This new development of buildings is on the southern edge of the campus, so a majority of pedestrian circulation will flow south across Campus Ave. At the intersection of Campus Ave and Franklin Street pedestrians will use a new crosswalk and traffic neckdown (a second proposed crossing is at the Campus Ave and Bardwell intersection). Once on the southern side of Campus Ave, pedestrians have a choice of continuing south on the new pedestrian oriented Franklin Walk until it reverts to the original Franklin Street. Pedestrians can also angle left or right to the entry terraces of the two buildings. In this same area the sidewalks on the south side of Campus Ave are set back from the street curb by six feet to allow room for street trees and snow storage. This proposed sidewalk condition also exists along Central Ave. New walkways connect all building entrances and there is a strong east-west walk that connects secondary building entrances at 55 and 65 Campus Ave. There are new accessible routes connecting accessible parking spaces with building entrances.

Bituminous concrete sidewalks will be installed as part of the project along the south side of Campus Avenue, east side of Bardwell Street, both sides of Franklin Street, and the West side of Central Avenue. The northern end of Franklin Street is proposed to be closed to normal vehicular traffic, and will be open only to pedestrians and emergency vehicles. This portion of Franklin Street will be narrowed and paved with pavers. Pedestrian plazas with pavers will border the main entrance sides of the buildings and connect to the sidewalks and open green spaces proposed as part of the project. Walkways will also connect to the proposed parking lots. Sidewalks shall meet the standards identified in the City of Lewiston's Policy for the Design and Construction of Streets and Sidewalks.

(f) *Stormwater management.*

The proposed development will remove the site runoff from the City's combined sewer and direct the flow to the separated sewer system. The stormwater management system for the proposed development captures and treats in excess of 95% of the runoff from the impervious surfaces and more than 80 % of the runoff from the entire site and treats the runoff. The storm water is treated by utilizing underdrained soil filters, roof line drip strips , bio retention areas, rain gardens and underdrained pervious pavement. The calculations for the 2, 10 and 25 year storm events have been prepared and results in minor overall increases in the rate of runoff from the site.

(g) *Erosion control.*

An Erosion and sedimentation control plan has been developed for the site that addresses both the construction and post construction stabilization of the site. In addition a maintenance plan for the site has been prepared that addresses the regular maintenance of the site and the stormwater management practices of the project.

(h) *Water supply.*

The project will utilize the public water supply and the City has indicated that there is adequate capacity in the system to support the project.

(i) *Sewage disposal.*

The project will utilize the public sewer and the City has indicated that there is adequate capacity in the system to support the project.

(j) *Utilities*

The project will utilize other public utilities including electricity, cable TV and natural gas and has coordinated the project service with each of these utilities. In addition several campus wide communication networks will be extended to the project to provide for telephone and internet services.

(k) *Natural features.*

The site generally is a redevelopment of an urban site. To the extent practical the applicant has avoided altering existing natural features in the project vicinity.

(l) *Groundwater protection.*

The project will not impact the groundwater. The soils are not conducive to significant infiltration and the stormwater treatment systems will be lined.

(m) *Water and air pollution.*

The project will not produce a negative impact to water and air pollution.

(n) *Exterior Lighting.*

The exterior of the building, site and crosswalks at Campus Avenue will be illuminated with full-cut off fixtures compliant with IEC illumination standards in order to provide for the safe use of the development in nighttime hours. Lighting will be provided at building entrances and exits, street intersections, pedestrian crossings, entrance roads, walkways and open space.

(o) *Waste disposal.*

Solid waste generated at the student housing will be collected in rooms within the buildings and picked up regularly by College staff and transported to a central collection facility for the campus. The College disposes solid waste from the campus at the Mid-Maine Waste Action Corporation in Auburn, Maine. The College estimates that the project will generate a total of 7.5 Tons per month. The project is not adding new students to the campus but will be filling an existing need to improve housing so the solid waste from the entire campus will not change with this project. See attached Exhibit J letter from the Bates Maintenance Department regarding solid waste.

The demolition of the existing structures will occur prior to this project and are being reviewed under a separate Minor Revision Application with the MDEP. The remaining demolition of foundations will occur as part of this application. The project will generate the following amounts for debris during the construction process:

<u>Material</u>	<u>Tons disposed / recycled</u>
Mixed Debris (C & D)	98
Clean Wood	24
Gypsum Board	86

Masonry / Concrete w/rebar	321
Masonry / Concrete w/o rebar	121
Cardboard	3
Metals	35

The project is primarily a facility expansion and the majority of the site work will be located within the existing developed area, therefore minimal clearing will be needed. In the areas required to be cleared, the work will be completed as noted in the construction notes. Trees and stumps will be disposed off-site by the contractor selected for the project.

Solid waste generated by the demolition of the buildings is estimated to be 150 tons. This material will be sorted on site and the materials will be disposed of at licensed facilities. Material will be hauled by a hauler to these facilities and included with this section is a letter from pine tree Waste indicating that they have the ability to haul the waste and to where they propose to haul the material.

There is no hazardous waste anticipated to be generated by this project.

(p) *Lot layout.* See attached civil engineering site plans for lot layout.

(q) *Landscaping.*

The proposed landscaping will comply with the guidelines contained in the City of Lewiston's Site Plan Review and Design Guidelines as amended. Several large existing trees located at the edges of the site will be protected and preserved. Hazard pruning, crown cleaning and structural bracing will be performed where necessary. In addition, proposed deciduous canopy trees will be planted along the street edges within the site boundary. Evergreen pine trees are introduced at the peripheries of the parking areas and loading dock at the south edges of the site. Lawn is the predominant groundcover which wraps the buildings on all sides. A large open lawn area is bounded by the two proposed buildings facing Campus Ave. The main entrances to both buildings are fronted by linear planters and seat walls. The planters include evergreen groundcover and deciduous trees. In addition, rain gardens that treat storm water are arranged in a linear east-west fashion parallel to the building wings. Rain garden plants include sedge and witchhazel.

(r) *Shoreland relationship.*

The project is not located within the Shoreland Zone.

(s) *Open Space.*

The project site has approximately 60,000 sf of open space that can be used by dormitory residents for a variety of active and passive uses. The open space is accessible by walkways connecting to both project buildings as well as by walkways connecting to Campus Avenue and Franklin Street. The dormitory portion of the development provides college dormitory sleeping units, not residential dwelling units, therefore the open space requirements for residential dwelling units are not applicable to this project.

(t) *Technical and financial capacity.*

Applicant will use only professional engineers, architects, and contractors to construct the project. Bates has engaged Ann Beha Architects of Boston, MA to design the proposed Student Housing project. Ann Beha Architects has engaged Wright-Pierce, 99 Main Street, Topsham, ME as its civil engineering consultant to prepare the site design and assist in site permitting. Ann Beha Architects has also engaged Richard Burke Associates of Somerville, MA as its landscape architect. Bates College has staff who oversees capital improvement projects for the College. Bates has undertaken other residence hall projects as well as other capital projects of similar scale to the current project. Bates staff involved in the project include Pamela Wichroski, AIA, Director Capital Planning and Construction and Chris Streifel, Project Manager. Jan Wiegman, P.E., is the technical representative of Wright-Pierce for the project. Wright-Pierce has extensive experience with site development and stormwater permitting projects.

The estimated cost of the proposed project, including buildings, parking lots, and site work is \$29 million. Bates College has on hand the funds to complete the work, which will commence upon receiving the required approvals from the City of Lewiston and the State of Maine. Attached Exhibit K is a statement from Bates College regarding the commitment and funding sources.

(u) *Buffering.*

The project is located in a heavily developed residential/institutional area. Adjacent properties have uses similar to those of the proposed project, and they are either other facilities owned by the College, individual/multifamily residential uses, and the Lewiston Middle School. The applicant proposes to buffer residential uses not in their ownership from the project in accordance with City of Lewiston Land Use Standards, including the installation of vegetation along the project boundaries. Landscaping will be installed along Central Avenue to buffer the buildings from the street and the Lewiston Middle School. The buffer areas will be improved and maintained in accordance with the standards set forth in the City of Lewiston's Site Plan Review and Design Guidelines.

(v) *Compliance with district regulations.*

College dormitories and campus store functions are permitted uses in the Institutional Office zoning district. The lot exceeds the minimum lot size of 10,000 square feet and the minimum frontage of 100 feet and the proposed building heights of 55 feet are less than the maximum allowed height of 75 feet. The setback requirements are not encroached upon and the lot coverage of 16.4% is less than the maximum ratio of 75% and the impervious surface of 55.4% is less than maximum ratio of 75%.

(w) Design consistent with performance standards.

The applicant has so designed the development as to make it probable that the development and its use will comply with the applicable performance standards of Article XII.

Sincerely,

A handwritten signature in black ink, appearing to read 'Philip Chen', with a long horizontal flourish extending to the right.

Philip Chen AIA, LEED AP

Principal

Ann Beha Architects

Bates Project Representative

ARTICLE 4
EXHIBITS A THROUGH K

A

BATES LEWISTON RESPONSE

EXHIBIT A

21 July 2014

Mr. David Hediger, City Planner
City of Lewiston
27 Pine Street
Lewiston, Maine 04240

Re: **Bates College Campus Housing Project – Responses to City Comments**

Dear Mr. Hediger,

The following are responses to comments received from the City of Lewiston on the preliminary Site Development Review Application dated 6/20/2014:

Lewiston Department of Public Works Comments:

July 10, 2014

Ryan Barnes, P.E., Project Engineer

Application:

1. Section 1.B. Construction Plan

- When will Structural BMPs be installed?

Response: The Structural BMPs will be installed near the end of the project when the site has been stabilized with either pavement or grass established. In the interim the outlet control structures will be installed and depressions created with underdrain to intercept the stormwater during construction.

2. Title, Right, and Interest

A temporary grading easement has been obtained from the property owner at 101 Franklin. Have easements been established regarding ownership and maintenance of the newly created private force main for this residence?

Response: The sewer connection and force main will be made within the right of way eliminating the need for an easement. The applicant will be in contact throughout the project with the resident of 101 Franklin Street.

3. Section 12.E. – Flooding Standard

The applicant will be required to meet the Flooding Standards for this development. Wright Pierce has contacted the Public Works to set up a meeting to review the flooding requirements for this project; the meeting is scheduled for July 15, 2014.

Response: The drainage calculations have been modified to address the flooding standard and other comments raised during our meeting. Revised calculations have been submitted that show the overall system impact of the 2, 10 and 25 year storm events for the pre-developed and post development conditions.

Traffic:

1. Section III – Pedestrian Accommodations

The traffic study recommends adding pavement markings to Campus Avenue and Central Avenue. This work as well as appropriate signage should be completed by the applicant since the study indicates that Bates will be promoting walking and biking on campus which will increase demand for these facilities in this area.

Response: Bates agrees to include the initial signage of Campus Ave from College St to Central Ave for sharing the road with bicyclists as part of the project. Bates is also planning to construct a sidewalk along the westerly side of Central Street from Campus Avenue to their existing sidewalk on Central, a distance of approximately 150 feet. Bates is not planning to address bicycle accommodations along Central at his time since the ultimate cross sectional configuration for Central Avenue has not been determined at this time.

2. Crash Summary

The crash summary indicates that there were two pedestrians collision, did either of these occur within the project area, and were they the result of a deficiency at the crossing.

Response: Based on information furnished by the MaineDOT, the collisions did not occur in the project area. One occurred on Sabattus St in 2010 when a resident backing out of their driveway hit a pedestrian in the sidewalk and the other also occurred in 2010 at the intersection of Ash St/ Central Ave/ Sabattus St and Webster Ave when a vehicle struck a pedestrian in the crosswalk, however the pedestrian did not have a walk signal.

Plans:

1. Sheet C2.0 – Proposed Conditions Site Plan

Please add the proposed cross walks, ADA Ramps, and appropriate signage to Campus Avenue for the proposed Campus Avenue Crossings.

Response: See revised drawing C2.0 for added cross walk, ADA Ramp and signage information at the Campus Avenue crossings.

Please show proposed signage associated with the discontinuance of Franklin Street.

Response: See revised drawing C2.0 for added signage at the ends of the closed portion of Franklin Street.

2. Sheet C2.2 – Proposed Site Utilities Plan

Is lighting proposed for the proposed pedestrian way at the end of Franklin Street? Lighting is proposed for the pedestrian way.

Response: Yes, lighting is proposed at the end of Franklin Street and along the proposed pedestrian way. See C2.2 and L1.1

3. Sheet C3.1 – Details II

The Cast Iron Handicap Detectable Warning Plate detail should be revised to indicate that it shall be bedded in concrete.

Response: The detail has been revised to indicate that the plate is bedded in concrete.

4. Sheet C3.2 – Erosion Control Details

The Stabilized Construction entrance should be revised to include geotextile fabric. **Response:** The detail has been revised to include geotextile fabric.

5. Sheet L1.3 – Planting Plan

Four Accolade Elm trees are proposed for the esplanade on Bardwell Street, these trees are being proposed 3-5 feet from the existing watermain. These trees shall be moved back to behind the sidewalk.

Response: The proposed esplanade on Bardwell Street has been eliminated, the side walk has been relocated and the trees have been moved off

Five Northern Red Oak trees are proposed for Franklin Street which will either be on top of the water main or within 3-5 feet of the watermain. These trees shall be moved back to provide separation.

Response: The trees have been relocated off of the watermain.

Lewiston Planning & Code Enforcement Comments:

July 14, 2014

David Hediger, City Planner

- The application/narrative should reference each of the development review criteria contained in Article XIII, Section 4, such that findings of facts and conclusions may be made.
Response: The application/narrative now includes references to each of the development review criteria in Article XIII, Section 4.
- Plan set does not appear to provide details site lighting, light types, etc. Lighting must be full cut off fixtures.
Response: Full cut off light fixtures will be used for all exterior site lighting. See lighting indicated on drawing L1.1
- Solid waste disposal: how is this being addressed; dumpster?
Response: Please find attached the sections and supporting information from the DEP applications for the project. One of the sections addresses the demolition debris and the other section addresses the construction debris and operational solid waste disposal from the student housing. There will not be dumpsters because of the manner in which Bates handles solid waste. The Bates facilities services picks up solid waste regularly from the facility and handles and disposes of the waste on a campus wide basis.
- Total site disturbance will exceed one acre and is located within an area of the city subject to municipal stormwater regulations (MS4). The applicant must submitted a performance guarantee for post construction stormwater management pursuant to Article XII, Section 15e(3). Staff recommends as a condition of approval that prior to a certificate of occupancy being issued that evidence of said documentation recorded in the Androskoggin Registry of Deeds is provided

regarding the maintenance and upkeep of the stormwater system pursuant to Lewiston MS4 stormwater requirements.

Response: A Performance Guarantee has been prepared and is submitted with this response. Please see attached Performance Guarantee.

- If development has not occurred as defined within the scope of the Zoning and Land Use Code within two years, development review approval shall expire, pursuant to Article XIII, Section 11. This must be noted on the site plan

Response: This note has been added to the Site Plan.

- Prior to a certificate of occupancy being issued a professional engineer must provide a stamped statement indicating all stormwater improvements have been completed in accordance with the approved plan. A note to this effect should be added to the plans.

Response: A note with this language has been added to the Site Plan.

- Transportation and Parking Assessment:

1. Plans and details needed for signs and striping proposed on Campus Avenue and Central Avenue. It's not clear what sections are being referenced. Bates will be responsible for sign and striping to be coordinated with the city. Will need Council approval.

Response: The plans and details are being updated to include signage and striping on Campus Avenue and Central Avenue.

2. Project is not expected to generate more parking and estimates have been provided on trip ends to the site. Assessment does not speak to the reprogramming of the space being vacated by the bookstore and mail center. What is the impact of the reprogramming?

Response: Bates has been anticipating and preparing for a major renovation of Chase Hall and has no immediate plans to re-occupy the spaces vacated by the bookstore and mail center.

3. Recommendation includes elimination of on street parking for 100' and 50' along Central Ave immediately abutting the project to allow for improved sight distance. Is this necessary? What is the typical requirement when on street parking is provided? Efforts to preserve on-street parking while maintaining safe access should be maintained. If necessary, will need Council approval.

Response: The comment was also brought up by Sgt Chick and our response is repeated for convenience. The posted speed on Central Avenue in the vicinity of the site driveway is 25 mph, which requires an available sight line of 200 feet. The available sight lines at the site driveways exceed 200 feet in each direction. In the Transportation and Parking Assessment we recommend that parking be restricted on the westerly side of Central from the proposed parking lot entrance a distance of 100 feet toward Campus Avenue and from the proposed parking lot southerly to the adjacent driveway a distance of 50 feet. While parking restricts sight lines at times in an urban area, it is quite common; The Manual on Uniform Traffic Control Devices (MUTCD) allows for vehicles to park within 20 feet of a crosswalk at an unsignalized intersection but does not specify the spacing for an unsignalized intersection.

Accordingly we concur with Sgt Chick that parking could be allowed within 50 feet of

the driveway given it is an urban area, Bates will work the Lewiston Police Department to evaluate whether there should be restrictions to these spaces such as prohibiting overnight parking.

4. What is the current parking demand of the uses on the site proposed for development? Comparison should be made as to pre/post parking conditions.

Response: The Office of Intercultural Education (OIE) has two faculty who will be relocating into Chase Hall and will continue to park on this site. The career development office at 53 Campus Avenue has eight staff who will be relocating to Wood Street (Canham House). The Alumni House moved to Lane two years ago and the Philosophy and Religion program moved three years ago to Hedge Hall. Several student organizations currently located in 45 Campus Avenue will be relocated to Chase Hall. These organizations generate pedestrian traffic but no parking demand. Several other rental units and single family homes will be removed with a total of 6 students and three units for faculty/staff. The total current staff/faculty is 19 in Chase Hall and 15 in the area of the proposed buildings. The total staff/faculty upon completion of the proposed project will be 15 in Chase Hall and 15 in the proposed buildings.

5. Report indicates that Bates will be completing a report on the parking condition in the vicinity of this project which will be submitted to City staff within 12 months of the opening of the project. Staff recommends this be a condition of approval tied to the date of issuance of the certificate of occupancy and that the applicant will submit and report their findings City staff and the Planning Board.

Response: Bates supports this recommendation.

6. The formation of a transportation committee is being recommended. Staff applauds this effort. Committee should include a representative from City staff; possible ad hoc member capacity. Ideally this would be implemented before the students return Fall 2014. Please provide deadlines of implementation.

Response: Bates supports this recommendation as a condition of approval. Bates is in the process of forming the committee and plans to hold the first meeting in October 2014.

7. Implementation of parking policy changes are recommended (p.2 section 8(f)). Those policies recommended should be implemented ASAP. Ideally this would be implemented before the students return Fall 2014. Please provide deadlines of implementation.

Response: The following parking policy changes to provide clarity and to promote increased usage of off street student parking will be implemented by the beginning of the 2014 fall semester. These policies will include:

- o Requiring all students that are issued parking permits to park in College lots only or risk forfeiture of parking privileges. The College will also work with the City and neighbors to identify which streets are not appropriate for faculty or staff parking. This will be a topic for the transportation committee to discuss.

- o Any student bringing a vehicle to Bates College regardless of whether or not they have obtained a Bates College Parking Permit must register with security for the

fall 2014 semester. This will help maintain better relations with neighbors and allow Bates to contact owners of vehicles when warranted.

- o Starting with the fall semester of 2014, the Bates parking office will work with the Lewiston Police Department to identify students who park on street in violation of the policy discussed above who are not tenants of off campus housing.

- o Update the parking section of the Campus Security Webpage to consolidate Parking Policy and Information. This is an ongoing effort which should be in place for the fall 2014 semester.

- o Creation of a dedicated parking map that is easily accessible on Bates.edu. The map will be compatible with smartphone and tablet devices. This is now in place and will be updated prior to the start of the fall semester to reflect realigned parking restrictions in on-campus parking lots.

8. On site parking on campus is proposed to be restricted to faculty and staff (p.2 section 8(b)). This should be implemented ASAP with a plan showing lots. Ideally this would be implemented before the students return Fall 2014. Please provide deadlines of implementation.

Response: Prior to the start of the fall 2014 semester student parking will be restricted in the Smith, Olin and Bardwell lots where it is currently allowed.

- **Right, title and interest:**

1. A number of the deeds provided reference covenants and restriction on setbacks, limitation on the number of dwelling units, and existing drainage rights to the city. Please reference limitations and restriction imposed by deeds.

Response: The majority of restrictive covenants imposed by the Franklin Company during the subdivision of the properties along Bardwell, Franklin, and Central (north of Vale) have expired. Bates has worked with property owners to secure the release of remaining restrictive covenants. Copies of these releases are included in Attachment E.

2. A purchase and sale agreement has been provided for 90 Central. Will the property be demolished? What will the use of the property be?

Response: The property will be demolished and the site is partially used as truck maneuvering for the proposed 65 Campus Avenue loading dock.

- **Sheet C2.0:**

1. Sidewalks proposed along Campus Ave are proposed to be located on Bates property; on Central Ave the sidewalk will straddle the ROW line; and depending upon Franklin Street status sidewalks will straddle the ROW line. Need to confirm whether easements will be need by city to maintain proposed sidewalks on Bates property as you are proposing to eliminate existing city sidewalks.

Response: See attached Exhibit: Easement Diagram for proposed easements from Bates to City of Lewiston.

2. Building A parking lot: front yard requirement is 10'; maybe be modified by 25% upon requesting a waiver. Clarify with respect to Bardwell and Franklin.

Response: The 10' front yard requirement is maintained at the 55 Campus Avenue parking lot at Bardwell Street and Franklin Street. Dimensions have been added to the drawings.

3. Plans are not clear on the blocks drawn (benches?) at the entrances to each building running parallel to Campus Ave. Clarify.

Response: Annotations have been added to the plans to clarify locations of site features including planters and site walls.

4. IO district has a 10' front yard requirement. Yards may be improved for gaining access to properties. However, expanse of impervious area within the yard area adjacent to the main entrances of each building seems to contrary to code requirement. Less impervious are should be provided in these area.

Response: The sidewalk along Campus Avenue is proposed to be pulled away from the street at the request of the City to create an esplanade and assist in snow storage and removal. Therefore the Campus Avenue sidewalk is brought closer to the buildings. There is expanded impervious paving at the entry terraces for the following reasons. First, these are active pedestrian areas with people approaching from several directions. The terraces not only allow passage but the opportunity to rest, wait for a friend, overlook the shared play lawn and people watch. Multiple seatwalls encourage such social interaction. The scale of these terraces is made more intimate with a rectangular planter set within the paving and surrounding canopy trees with overhanging branches.

- **Sheet C2.1:** grading behind 101 Franklin is not the same as shown on grading easement in application. Please address. Need to ensure water does not impact abutting property owner.
Response: The grading on the 101 Franklin Street property, for which the easement applies, is the same in the exhibit as in the plan. The grading outside the 101 Franklin Property has been revised to respond to changes in the site plan since the easement exhibit was prepared. The site will drain to the vegetated soil filter south of the 101 Franklin property which meets the intent of the easement.

- **Sheet L1.3:**

1. Street trees proposed with ROW. Staff is confirming with City Arborist on type and location proposed.

Response: Hedge Maple (*Acer campestre*) is not on Lewiston's approved list of shade trees but is a native slow growing tree hardy to zones 4-8 that eventually reaches 35' ht. The landscape architect has successfully used this tree underneath utility wires on past projects. The team proposes this tree for the Central Ave esplanade which is underneath utility wires and will specify the tree with initial six ft. height branching.

Similarly Accolade Elm (*Ulmus accolade*) is not on Lewiston's approved list but is a great candidate for esplanade planting on Campus Ave. This shade trees is hardy to zones 4-8, grows to 60 feet tall, and has a strong resistance to Dutch Elm Disease. Over time these trees will develop a signature vase-like outline and provide a partial ceiling on Campus Ave with their high overhanging branches.

2. Buffer is needed around 101 Franklin meeting buffer requirements unless owner signs off on proposed landscaping.

Response: The rear yard of 101 Franklin Street will be separated from the adjacent development by a variety of plant types including evergreen shrubs, deciduous shrubs, small deciduous trees and large deciduous trees. This varied planting will provide a stepped vegetated buffer that softens the surrounding development at various heights. In addition, evergreen shrub plantings are employed at the parking lots behind 55 and 65 Campus Ave to soften views from the public ways of Central Ave, Franklin Street and Bardwell Street. The loading dock at the back of 65 Campus Ave will also be screened with a variety of plants including numerous evergreen shrubs and trees. Several proposed evergreen and deciduous trees will further screen and soften views to the parking lots.

3. Depending upon use of 90 Central Avenue, there may be a need to meet the city's buffer requirements for this site. 90 Central was acquired by Bates and is to be demolished prior to project completion.

Response: Bates owns the adjacent property therefore buffer is not needed at this location.

- Overall, plans are difficult to distinguish curbing types.

Response: Additional annotations have been added to the drawings to more clearly distinguish curbing types.

- A plan should be provided showing easements necessary for drainage, sidewalks, Franklin t-turn.

Response: Refer to attached exhibit for necessary easements related to this project.

- Overall details are lacking on what is being proposed for Franklin Street (i.e. surface type, use of corridor, barriers, and signage, etc.). More information is needed.

Response: Refer to drawings C2.0 and L1.1 for information regarding surface type and signage and paving transitions at Franklin Street. Barriers will not be utilized.

- Building height; confirm compliance with IO district standards.

Response: The height of both buildings is approximately 55'-0" measured from average adjacent grade to the highest point of the roof which is under the 75'-0" allowed by IO district standards.

Lewiston Fire Department Comments:

July 15, 2014

Paul Ouellette, Lewiston Fire Department

No Comments at this time.

Lewiston Police Department Department Comments:

July 15, 2014

Sgt. David Chick, Lewiston Police Department

Comment: *"As I look at the prepared assessment, and include checking of maps and the surrounding geographic, I actually wonder if the bookstore/student gathering place and mailroom might potentially generate more vehicle activity (movement wise) than previous, where it will be made more accessible for vehicles to be brought there. There may be a greater perception of convenience to bring a vehicle there; not sure if that one is an apples-to-apples comparison with what is experienced now at the current location; that's speculating..."*

Response: The proposed bookstore/student gathering place and mailroom will be made more convenient for vehicle access for both passenger cars and delivery vehicles. Although the access for delivery vehicles will be more convenient for maneuvering, the number of delivery vehicles is anticipated to be the same. With respect to student vehicles, the parking lot will be restricted to staff and faculty and unauthorized vehicles will be ticketed by the Bates Parking Office.

Comment: *"I'm also still not fully sold as yet that the approach to direct all or most student parking over to the Merrill Gym lot provides enough incentive for the students to participate. It may work for those who will be displaced in the lots around the "village" dorms (Russell/Central); but I suspect that Vale St and the side streets off of it will likely pick up an increase of use for student parking. As discussed in the last meeting, the best deterrent for over encroachment of student vehicles is instituting a prohibition for overnight parking; but that is a broad brush which could also impact residents in the neighborhood (I don't want to propose that too casually)."*

Response: Bates will be requiring the students to park in the Merrill lot and will work with the Lewiston Police Department to monitor any student parking which occurs in the neighborhood south of Campus. The College is taking measures to encourage more students to leave their car at home by providing alternatives, however those who do bring a car will be required to register it with the College. This new requirement will assist Bates in contacting any students who are parking on street. Bates has experience in managing student parking in the vicinity of neighborhoods such as Mountain Avenue where there are 263 students housed with minimal disruption to parking in the neighborhood. Bates is committed to monitoring the parking in the neighborhood surrounding the project and in addition, is forming a transportation committee to meet regularly and to work with the City staff and neighborhood to address any parking issues that arise.

Comment: *"The section of Campus Ave situated between Central and Golder St is currently being fought over by SMRHC and LMS staff (SMRHC vehicles often even "bleed" over around the corner on Central Ave; encroaching into the campus surrounding)...This project is very likely to place another major competitor here. Note: however; from a PD standpoint, I really don't care so much, as other parking alternatives are presumably made available to all those parties, and the on-street parking is a preference selection. I'm more concerned with whether there will be an encroachment in the direction towards the Vale St direction and its more residential side streets; which have not experienced much of that pressure as yet."*

Response: As stated in our prior response, student parking will not be allowed on site and they will be directed to the Merrill Lot. Bates is committed to monitoring the parking in the neighborhood surrounding the project and in addition, is forming a transportation committee to meet regularly and to work with the City staff and neighborhood to address any parking issues that arise.

Comment: *"I initially questioned whether Central Ave will require creating as great a sight distance specified to become "no parking"; however, I will note that in giving consideration to the school activity with bus traffic, etc., that maybe one area where an overnight parking prohibition might work to mitigate it becoming an attractive "cluster" of student parking. That western side of Central Ave has a number of driveway openings which limit the density of parking there already, and we will need to be prepared to defend those driveways from vehicle encroachment; probably by delineated painted spaces. With the nearby angled parking in front of the Rec Armory creating an odd traffic adjustment, it might be worth exploring with residents whether or how they would support further limiting the parking on that side of the street (but then again, we need to be careful how much we eliminate available parking spaces). I think that it's worth noting that a study of this area done last year revealed there are a total of 18 potential legitimate parking spaces which could be made available on that westerly side for the entire block stretch (accounting with the driveways)-the same study determined that removing those (18) spaces would then be recovered by extending the angled parking on the opposite side of the entire block distance; which could be accomplished by restriping and shifting the travel lanes over. The school department representative did not favor making that change, and the dynamics associated with that school will remain a part of any equation being proposed here."*

Response: The posted speed on Central Avenue in the vicinity of the site driveway is 25 mph, which requires an available sight line of 200 feet. The available sight lines at the site driveways exceed 200 feet in each direction. In the Transportation and Parking Assessment we recommend that parking be restricted on the westerly side of Central from the proposed parking lot entrance a distance of 100 feet toward Campus Avenue and from the proposed parking lot southerly to the adjacent driveway a distance of 50 feet. While parking restricts sight lines at times in an urban area, it is quite common, The Manual on Uniform Traffic Control Devices (MUTCD) allows for vehicles to park within 20 feet of a crosswalk at an unsignalized intersection but does not specify the spacing for an unsignalized intersection. Accordingly we concur with Sgt Chick that parking could be allowed within 50 feet of the driveway given it is an urban area. Bates will work the Lewiston Police Department to evaluate whether there should be restrictions to these spaces such as prohibiting overnight parking.

Comment: *"Finally, I do think that there are options which can be explored, either proactively or reactionary, which can be acted further upon when problems are foreseen/seen which trigger the need for such response. We will continue to work collaboratively on this."*

Response: We agree with this conclusion. Bates is in the process of implementing the following measures to be proactive. These measures will be implemented by the fall 2014 semester:

- Implementation of parking policy changes to provide clarity and to promote increased usage of off street student parking. These policies will include:
 - Requiring all students that are issued parking permits to park in College lots only or risk forfeiture of parking privileges. The College will also work with the City and neighbors to identify which streets are not appropriate for faculty or staff parking.
 - Any student bringing a vehicle to Bates College regardless of whether or not they have obtained a Bates College Parking Permit must register the vehicle with security. This will help maintain better relations with neighbors and allow Bates to contact owners of vehicles when warranted.
 - The Bates parking office will work with the Lewiston Police Department to identify students who park on street in violation of the policy discussed above who are not tenants of off campus housing.
- Update parking information available on the web
 - Bates is updating the parking section of the Campus Security Webpage to consolidate Parking Policy and Information.
- Bates is creating a dedicated parking map that is easily accessible on Bates.edu. The map should be compatible with smartphone and tablet devices.
- Realigned parking restrictions in on-campus parking lots to improve the utilization of campus parking

Bates also recognizes that there may be issues that arise following the completion of the project which will need to be addressed. To facilitate this, Bates is forming a transportation committee which will be in place by the fall 2014 semester. This committee will be charged with promoting and monitoring initiatives that minimize the number of cars on campus and therefore demand for new spaces; to monitor the parking and transportation issues in and around this project; and to identify more sustainable transportation practices for official college business. The committee will include faculty and staff and students and a representative from the City. The committee will hold meetings with the neighborhood on an annual basis to gain feedback on what is working and what can be improved. The committee will also continue discussions with city staff on a regular basis to monitor progress.

Sincerely,

AnnBehaArchitects

33 Kingston Street
Boston, MA 02111
617.338.3000 T
617.482.9097 F

A handwritten signature in black ink, appearing to read 'Philip Chen', with a long horizontal flourish extending to the right.

Philip Chen AIA, LEED AP
Principal
Ann Beha Architects
Project Representative for Bates College

B

PERFORMANCE GUARANTEE

EXHIBIT B

PERFORMANCE GUARANTEE POST CONSTRUCTION STORMWATER MANAGEMENT PLAN

The President and Trustees of Bates College
2 Andrews Road
Lewiston, Maine

To the extent required by local ordinances and/or Maine state law, The President and Trustees of Bates College (Bates College), its successors, heirs, and assigns hereby acknowledge their obligations to operate, repair, maintain and replace the proposed Stormwater Management Facilities to be located at its proposed Student Housing Facilities located at 55 and 65 Campus Avenue, Lewiston, Maine, as described in the Inspection, Maintenance and Housekeeping Plan which is annexed to its Application for Development Review dated July 21, 2014 as Attachment 14. The Stormwater Management Facilities are depicted on Drawings C 4.1 Post Development Drainage Plan by Wright-Pierce dated July 21, 2014. Nothing in this guarantee shall obligate Bates College to construct the Stormwater Management Facilities if it does not construct the proposed student housing facilities. This performance guarantee shall be valid until Bates College has legally relinquished ownership of the subject property.

The City of Lewiston shall have the ability to establish a special assessment, district, or other means upon the parties responsible for the post construction stormwater management plan to ensure resources are available to perform the repairs, maintenance, and replacement of the Stormwater Management Facilities. The Stormwater Management Facilities at the subject property include: roof drip strips, under drains, storm drains, catch basins, outlet control structures, grassed soil filters, bio retention filters, rain gardens, and pervious concrete pavement with filters. Repairs or replacement for these items will have a wide range of costs associated with each particular device.

This Performance Guarantee may be recorded in the Androscoggin County Registry of Deeds. In witness whereof, The President and Trustees of Bates College has caused this Performance Guarantee to be executed by Natalie Williamson, its Interim Treasurer, thereunto duly authorized, on this ___ day of July, 2014.

The President and Trustees of Bates College

By: Natalie Williamson, Interim Treasurer

STATE OF MAINE
Androscoggin, __SS.

_____, 2014

Personally appeared before me the above-named Natalie Williamson, in her capacity as Interim Treasurer for President and Trustees of Bates College and acknowledged this Performance Guarantee to be her free act and deed in said capacity for President and Trustees of Bates College.

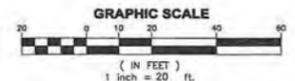
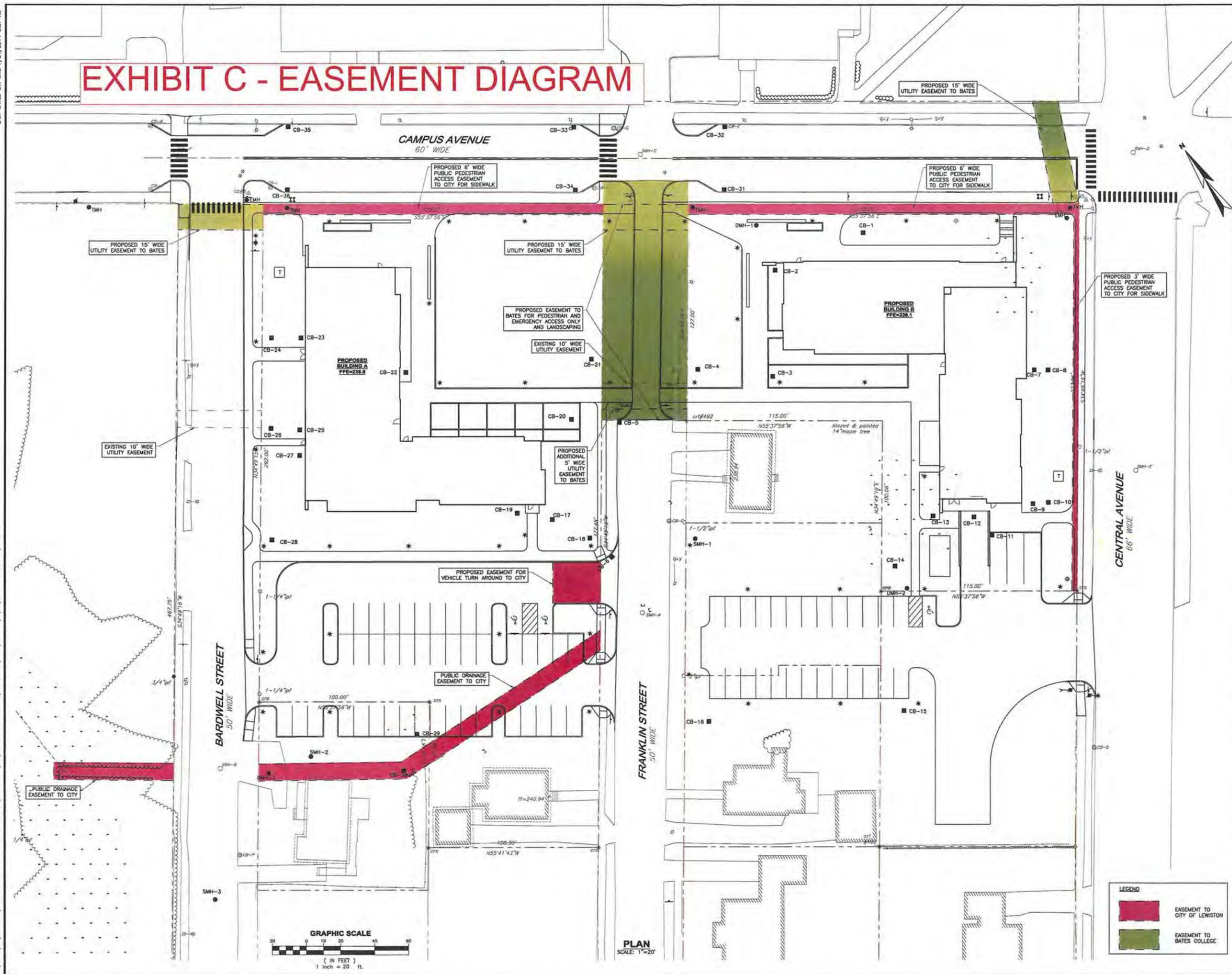
Notary Public
Print Name:
My commission expires:

C

EASEMENT DIAGRAM

LAST SAVED BY: MRL 7/21/2014 9:37 AM
 C:\UNIS\ME\LEWISTON\BATESCOLLEGE RESIDENCE - HALL\12929A-FD\G:\12929A\ASSEMBLY.DWG | easement | 1:1 | 7/21/2014 9:36:35 AM | MRL

EXHIBIT C - EASEMENT DIAGRAM



PLAN
SCALE: 1"=20'

LEGEND	
	EASEMENT TO CITY OF LEWISTON
	EASEMENT TO BATES COLLEGE

BATES COLLEGE
55 & 65 CAMPUS AVENUE HOUSING

LEWISTON, MAINE 04240

KEY PLAN

LEGEND

NO.	REVISION	DATE
1	RESPONSE TO CITY COMMENTS	7-21-14

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AnnBeha Architects
33 Kingston Street Boston, MA 02111
p.617.338.3000 f.617.482.9097

Job Number: 01313.01
Project: BATES COLLEGE
Drawn By: MRL Checked By: JSW
Date: 07.21.2014
Scale: 1"=20'
Drawing Title: EASEMENT PLAN

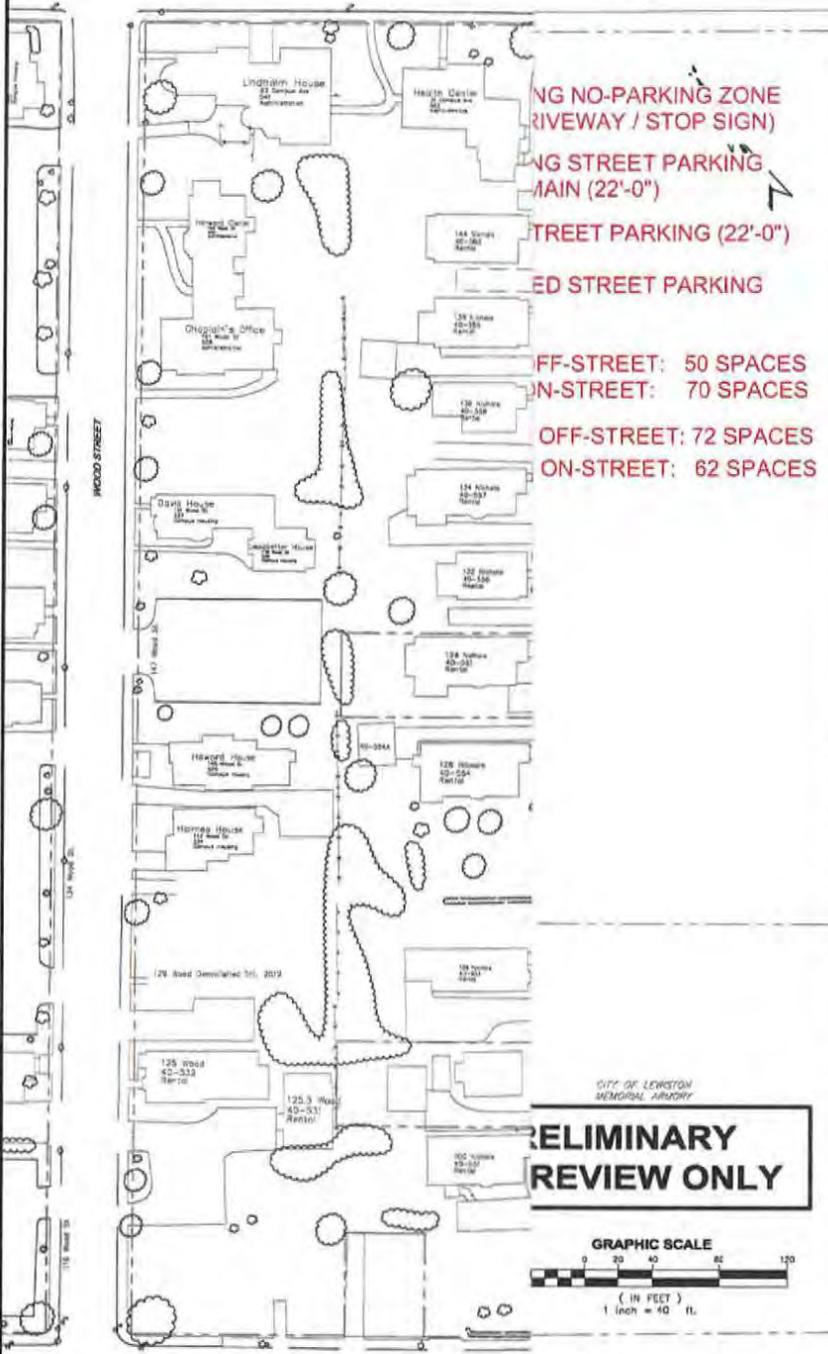
FIG 1

D

BATES PARKING DIAGRAM

LAST SAVED BY: MRL 7/1/2014 7:31 AM

EXHIBIT D - PA



NO-PARKING ZONE
RIVEYWAY / STOP SIGN)

STREET PARKING
MAIN (22'-0")

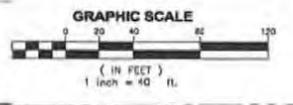
STREET PARKING (22'-0")

ED STREET PARKING

OFF-STREET: 50 SPACES
ON-STREET: 70 SPACES

OFF-STREET: 72 SPACES
ON-STREET: 62 SPACES

**PRELIMINARY
REVIEW ONLY**



BATES COLLEGE
55 & 65 CAMPUS AVENUE HOUSING

LEWISTON, MAINE 04240

KEY PLAN

LEGEND

3	80% D.D. REVIEW	7-2-14
2	CITY OF LEWISTON REV.	6-20-14
1	DEP REVIEW	6-13-14
NO.	REVISION	DATE

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Job Number: 01313.01
Project: BATES COLLEGE
Drawn By: MRL, Checked By: JBRW
Date: 06.20.2014
Scale: 1" = 40'
Drawing Title:

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E

TITLE RIGHT OR INTEREST

EXHIBIT E

EXHIBIT E: TITLE, RIGHT OR INTEREST

Title, Right or Interest

The project site is generally bounded by Campus Avenue on the northeast, Central Avenue on the southeast and Nichols Street on the northwest and is comprised in whole or in part of property conveyed to The President and Trustees of Bates College (Bates College) by several deeds, one property with a purchase agreement and one grading easement. The following Table 2.1 lists the parcels comprising the project site arranged by City of Lewiston tax map and lot number with the corresponding street addresses and book and page references to the deeds recorded at the Androscoggin County Registry of Deeds:

TABLE 2.1

Grantee: THE PRESIDENT AND TRUSTEES OF BATES COLLEGE

Tax Map/Lot Number	Address	Deed References - Book/Page
173/5	90 Central Ave	P&S Agreement
173/9	101 Franklin	Grading Easement
173/6	100 Central St	1404/350
173/7	106 Central St & 75 Campus St	1405/7
173/8	67 Campus St	3223/267
173/10	63 Campus St	1030/493
194/113	85 Franklin St	3014/180
194/114	85 Franklin St	3014/180
194/136	84 Franklin St	2258/210
194/137	Parking Lot	2258/210 & 2023/250
194/138	53 Campus St	2023/252
194/139	111 Bardwell St	2212/349
194/140	109 Bardwell St	2249/12
194/167	45 Campus St	1681/202
194/168	Parking Lot	903/58

(Tax Map/Lot Numbers are City of Lewiston Tax Maps/Lot Numbers)
(Deed References are Book and Page, Androscoggin County Registry of Deeds)

Attached hereto are copies of the deeds and agreements referenced above.

EXHIBIT E

BOOK 1404 PAGE 350

04902

MAINE SHORT FORM WARRANTY DEED

I, FRANK S. HOY, being a widower, of Lewiston in the County of Androscoggin and State of Maine, for consideration paid, grant to THE PRESIDENT AND TRUSTEES OF BATES COLLEGE, a Maine corporation located in Lewiston in said County and State, with WARRANTY COVENANTS, a certain parcel of land with the buildings thereon situated in Lewiston in said County of Androscoggin and State of Maine, bounded and described as follows:

BEGINNING at a point in the northwesterly line of Central Avenue which point is one hundred thirtyseven (137) feet southwesterly along Central Avenue from the southwesterly line of Campus Avenue and thence the line runs in a southwesterly direction along said line of Central Avenue a distance of ninety and six hundredths (90.06) feet, more or less, to land conveyed by Frank S. Hoy to Mabel H. Ballis by deed dated March 8, 1972, recorded in the Androscoggin County Registry of Deeds in Book 1053, Page 54; thence the line runs in a northwesterly direction along said land conveyed to Ballis a distance of one hundred fifteen (115) feet to a point; thence the line runs in a northeasterly direction parallel with said line of Central Avenue a distance of ninety and twentyone hundredths (90.21) feet, more or less, to a point which is one hundred thirty-seven (137) feet southwesterly from and along a line drawn perpendicular to Campus Avenue; thence the line runs in a southeasterly direction a distance of one hundred fifteen (115) feet to the point of beginning.

For the source of title of this grantor, see deed from Hoywood Realty & Investment Corp. dated September 11, 1974, recorded in said Registry of Deeds in Book 1123, Page 100. See also deed from Franklin Company to this grantor and Gladys P. Hoy as joint tenants dated December 20, 1945, recorded in said Registry of Deeds in Book 342, Page 178. The said Gladys P. Hoy died testate on February 29, 1968.

The northeasterly portion of the above premises with a frontage of forty (40) feet on Central Avenue and a depth therefrom of one hundred fifteen (115) feet is subject to the restrictions contained in a quitclaim deed from Franklin Company to said Hoyas dated January 20, 1944, recorded in the Androscoggin County Registry of Deeds in Book 342, Page 158, and the southwesterly portion of the above premises with a frontage of 50.06 feet and a depth of 115 feet is subject to the restrictions contained in a deed from Franklin Company to said Hoyas dated December 20, 1945, recorded in said Registry of Deeds in Book 342, Page 178.

ALSO hereby conveying all rights, easements, privileges, and appurtenances belonging to the premises hereinabove described.

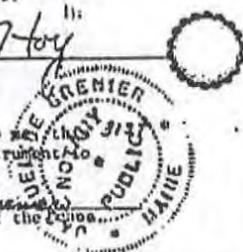
WITNESS my hand and seal this 31st day of May, 1979.

Dorenda Ross
 STATE OF MAINE
 ANDROSCOGGIN, SS.

Frank S. Hoy
 Frank S. Hoy

Personally appeared the above named FRANK S. HOY, known to me the day of May, 1979, and acknowledged before me the foregoing instrument to be his free act and deed.

Jacqueline G. G...
 Notary Public/Justice of the Peace



ANDROSCOGGIN SS.
 RECEIVED JUN - 4 1979
 AT 9 H. 03 M. P. M.
 and recorded from original

04545

BK3223 PG267

I, JOHN J. BOURISK, JR., Conservator of Nellie M. Bourisk, of 84 Standish Street, Worcester, County of Worcester and Commonwealth of Massachusetts, for consideration paid, grant to THE PRESIDENT AND TRUSTEES OF BATES COLLEGE, a corporation of Lewiston, County of Androscoggin and State of Maine, the land situated in said Lewiston, bounded and described as follows; to wit:

Two certain lots or parcels of land, with the buildings thereon, situated in said Lewiston and bounded and described as follows:

PARCEL 1. A certain lot or parcel of land, with the building thereon, situated on the southwesterly side of Campus Avenue, bounded and described as follows: Commencing at the intersection of the southwesterly line of Campus Avenue with the southeasterly line of Franklin Street; thence southwesterly by the southeasterly line of Franklin Street for a distance of One Hundred Twenty-Seven (127) Feet; thence southeasterly parallel with the southwesterly line of Campus Avenue for a distance of Sixty-Five (65) Feet; thence northeasterly parallel to the southeasterly line of Franklin Street for a distance of One Hundred Twenty-Seven (127) Feet to Campus Avenue; thence northwesterly by the southwesterly line of Campus Avenue for a distance of Sixty-Five (65) Feet to Franklin Street and the point of beginning.

PARCEL 2. A certain lot or parcel of land situated on Campus Avenue in said Lewiston, bounded and described as follows: Beginning at a point on the southwesterly line of Campus Avenue One Hundred Fifteen (115) Feet easterly from the intersection of said Campus Avenue and Franklin Street, said point being the northerly corner of land conveyed by the Franklin Company to William J. Hall by Deed No. 1666, dated December 15, 1924; thence southwesterly by the northwesterly line of said Hall land for a distance of One Hundred Twenty-Seven (127) Feet; thence northwesterly parallel to the southwesterly line of said Campus

1948-49 REAL ESTATE
TRANSFER TAX PAID

Know All Men by These Presents,

That Mildred D. Ehrenfried of Lewiston, County of Androscoggin, State of Maine

in consideration of One Dollar and other valuable considerations

paid by President and Trustees of Bates College, a corporate body, of Lewiston, County and State aforesaid

the receipt whereof I do hereby acknowledge, do hereby give, grant, bargain, sell and convey unto the said

President and Trustees of Bates College, its administrators and

heirs and assigns forever, a certain lot or parcel of land

Commencing at a point in the South Westerly line of Campus Avenue fifty (50) feet South Easterly, from the most Easterly corner of land conveyed by the Franklin Company to Ella W. Jones by deed No. 1419 dated May 20, 1914; thence South Easterly by the South Westerly line of Campus Avenue fifty (50) feet to Franklin Street; thence South Westerly by the North Westerly line of said Franklin Street one hundred twenty-seven (127) feet; thence North Westerly parallel to the South Westerly line of Campus Avenue fifty (50) feet; thence North Easterly parallel to the North Westerly line of Franklin Street one hundred twenty-seven (127) feet to Campus Avenue and point of beginning.

Lewiston

Subject to the restrictions that no buildings erected thereon shall be placed nearer the line of Campus Avenue than twenty-five (25) feet, or nearer the line of Franklin Street than ten (10) feet, and that for twenty-five (25) years, from October 18, 1922, no buildings shall be built thereon for occupancy of or occupied by more than two (2) families.

The title of this Grantor was obtained by deed from Jacob B. Ehrenfried dated March 23, 1934, recorded in the Androscoggin County Registry of Deeds in Book 432, Page 419, and by deed of Charlotte A. Dawber dated December 8, 1948, recorded in said Registry in Book 620, Page 413.

Also another certain lot or parcel of land situated in said Lewiston and bounded and described as follows, to wit:

Commencing in the southwesterly line of Campus Avenue at the northerly corner of land of this Grantor; thence in a southwesterly direction, by land of this Grantor, one hundred twenty-seven (127) feet to land now or formerly of the Franklin Company; thence in a northwesterly direction by land now or formerly of the Franklin Company five (5) feet; thence in a northeasterly direction, parallel with the first described line, one hundred twenty-seven (127) feet to the said southwesterly line of Campus Avenue; thence

that I have good right to sell and convey the same to the said Grantee to hold as aforesaid; and that I and my heirs shall and will Warrant and Defend the same to the said Grantee, its administrators heirs and assigns forever, against the lawful claims and demands of all persons.

In Witness Whereof, the said Mildred D. Ehrenfried (widow)

~~XXXX~~

~~XXXX~~ ~~XXXXXXXXXXXX~~

~~giving in this deed my grantor and~~
~~relinquishing and conveying~~ ~~right by descent and all other~~
~~rights in the above described premises~~ have hereunto set my hand and seal this 11th day of March in the year of our Lord one thousand nine hundred and seventy-one.

Signed, Sealed and Delivered in presence of

Frank W. Russell

Mildred D. Ehrenfried
Mildred D. Ehrenfried 

State of Maine, Androscoggin ss. March 11, 1971.

Personally appeared the above named

Mildred D. Ehrenfried

and acknowledged the foregoing instrument

to be her free act and deed.

ANDROSCOGGIN, ss. 8
RECEIVED MAR 24 1971 H. - W. A. W.
and recorded from the original

Before me,
Frank W. Russell
Justice of the Peace
~~Notary Public~~

PARCEL THREE: Beginning at an iron pipe at the southeasterly corner of land of these grantors as described in a deed recorded in Book 956, Page 688 of said Registry, said parcel being Parcel Two described herein; thence in a northeasterly direction and parallel with the southeasterly line of Franklin Street, a distance of six (6) feet to a point; thence at a right angle in a northwesterly direction and parallel with the northeasterly line of Parcel Two herein a distance of sixty (60) feet to a point; thence at a right angle in a southwesterly direction and parallel with the southeasterly line of Franklin Street a distance of six (6) feet to a point in the northeasterly line of Parcel Two herein; thence in a southeasterly direction along the northeasterly line of Parcel Two herein a distance of sixty (60) feet to the point of beginning.

This conveyance is subject to the restrictions that no buildings erected on said premises shall be placed nearer the line of said Franklin Street than ten (10) feet, and that no building other than a dwelling house for occupancy of not more than two (2) families, and only such garage or other outbuildings as may be appropriate for or incidental to the use of the occupants of such dwelling house shall be built on the lot hereby conveyed, and no such house shall be used in whole or in part as a store or for business purposes.

BEING THE SAME PREMISES conveyed to these grantors by deed of Lionel and Diane Rodrigue dated May 26, 1988 and recorded in the Androscoggin County Registry of Deeds in Book 2252, Page 10.

ALSO ANOTHER CERTAIN LOT OR PARCEL OF LAND situated in said Lewiston, bounded and described as follows: Beginning at a point in the southeasterly line of Franklin Street, so called, at the westerly corner of land conveyed to David A. Nelson and Selma A. Nelson by the Franklin Company by Deed #2026, dated July 2, 1962 said point also being one hundred eighty-seven (187) feet southwesterly along the southeasterly line of said Franklin Street from the southwesterly line of Campus Avenue, so called; thence in a southwesterly direction along the southeasterly line of said Franklin Street a distance of ninety (90) feet to a point at the northerly corner of land conveyed to Kate Minsky by the Franklin Company by Deed #1949, dated May 14, 1956; thence in a southeasterly direction along the northeasterly line of said Minsky's land and parallel to the southwesterly line of said Campus Avenue a distance of one hundred fifteen (115) feet to a point at the easterly corner of said Minsky's land; thence in a northeasterly direction parallel to the southeasterly line of said Franklin Street a distance of ninety (90) feet to a point at the southerly corner of said Nelson's land; thence in a northwesterly direction along the southwesterly line of said Nelson's land and parallel to the southwesterly line of Campus Avenue a distance of one hundred fifteen (115) feet to the point of beginning.

MAINE SHORT FORM WARRANTY DEED

I, PAUL M. BOULAY, of Lewiston in the County of Androscoggin and State of Maine, for consideration paid, grant to THE PRESIDENT AND TRUSTEES OF BATES COLLEGE, a Maine corporation with its principal place of business in Lewiston in said County and State, with WARRANTY COVENANTS, a certain lot or parcel of land with the buildings thereon situated in said Lewiston, being more particularly described on the attached Exhibit A.

The premises are conveyed subject to any easements and restrictions of record and this deed includes all rights, easements, privileges, and appurtenances belonging to the premises hereinabove described.

~~JEANNE~~
JEANNE M. BOULAY, wife of the said PAUL M. BOULAY, joins as a grantor and releases her rights by descent and all other rights.

WITNESS our hands and seals this _____ day of _____ 1988.

Jacqueline Greiner
Witness
to both

Paul M. Boulay
Paul M. Boulay
Jeanne M. Boulay
JEANNE



STATE OF MAINE
ANDROSCOGGIN, SS.

Personally appeared the above named PAUL M. BOULAY, known to me, this 31st day of MAY, 1988, and acknowledged before me the foregoing instrument to be his free act and deed.

Jacqueline Greiner
Notary Public/Attorney-at-Law
Jacqueline Greiner
Type or Print Name
Commission expires 2/2/92



Jrea3.6

- O V E R -

MAINE REAL ESTATE
TRANSFER TAX PAID

MAINE SHORT FORM WARRANTY DEED

I, ABIGAIL S. CARRIER, of Lewiston in the County of Androscoggin and the State of Maine, for consideration paid, grant to the PRESIDENT AND TRUSTEES OF BATES COLLEGE of Lewiston in said County and State, with WARRANTY COVENANTS, a certain parcel of land with the buildings thereon situated on the southwesterly side of Campus Avenue in said Lewiston and bounded and described as follows:

BEGINNING at the intersection of the southwesterly line of Campus Avenue and the southeasterly line of Bardwell Street;

THENCE, southeasterly along said line of Campus Avenue a distance of one hundred forty-five (145) feet to the five (5) foot strip of land conveyed by said Abigail S. Carrier to Mildred D. Ehrenfried on March 29, 1957 by deed recorded in the Androscoggin County Registry of Deeds in Book 754, Page 394;

THENCE, southwesterly by land of said Ehrenfried and parallel to said Bardwell Street one hundred and twenty-seven (127) feet to the land conveyed by Stanley Schar as trustee of the Franklin Property Trust to the grantor herein on June 21, 1977 by deed recorded in said Registry of Deeds in Book 1275, Page 273 (said land also being conveyed to the grantee herein by Quitclaim Deed of even date herewith);

THENCE, northwesterly parallel to Campus Avenue by the land of the grantor herein and also land now or formerly of Walter A. Lawrence a distance of sixty-five and sixty-seven hundredths (65.67) feet to the most southerly point of a five (5) foot strip of land conveyed by said Abigail S. Carrier to Walter A. Lawrence in June of 1959 by deed recorded in said Registry of Deeds in Book 806, Page 413;

THENCE, northeasterly by said strip of land and parallel to Bardwell Street a distance of five (5) feet to a point;

THENCE, northwesterly by said strip of land and parallel to Campus Avenue a distance of seventy-nine and thirty-three hundredths (79.33) feet to the southeasterly line of Bardwell Street;

THENCE, northeasterly by said line of Bardwell Street one hundred twenty-two (122) feet to the point of beginning.

This parcel is conveyed subject to the following building setback restrictions: (1) no building shall be erected within ten (10) feet of Bardwell Street; (2) no building shall be erected on the northwesterly one hundred (100) foot portion of this parcel within twelve (12) feet of Campus Avenue; and (3) no building shall be erected on the southeasterly forty-five (45) foot portion of this parcel within twenty-five (25) feet of Campus Avenue.

Being a portion of the two parcels conveyed to the grantor herein by Emma M. Eaton, Conservator of Ella W. Jones, on August 12, 1955 by deed recorded in said Registry of Deeds in Book 730, Page 27.

ALSO hereby conveying all rights, easements, privileges, and appurtenances belonging to the premises hereinabove described.

MAINE REAL ESTATE
TRANSFER TAX PAID

.02499

KNOW ALL MEN BY THESE PRESENTS,

THAT KEY TRUST COMPANY OF MAINE, of Augusta, County of Kennebec, State of Maine, being the Personal Representative of the Estate of Walter A. Lawrance, who died on November 23, 1987, having been appointed by the Androscoggin County Probate Court on January 4, 1988, and also being the duly qualified Trustee under the Testamentary Trust of WALTER A. LAWRENCE created by his Will dated October 11, 1985, do hereby release to the President and Trustees of Bates College, of Lewiston, County of Androscoggin, State of Maine, to them and their heirs and assigns forever, the following described real estate located in Lewiston, County of Androscoggin, State of Maine, bounded and described as follows:

Two certain lots or parcels of land with the buildings thereon situated in said Lewiston and bounded and described as follows, to wit:-

FIRST PARCEL: Commencing at a point in the Southeasterly line of a proposed Street known as Bardwell Street at the Westerly corner of land conveyed by the Franklin Company to Ella W. Jones by Deed No. 1419, dated May 20, 1914; thence Southeasterly by the Southwesterly line of said Ella W. Jones land one hundred (100) feet; thence Southwesterly parallel with the Southeasterly line of Bardwell Street fifty (50) feet; thence Northwesterly parallel with the Southwesterly line of said Ella W. Jones land one hundred (100) feet to Bardwell Street; thence Northeasterly by the Southeasterly line of said Bardwell Street fifty (50) feet to the point of beginning.

Subject to the restriction that no buildings erected thereon shall be placed nearer the line of Bardwell Street than ten (10) feet.

SECOND PARCEL: Commencing at a point in the Southeasterly line of a proposed Street, known as Bardwell Street, at the Westerly corner of land conveyed to Chester E. Warren by the Franklin Company by Deed No. 1544, dated May 24, 1920; thence Southeasterly by the Southwesterly line of said lot conveyed to Chester E. Warren one hundred (100) feet; thence Southwesterly parallel with the Southeasterly line of Bardwell Street fifty (50) feet; thence Northwesterly parallel with the Southwesterly line of said lot conveyed to Chester E. Warren one hundred (100) feet to Bardwell Street; thence Northeasterly by the Southeasterly line of said Bardwell Street fifty (50) feet to the point of beginning.

Subject to the restriction that no buildings erected thereon shall be placed nearer the line of Bardwell Street than ten (10) feet.

Being the same premises described in a certain deed from Lizzie A. Curtis, Emma Curtis, Ella Curtis and Lue M. Curtis to Walter

MAINE REAL ESTATE
TRANSFER TAX PAID

BK2212 PG351

STATE OF MAINE

ANDROSCOGGIN, SS.

FEBRUARY 7, 1988

Personally appeared the above-named William B. Swales and acknowledged the above instrument to be his free act and deed in his said capacities.

Jeanette M. Lunt
NOTARY PUBLIC
JEANETTE M. LUNT
(Printed Name of Notary)

SEAL

ANDROSCOGGIN SS
RECEIVED FEB 19 1988
AT 9 H. — M. A. M.

FALES & FALES, P.A.
ATTORNEYS AT LAW
182 LEBRON ST
LEWISTON, MAINE 04240

10.00
Fales & Fales

SKELTON, JAINSON, ABBOTT & ONESTIS
488 MAIN STREET
P. O. BOX 1728
LEWISTON, MAINE 04240

BOOK 1681 PAGE 0202

MAINE SHORT FORM WARRANTY DEED

14255

We, ROBERT E. DILLINGHAM and ALICE C. DILLINGHAM, being husband and wife, of Lewiston in the County of Androscoggin and State of Maine, for consideration paid, grant to the PRESIDENT AND TRUSTEES OF BATES COLLEGE, a Maine corporation located in said Lewiston, with WARRANTY COVENANTS, a certain lot or parcel of land with the buildings thereon situated in said Lewiston, bounded and described as follows:

BEGINNING at the intersection of the southwesterly line of Campus Avenue with the northwesterly line of Bardwell Street and thence the line runs in a southwesterly direction along said line of Bardwell Street a distance of one hundred seventy-seven (177) feet to a point; thence the line runs in a northwesterly direction a distance of one hundred one (101) feet to a point; thence the line runs in a northeasterly direction a distance of one hundred seventy-seven (177) feet to said line of Campus Avenue; thence the line runs in a southeasterly direction along said line of Campus Avenue a distance of one hundred one (101) feet to the point of beginning.

The above premises are conveyed subject to the restriction that no building shall be erected thereon nearer the line of Campus Avenue than twelve (12) feet nor nearer the line of Bardwell Street than ten (10) feet.

For the source of title of these grantors, see deed from the Trustees of the will of Louis B. Costello dated October 19, 1959, recorded in the Androscoggin County Registry of Deeds in Book 815, Page 450.

ALSO hereby conveying all rights, easements, privileges, and appurtenances belonging to the premises hereinabove described.

ROBERT E. DILLINGHAM and ALICE C. DILLINGHAM both join as grantors and release all rights by descent and all other rights.

WITNESS our hands and seals this 28th day of September, 1983.

Jacqueline Grenier
Witness

Robert E. Dillingham
Robert E. Dillingham

to both

Alice C. Dillingham
Alice C. Dillingham

STATE OF MAINE
ANDROSCOGGIN, SS.

Personally appeared the above named ROBERT E. DILLINGHAM, known to me, this 28th day of September, 1983, and acknowledged before me the foregoing instrument to be his free act and deed.

ANDROSCOGGIN SS.
RECEIVED OCT 31 1983
AT 4 H. 30 M. P. M.

Jacqueline Grenier
Notary Public/Attorney-at-Law
JACQUELINE GRENIER



5. Purchase Price. The purchase price (the "Purchase Price") for the Premises is [REDACTED] of which \$1,000.00 shall be paid as earnest money within seven (7) days of the execution of this Agreement by Seller and Buyer (the "deposit"). The deposit is to be held by Skelton, Taintor & Abbott in a non-interest bearing account, and no payment to Seller or refund to Buyer in accordance with the terms of this Agreement shall take place without the written agreement of both Seller and Buyer. The deposit shall be applied without interest to the Purchase Price at Closing. The balance of the purchase price shall be paid by Buyer at the time of the delivery of the Deed.

6. Time for Performance; Delivery of the Deed. The parties shall close on July 1, 2014 (the "Closing Date") or after, but must close no later than July 10, 2014. The Deed is to be delivered on the Closing Date (the "Closing") at the offices of Buyer at 11 Andrews Road, Lewiston, Maine, or such other date as the parties may agree in writing. The parties agree that time is of the essence in the performance of this Agreement. The Buyer agrees to pay up to One Thousand Five Hundred Dollars (\$1,500.000) of Seller's documented closing costs.

7. Condition of Premises; Inspections; Contingencies. Buyer's obligations under this Agreement are contingent upon satisfaction of the following:

Buyer's review and approval according to Buyer's sole discretion, of building, survey or environmental inspections regarding the Premises performed at Buyer's expense, including, without limitation, Buyer obtaining an Environmental Site Assessment and such building inspections and other tests and assessments as Buyer shall desire (collectively, the "Studies"). Buyer shall have a right of entry on the Premises in order to perform the Studies, provided Buyer agrees in connection with such entry and the performance of the Studies to repair and restore any property damage caused by its entry and activities in connection with the Studies. If Buyer exercises any such contingency, this Agreement shall terminate, the deposit shall be returned to Buyer, and neither party shall have any further obligations hereunder.

8. Possession. Full possession of the Premises shall be delivered to Buyer by Seller at the Closing free and clear of any tenant or occupant in the same condition as they are now, reasonable wear and excepted, in broom clean condition and free and clear of any debris or personal property except as set forth in paragraph 2 hereof. Buyer shall have the right to view the Premises within twenty-four (24) hours prior to Closing to determine that the Premises are in conformance with this Agreement.

9. Trustee Approval. The Buyer's obligations hereunder are subject to the approval of the Executive Committee of the Board of Trustees of the Buyer at its May meeting.

10. Seller's Covenants and Warranties. Seller make the following warranties to the Buyer, each of which is true as of the date of this Agreement; will be true on the Closing Date and shall survive the Closing:

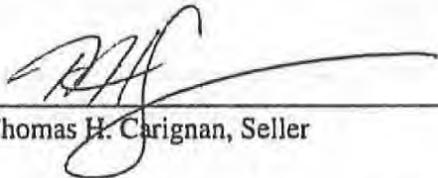
17. Title Insurance Affidavits and Indemnifications. The Seller agrees to furnish the Buyer at the Closing executed affidavits and indemnifications regarding survey matters, mechanics' and materialmen's liens and parties in possession. The Seller agrees that if the Deed is signed by Seller' attorney-in-fact the original power of attorney shall be produced at the Closing for recording at the Androscoggin County Registry of Deeds.

18. Construction of Agreement. This Agreement shall be construed under and governed by the laws of the State of Maine; is to take effect as a sealed instrument; sets forth the entire contract between the parties with respect to the Premises; is binding upon and inures to the benefit of the parties hereto and their respective heirs and devisees, executors, administrators, legal representatives, successors and subject to any provisions regarding non-assignability of this Agreement, assigns, and may be canceled, modified or amended only by a written instrument executed by both the Seller and the Buyer or his or their successors in interest. This Agreement may be executed in any number of counterparts, each of which shall be deemed an original, and all of which together shall be one and the same Agreement. The captions are used only as a matter of convenience and are not to be considered a part of this Agreement or to be used in determining the intent of the parties. Seller shall from time to time after the Closing at the request of Buyer without further consideration, execute and deliver such further instruments of transfer or take such additional actions as Buyer may reasonably request to transfer more effectively and vest in Buyer the Premises.

19. Effective Date. This Agreement is a binding contract on the date first written above.

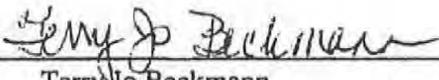
WITNESS the execution hereof under seal as of the date first above written.

WITNESS:

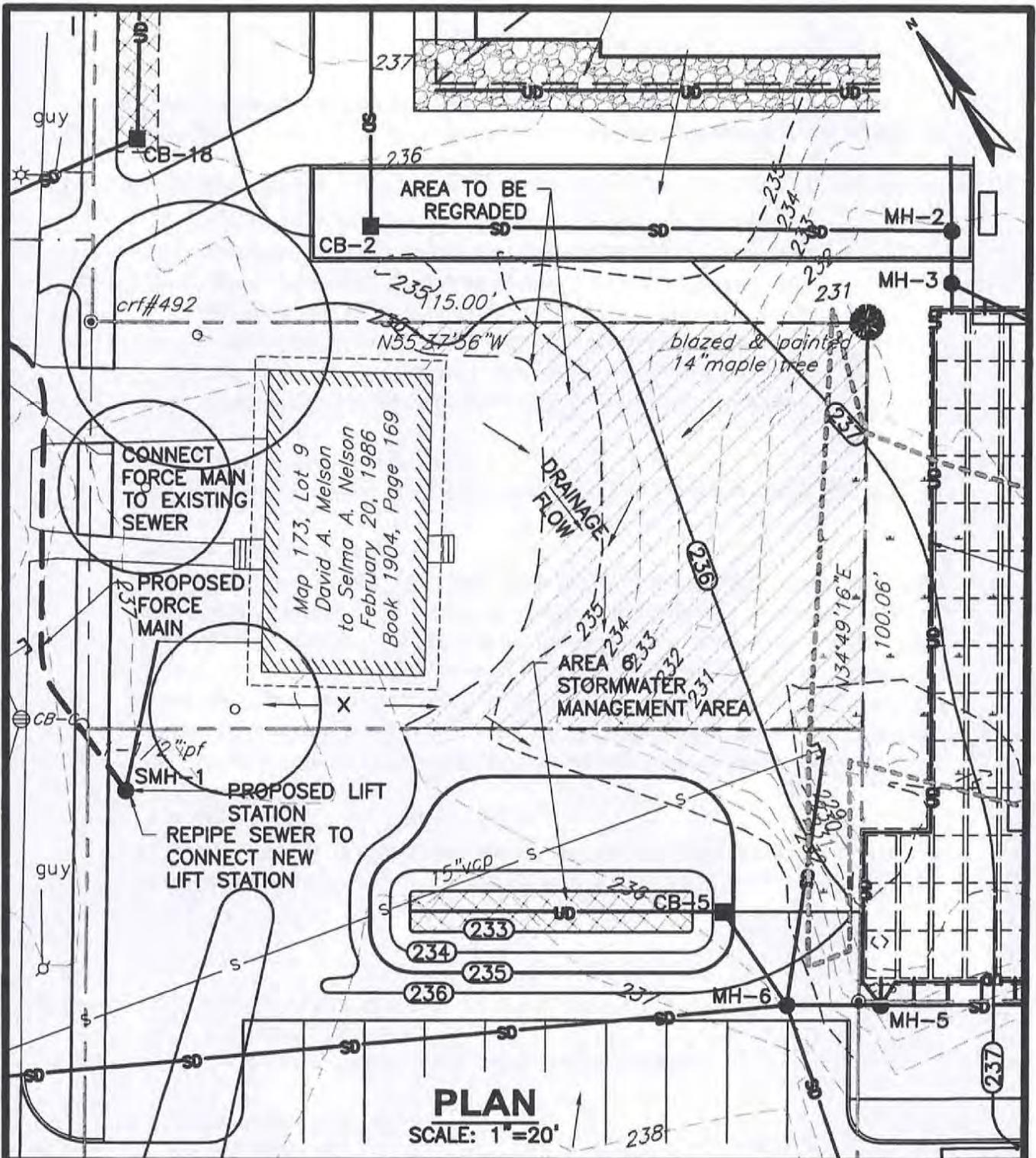


Thomas H. Crignan, Seller

PRESIDENT AND TRUSTEES OF
BATES COLLEGE, Buyer

By: 

Terry Jo Beckmann
Vice President for Finance and
Administration and Treasurer



PLAN
SCALE: 1"=20'

101 FRANKLIN STREET
LEWISTON, ME

PROJ NO: 12929A DATE: MAY 2014

WRIGHT-PIERCE
Engineering a Better Environment

NO.	REVISIONS	APP'D
1		
2		
3		

EXHIBIT A

EXHIBIT:
A

NOW, THEREFORE, in consideration of the premises, the Grantor and the College agree as follows:

1. The Grantor releases to the College all of the Grantor's right, title and interest in the College's Lots, including any right to enforce the Restrictions.
2. The College releases to the Grantor all of the College's right, title and interest in the Grantor's Lot, including any right to enforce the Restrictions.
3. The Grantor and the College agree that the Restrictions are terminated and no longer of any force or effect.
4. The College agrees that its use of the College's Lots shall be limited to uses permitted of "Academic Institutions" by the Lewiston Zoning and Land Use Code.
5. The terms of this instrument are binding upon the heirs, successors and assigns of the parties hereto.

IN WITNESS WHEREOF, the Grantor and the College have signed and sealed or caused this instrument to be signed and sealed this 19th day of February, 2014.



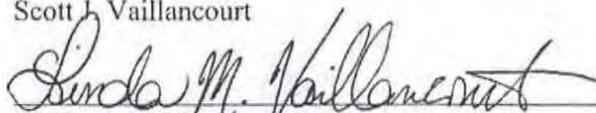
Witness



Witness

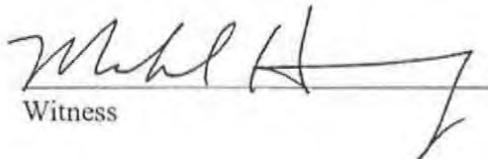


Scott J. Vaillancourt

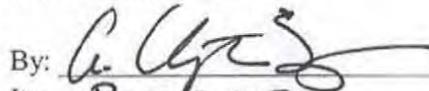


Linda M. Vaillancourt

PRESIDENT AND TRUSTEES OF
BATES COLLEGE



Witness

By: 

Its: PRESIDENTS
Print/type name: A. CLAYTON SPENCER

EXHIBIT E

COPY

RELEASE OF RESTRICTIONS

GRANTOR: Frederick A. Hall, Jr. ("Grantor")
Charlene Hall ("Grantor")
72 Franklin Street
Lewiston, Maine 04240

GRANTEE: President and Trustees of Bates College (the "College")
2 Andrews Road
Lewiston, Maine 04240

RECITALS

1. The Grantor is the owner of the real estate located at 85 Bardwell Street, Lewiston, Maine, and described in the deed recorded in the Androscoggin County Registry of Deeds in Book 899, Page 501 (the "Grantor's Lot").
2. The College is the owner of the real estate in Lewiston, Maine, located at the following addresses and recorded in the Androscoggin County Registry of Deeds in the following Books and Pages: 86 Franklin Street (Book 2023, Page 250 and part of Book 2258, Page 210); 85 and 93 Franklin Street (Book 3014, Page 180); 75 Franklin Street (Book 3324, Page 338); 63 Franklin Street (Book 8301, Page 126); 100 Central Avenue (Book 1404, Page 350), the abutting ten feet of 106 Central Avenue (part of Book 1405, Page 7); 101 Bardwell Street (Book 7913, Page 226); and 93 Bardwell Street (Book 8080, Page 64); (the "College's Lots").
3. The following restrictions (the "Restrictions") are included in the chains of title to the Grantor's Lot and the College's Lots:
 - No building other than a dwelling house for occupancy of not more than [one or] two families and only such garage or other outbuildings as may be appropriate for or incidental to the use of the occupants of such dwelling house shall be built on the lot hereby conveyed: No such house shall be occupied by more than [one or] two families and no such house or buildings shall be built for, or used in whole or in part, as a store or for business purposes.*
4. The Grantor and the College have agreed to terminate the Restrictions and release the Grantor's Lot and the College's Lots from the Restrictions.

STATE OF MAINE
COUNTY OF ANDROSCOGGIN, SS.

Personally appeared before me this 19th day of February 2014, the above-named A. Clayton Spencer President of President and Trustees of Bates College and acknowledged the foregoing instrument to be his/her free act and deed in said capacity.

Claire B Schmall

Notary Public/Maine Attorney-at-Law

Print/type name: CLAIRE B SCHMALL

My commission expires: February 24, 2019

SEAL

ANDROSCOGGIN COUNTY
TINA M CHOUINARD
REGISTER OF DEEDS

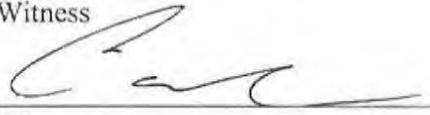
NOW, THEREFORE, in consideration of the premises, the Grantor and the College agree as follows:

1. The Grantor releases to the College all of the Grantor's right, title and interest in the College's Lots, including any right to enforce the Restrictions.
2. The College releases to the Grantor all of the College's right, title and interest in the Grantor's Lot, including any right to enforce the Restrictions.
3. The Grantor and the College agree that the Restrictions are terminated and no longer of any force or effect.
4. The College agrees that its use of the College's Lots shall be limited to uses permitted of "Academic Institutions" by the Lewiston Zoning and Land Use Code.
5. The terms of this instrument are binding upon the heirs, successors and assigns of the parties hereto.

IN WITNESS WHEREOF, the Grantor and the College have signed and sealed or caused this instrument to be signed and sealed this 20th day of February, 2014.



Witness



Witness

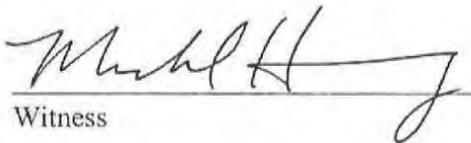


Philip Laperriere



Nathalie Nepveu

PRESIDENT AND TRUSTEES OF
BATES COLLEGE



Witness

By: 

Its: PRESIDENT
Print/type name: A. CLAYTON SPENCER

RELEASE OF RESTRICTIONS

COPY

GRANTOR: Selma A. Nelson ("Grantor")
101 Franklin Street
Lewiston, Maine 04240

GRANTEE: President and Trustees of Bates College (the "College")
2 Andrews Road
Lewiston, Maine 04240

RECITALS

1. The Grantor is the owner of the real estate located at 101 Franklin Street, Lewiston, Maine, and described in the deed recorded in the Androscoggin County Registry of Deeds in Book 875, Page 248 and in Book 1904, Page 169 (the "Grantor's Lot").
2. The College is the owner of the real estate in Lewiston, Maine, located at the following addresses and recorded in the Androscoggin County Registry of Deeds in the following Books and Pages: 86 Franklin Street (Book 2023, Page 250 and part of Book 2258, Page 210); 85 and 93 Franklin Street (Book 3014, Page 180); 75 Franklin Street (Book 3324, Page 338); 63 Franklin Street (Book 8301, Page 126); 100 Central Avenue (Book 1404, Page 350), the abutting ten feet of 106 Central Avenue (part of Book 1405, Page 7); 101 Bardwell Street (Book 7913, Page 226); and 93 Bardwell Street (Book 8080, Page 64); (the "College's Lots").
3. The following restrictions (the "Restrictions") are included in the chains of title to the Grantor's Lot and the College's Lots:

No building other than a dwelling house for occupancy of not more than [one or] two families and only such garage or other outbuildings as may be appropriate for or incidental to the use of the occupants of such dwelling house shall be built on the lot hereby conveyed: No such house shall be occupied by more than [one or] two families and no such house or buildings shall be built for, or used in whole or in part, as a store or for business purposes.

4. The Grantor and the College have agreed to terminate the Restrictions and release the Grantor's Lot and the College's Lots from the Restrictions.

STATE OF MAINE
COUNTY OF ANDROSCOGGIN, SS.

Personally appeared before me this 20th day of February, 2014, the above-named A. Clayton Spencer, President of President and Trustees of Bates College and acknowledged the foregoing instrument to be his/her free act and deed in said capacity.

Claire B Schmolli

Notary Public/Maine Attorney-at-Law

Print/type name: CLAIRE B SCHMOLLI

My commission expires: February 24, 2019

SEAL

ANDROSCOGGIN COUNTY
TINA M CHOUINARD
REGISTER OF DEEDS

F

STORM WATER

EXHIBIT F-1

STORMWATER MANAGEMENT PLAN

Bates College Student Housing Project Campus Avenue, Central Avenue, Franklin Street and Bardwell Street Lewiston, Maine

1.1 INTRODUCTION

This Stormwater Management Plan has been prepared to address the stormwater runoff impacts for stormwater quality associated with the development of the Student Housing Project.

2.1 EXISTING CONDITIONS

The 144,946-square foot (3.33 acres) area proposed for development is part of a densely-developed neighborhood comprised of a mix of college-related office buildings and single and multi-family residences and is generally bounded on the northwest by Bardwell Street, on the northeast by Campus Avenue, and on the southeast by Central Street, and includes portions of Bardwell Street and Franklin Street. The site will be subject to a variety of development scenarios, including subsurface utilities improvements, building demolition and removal, site grading, landscaping, pedestrian access improvements, vehicular maneuvering and parking improvements, and the construction of two buildings.

The existing development site consists of:

- 26,157 square feet of existing parking/walkways;
- 20,982 square feet of buildings.
47,139 square feet of impervious area, and
- 97,807 square feet of lawn; & trees/lawn
97,807 square feet of pervious area

The stormwater management plan will address the stormwater quality treatment for the site. In the proposed condition the site will drain to the City's stormdrain system which outlets to the Androscoggin River. Because the outfall of the storm drain system is the Androscoggin River a waiver from the flooding standard. We will satisfy the City of Lewiston that the flows we are

The proposed development will consist of:

- 23,822 square feet of structures;
- 56,771 square feet of walkways; paved access and parking
80,593 square feet of impervious, and

- 64,886 square feet of lawns and landscaped areas;
64,886 square feet of pervious area.

The existing drainage characteristics of the portion of the site runoff that flows to the combined sewer will be altered in the developed condition where the site runoff will be routed through the stormwater management areas and directed to the separated storm drain system. Treatment of the stormwater runoff will be achieved in part by directing runoff from the site to several roof drip strip filters, underdrained grassed soil filters, bio-retention filters and subsurface filters.

3.1.1 Alteration to Land Cover

In general terms the project will replace existing roof areas with an equivalent overall roof area and the overall paved surface of the site will be increased to accommodate increased pedestrian circulation and volume on the site and to accommodate parking and receiving functions of the new uses.

3.1.2 Downstream Waterbodies

Surface runoff from the development is directed into the City's storm drain system or combined system. The storm drain system discharges into the Androscoggin River while flows in the combined system are treated at the municipal sewage treatment facility.

- D. Urban Impaired Stream Standard: Stormwater from this project is not tributary to an "Urban Impaired Stream" as defined by MDEP Chapter 502 and, therefore is not subject to the Urban Impaired Stream Standards.
- E. Flooding Standard: As a modification of an existing SLDP, the proposed project must meet the Flooding Standard. As mentioned above, the site will discharge to the Androscoggin River, defined as a Major River Segment under Chapter 500. The applicant is requesting a waiver of the flooding standard under Chapter 500 Section 4. E. (2) (a). A statement from the City of Lewiston is being pursued to allow the discharge to their separated storm drainage system.

4.2 WATER QUALITY TREATMENT

4.2.1 General Considerations

Stormwater quality treatment for the project will be provided through a variety of stormwater treatment measures. The measures consist of roof line drip strips, bio-retention areas, underdrained soil filters and porous pavement with underground filters. The measures are located throughout the site and capture runoff from relatively small watersheds. Generally the building runoff is captured and treated through roof line drip strips and bio-retention cells around the perimeter of the buildings. Runoff from the walkways and landscaped areas are treated by bio-retention cells down gradient of these areas and the parking areas are treated by underdrained soil filters and through porous pavement with underground filters. The treatment systems and the associated contributing drainage areas are shown on the attached Post Development Stormwater Plan. The details for the treatment systems are shown on the attached plan set.

4.2.2 BMP Design Considerations

- A. The underdrained soil filters were designed and sized in accordance with "Section 7.1.3 General Design Criteria", of MDEP Volume III. BMP Technical Design Manual:
- a. Treatment Volume - A grassed underdrained soil filter (USF) must detain and filter a runoff volume equal to 1.0 inch times the subcatchment's impervious area plus 0.4 inch times the subcatchment's landscaped developed area.

D. The porous pavement with underground filters were designed and sized in accordance with “Section 7.7.4 Design Criteria for Storage and Filtration”, of MDEP Volume III. BMP Technical Design Manual:

- a. Treatment Volume - a minimum storage capacity within the filter layer or subbase and base layers shall be a runoff volume equal to 1.0 inch times the subcatchment's impervious area plus 0.4 inch times the subcatchment's landscaped developed area.
- b. Filter Media – the filter media must be a mineral soil with between 4% and 7% fines and must be a minimum of 4 inches thick.
- c. Storage Volume: The stored volume must be fully drained within 24 to 48 hours.

4.2.3 Required and Proposed Project Stormwater Runoff Quality Treatment

1. As it applies to the redeveloped portion of the project site, the General Standards require that the stormwater management system control runoff from no less than 95% of the impervious area and no less than 80% of the developed area that is impervious or landscaped;

The following **Table 1** shows the treatment measures with their drainage areas, treatment volumes and required treatment volumes and demonstrates that each of the proposed treatment measures is sized appropriately to accommodate the stormwater from the site.

The following **Table 2** tabulates the site areas and breaks out the impervious and vegetated by treated and non-treated to determine the percentages of treated areas.

Table 2 Treatment Percentages for the Site

	55 Campus	65 Campus	Overall Site
Overall Area	66,960	78,519	145,479
Proposed Impervious Area	39,871	40,722	80,593
Treated Impervious Area	37,900	39,135	77,035
% Impervious Area Treated	95.1	96.1	95.6
Proposed Vegetated Area	27,089	37,797	64,886
Treated Vegetated Area	22,840	34,884	57,724
% Vegetated Area Treated	84.3	92.3	89.0

By incorporating the twenty six treatment measures into the project's stormwater management system, the required standard of 80% treatment of the project's developed area is exceeded, by achieving 89.0% treatment, and the required standard of 95% treatment of the project's impervious area is exceeded, by achieving 95.6% treatment.

4.3 RUNOFF ANALYSIS

The calculations for the 2, 10 and 25 year storm events have been performed and are attached to this report. The attached Table A compares the pre, post and differences for the study points at the combined sewers and at the storm drain system for the City. The table shows the reduction in flows to the combined system and the increased flows to the separated storm drain system. In general terms, in the post development condition the stormwater runoff in Campus Ave remains

EXHIBIT F-2

BATES COLLEGE RESIDENTIAL HOUSING PROJECT LEWISTON MAINE

TABLE A
STORMWATER FLOW RATE COMPARISONS
PREDEVELOPMENT VS POST DEVELOPMENT
July 21, 2014

Study Points	2-Year Storm			10-Year Storm			25-Year Storm		
	Pre (cfs)	Post (cfs)	Diff. (cfs)	Pre (cfs)	Post (cfs)	Diff. (cfs)	Pre (cfs)	Post (cfs)	Diff. (cfs)
Campus Ave. CS at CB F – removed in post condition & new CB 31 installed & connected to Campus Ave. SD	0.50	0	-0.50	0.81	0	-0.81	0.96	0	-0.96
Campus Ave. CS at CB H – removed in post condition & new CB 34 installed & connected to Campus Ave. SD	0.27	0	-0.27	0.48	0	-0.48	0.59	0	-0.59
Total change to Campus Ave. CS	0.77	0	-0.77	1.29	0	-1.29	1.55	0	-1.55
Campus Ave. CS at CB F - removed in post condition & new CB 31 installed & connected to SD	0	0.32	+0.32	0	0.71	+0.71	0	0.90	+0.90
Campus Ave. CS at CB H – removed in post condition & new CB 34 installed & connected to Campus Ave. SD	0	0.19	+0.19	0	0.32	+0.32	0	0.39	+0.39
Campus Ave. SD at CB I - removed in post condition & new CB 36 installed & connected to Campus Ave. SD	0.12	0.32	+0.20	0.21	0.52	+0.31	0.25	0.61	+0.36
Total change to Campus Ave. SD	0.12	0.83	+0.71	0.21	1.55	+1.34	0.25	1.90	+1.65
Net change to Campus Ave.	-	-	-0.06	--	-	+0.05	-	-	+0.10

G

EROSION

EXHIBIT G-1

SECTION 14 BASIC STANDARDS

14.0 Basic Standards

A. Narrative

A narrative addressing the measures and practices for the project's erosion and sedimentation control measures has been attached to this section. Also included in this section as attachments to the erosion and sedimentation control measures are the maintenance and housekeeping practices for the stormwater facilities.

B. Implementation Schedule

The implementation schedule is hinged on the construction start date and a detailed schedule from that date is provided within the Erosion and Sediment Control Notes provided on the plans.

C. Plan of Existing Conditions and Plan of Proposed Conditions

See drawings C-1.2 for the existing conditions and C-2.0 depicting the proposed site features.

D. Plan of Locations of Erosion Control BMPs

See drawing C-2.3 depicting the locations of all proposed temporary and permanent erosion control measures to be installed on the site.

E. Plan of Limits of Areas to be Disturbed by Construction

See drawing C-2.1, Proposed Grading and Drainage Plan depicting the limits of areas to be disturbed by construction activities.

F. Plan of Details and Specifications of Erosion and Sediment Control Measures

See drawing C-3.2 for Notes and Details of Erosion and Sediment Control Measures.

G. Design Calculations

The erosion and sediment control measures to be implemented have been designed considering the peak flows presented in "Section 12, Stormwater Management" of this application and in accordance with the guidelines presented in the Maine Stormwater Best Practices Manual (BMPs).

H. Third Party Inspection – The installation of the stormwater management system will be monitored by an engineer for compliance with the system design. The engineer will issue a report on the installation and the City will review the report prior to issuing a certificate of occupancy.

underdrain piping shall be cleaned or replaced prior to placement of filter media to ensure that silts accumulated during construction do not remain in the storm system.

1.3 Post-construction phase

Once the site has reached permanent stabilization, remove any temporary sediment control measures, such as silt fence, within 30 days. All accumulated sediment/debris in the permanent stormwater management system, ditches, swales, paved surfaces, and/or any other location that has accumulated sediment/debris during construction shall be removed and disposed of in an approved manner.

2.0 PERMANENT STABILIZATION

The strategies outlined in this E&S Plan shall be in effect until the site reaches permanent stabilization. Newly seeded or sodded areas must be protected from vehicle traffic, excessive pedestrian traffic, and concentrated runoff until the vegetation is well established. If necessary, areas must be seeded and mulched again if germination is sparse, plant coverage is spotty, or topsoil erosion is evident. The following list defines permanent stabilization for applicable situations.

- 2.1 **Seeded Areas:** For seeded areas, permanent stabilization means a 90% cover of vigorous perennial growth with no evidence of washing or rilling of the topsoil.
- 2.2 **Sodded Areas:** For sodded areas, permanent stabilization means the complete binding of the sod roots into the underlying soil with no slumping of the sod or die-off.
- 2.3 **Permanent Mulch:** For mulched areas, permanent mulching means total coverage of the exposed area with an approved mulch material. Erosion control mix may be used as mulch for permanent stabilization according to approved application rates and limitations.
- 2.4 **Riprap:** For areas stabilized with riprap, permanent stabilization means that slopes stabilized with riprap have an appropriate backing of well-graded gravel or approved geotextile to prevent soil movement from behind the riprap.
- 2.5 **Paved Areas:** For paved areas, permanent stabilization means the placement of compacted gravel subbase is completed.
- 2.6 **Ditches, channels, and swales:** For open channels, permanent stabilization means the channel is stabilized with a 90% cover of vigorous perennial growth, a well-graded riprap lining, or with another non-erosive lining such as specified. There must be no evidence of slumping of the channel lining, undercutting of the channel banks, or down-cutting of the channel.

3.0 TEMPORARY EROSION AND SEDIMENTATION CONTROL BMPS

The placement/use of the following erosion and sedimentation control measures shall be in accordance with the "Maine Erosion and Sediment Control BMPS" manual published by the Maine Department of Environmental Protection (MDEP) dated March 2003, as revised.

- 3.1 **Sediment Barriers:** Prior to the beginning of any construction, sediment barriers (i.e. silt fence, erosion control mix berms, etc...) shall be installed across the slope(s), on the contour, at or just below the limits of clearing or grubbing, and/or just above any adjacent property line or watercourse to protect against construction related erosion. Sediment barriers shall be maintained until all tributary open areas have been permanently stabilized. The following are recommended perimeter sediment barriers:

or washed to further minimize the tracking of mud, dust or debris from the construction area. When washing is required, it shall be done on an area stabilized with aggregate, which drains into an approved sediment trapping device. Stabilized construction exits shall be constructed in areas as specified and detailed on the plans.

- 3.5 **Stone Check Dams:** Stone check dams are generally temporary devices, which are constructed across a swale or drainage ditch. Their purpose is to reduce the velocity of concentrated stormwater flows, thereby reducing erosion of the swale or ditch. These devices will also trap small amounts of sediment generated in the ditch itself, however, they are not an effective sediment trapping device and should not be used as such. Stone check dams are typically constructed of 2"-3" crushed stone and stand 24 inches in height.
- 3.6 **Storm Drain Inlet Protection:** Storm drains are typically operational prior to permanent stabilization of tributary areas. In these instances hay bales, crushed stone barriers, and/or silt sacks shall be used within a catch basin or prior to a pipe entrance. This temporary protection will assist in the removal of sediment prior to entrance into a storm drainage system and the prevention of clogging and/or loss of capacity. These devices alone will not prevent all sediment from entering the stormwater system and should be used in conjunction with other devices to achieve desired sediment removal levels.
- 3.7 **Dewatering:** Water from construction dewatering will pass first through a filter bag or secondary containment structure (e.g. hay bale lined pool) prior to discharge. The discharge site shall be selected to avoid flooding, icing and sediment discharges to a protected natural resource. Discharge is permitted within the filter basin locations prior to the installation of the filter media.
- 3.8 **Dust Control:** Dust control during construction shall be achieved by the use of a watering truck to periodically sprinkle the exposed roadway areas as necessary to reduce dust during the dry months. Applying other dust control products such as calcium chloride or other manufactured products are allowed if authorized by the proper local, state and/or federal regulating agencies. However, it is the contractor's ultimate responsibility to mitigate dust and soil loss from the site.

4.0 VEGETATIVE MEASURES

- 4.1 **Temporary Vegetation:** If any disturbed area of soil will be left bare for more than 7 days, or if construction is to be completed in phases over an extended duration, temporary seeding and mulching shall commence immediately following initial fine grading of the site. In sensitive areas (within 75 feet of protected natural resources) temporary mulch must be applied within 48 hours or prior to any storm event on all disturbed surfaces. It shall be maintained and reseeded, as necessary, to ensure good vegetative cover for the entire duration of construction. Seed will be selected from the following table (Table 1 - Temporary Seed Mixture) according to the time of year or via an approved equivalent method.

**TABLE 1
TEMPORARY SEED MIXTURE**

Seed	Lbs./Acre	Lbs./1000s.f.	Recommended Seeding Date
Winter Rye	112	2.6	8/15 thru 10/1
Oats	80	1.8	4/1 thru 7/1 8/15 thru 9/15
Annual Ryegrass	40	0.9	4/1 thru 7/1
Sudangrass	40	0.9	5/15 thru 8/15
Perennial	40	0.9	8/15 thru 9/15

Note:

fertilizer may be applied simultaneously with the seed. Recommended seeding rates must be increased by 10% when hydroseeding.

- **Surface Stabilization:** Mulching or other approved surface stabilization methods shall commence immediately after seed is applied. Refer to the surface stabilization section of this plan for more information.

4.2.3. Sodding

Following seedbed preparation, sod can be applied in lieu of seeding in areas where immediate vegetation is most beneficial such as ditches, around stormwater drop inlets and areas of aesthetic value. Sod should be laid at right angles to the direction of flow starting at the lowest elevation. Sod should be rolled or tamped down to even out the joints once laid down. Where flow is prevalent the sod must be properly anchored down. Irrigate the sod immediately after installation. In most cases, sod can be best established between April 1 and November 15 of the construction year.

5.0 WINTER CONSTRUCTION

The winter construction period is from November 1 through April 15. If the construction site is not permanently stabilized by November 15 then the site needs to be protected with over-winter stabilization.

Winter excavation and earthwork shall be completed such that no more than 1 acre of the site is without stabilization at any one time. Limit the exposed area to those areas in which work is expected to be under taken during the proceeding 15 days and that can be mulched in one day prior to any snow event. All areas shall be considered to be denuded until the subbase gravel is installed in roadway areas or the areas of future loam and seed have been loamed, seeded and mulched.

Any added measures, which may be necessary to control erosion/sedimentation from the site dependent upon the actual site and weather conditions, must be installed. Continuation of earthwork operations on additional areas shall not begin until the exposed soil surface on the area being worked has been stabilized, in order to minimize areas without erosion control protection.

5.1 Winter Construction BMP Adjustments

- 1) **Sediments Barriers:** During frozen conditions, sediment barriers shall consist of erosion control mix berms as frozen soil prevents the proper installation of hay bales and silt fences.
- 2) **Mulching:** Between the dates of November 1 and April 15, all mulch shall be anchored by either mulch netting, asphalt emulsion chemical, track or weed cellulose fiber. When the ground surface is not visible through the mulch then cover is sufficient. After November 1st, mulch and anchoring of all exposed soil shall occur at the end of each final grading workday.
 - **Open Surfaces (flatter than 8%):** Hay and straw mulch shall be applied at a rate of 150 lb. per 1,000 square feet or 3 tons/acre (twice the normal accepted rate of 75-lbs./1,000 square feet or 1.5 tons/acre) and shall be properly anchored. Mulch shall not be spread on top of snow. The snow will be removed down to one-inch depth or less prior to application. After each day of final grading, the area will be properly stabilized with anchored hay or straw or erosion control matting. An area shall be considered to have been stabilized when exposed

- **Install a sod lining in the ditch:** A ditch must be lined with properly installed sod by October 1. Proper installation includes the contractor pinning the sod onto the soil with wire pins, rolling the sod to guarantee contact between the sod and underlying soil, watering the sod to promote root growth into the disturbed soil, and anchoring the sod with jute or plastic mesh to prevent the sod strips from sloughing during flow conditions.
 - **Install a stone lining in the ditch:** A ditch must be lined with stone riprap by November 15. A registered professional engineer must be hired to determine the stone size and lining thickness needed to withstand the anticipated flow velocities and flow depths within the ditch. If necessary, the ditch must be regraded prior to placing the stone lining to prevent the stone lining from reducing the ditch's cross-sectional area.
- 2) **Disturbed Slopes:** All stone-covered slopes must be constructed and stabilized by November 15. All slopes to be vegetated must be seeded by September 15. The MDEP will consider any area having a grade greater than 15% (10H:1V) to be a slope. If a slope to be vegetated is not stabilized by September 1, then one of the following actions must be taken to stabilize the slope for late fall and winter.
- **Stabilize the soil with temporary vegetation and erosion control blankets:** By October 1 the disturbed slope must be seeded with winter rye at a seeding rate of 3 pounds per 1,000 square feet and apply erosion control blankets over the mulched slope. If the rye fails to grow at least three inches or cover at least 90% of the disturbed slope by November 1, the slope will be covered with a layer of erosion control mix or stone riprap as described in the following standards.
 - **Stabilize the slope with sod:** The disturbed slope must be stabilized with properly installed sod by October 1. Proper installation includes pinning the sod onto the slope with wire pins, rolling the sod to guarantee contact between the sod and underlying soil, and watering the sod to promote root growth into the disturbed soil. Slopes steeper than 33% (3H:1V) or having groundwater seeps on the slope face, may not use late-season sod installation for stabilization.
 - **Stabilize the slope with erosion control mix:** A six-inch layer of erosion control mix must be spread over the slope by November 15. Prior to placing the erosion control mix, any snow accumulation on the disturbed slope must be removed. Slopes steeper than 50% (2H:1V) or having groundwater seeps on the slope face can not use erosion control mix to stabilize slopes.
 - **Stabilize the slope with stone riprap:** A layer of stone riprap can be placed on the slope by November 15. A registered professional engineer must be hired to determine the stone size needed for stability and to design a filter layer for underneath the riprap.
- 3) **Other Disturbed Soils:** By September 15, all disturbed soils on areas having a slope flatter than 15% (10H:1V) must receive seed and mulch. If disturbed areas are not stabilized by this date, then one of the following actions must be taken to stabilize the soil for late fall and winter.

EXHIBIT G-3

INSPECTION, MAINTENANCE AND HOUSEKEEPING PLAN

Bates College
55 & 65 Campus Avenue Housing
Lewiston, Maine

Introduction

The following plan outlines the anticipated inspection and maintenance procedures for the erosion and sedimentation controls as well as stormwater management devices for the project site. Also, this plan outlines several housekeeping requirements that shall be followed during and after construction. These procedures should be followed in order to ensure the intended function of the designed measures and to prevent unreasonable adverse impacts to the surrounding environment.

The procedures outlined in this inspection and maintenance plan are provided as an overview of the anticipated practices to be used on this site. In some instances, additional measures may be required due to unexpected conditions. For additional detail on any of the erosion and sedimentation control measures or stormwater management devices to be utilized on this project, refer to the most recently revised edition of the "Maine Erosion and Sediment Control BMPs" manual and/or the "Maine Stormwater Best Practices Manual (BMPs)" manual as published by the Maine Department of Environmental Protection (MDEP).

During Construction

1. **Inspection:** During the construction process, it is the Contractor's responsibility to comply with the inspection and maintenance procedures outlined in this section. These responsibilities include inspecting disturbed and impervious areas, erosion control measures, material storage areas that are exposed to precipitation, and locations where vehicles enter or exit the site. These areas shall be inspected at least once a week as well as before and after a storm event, and prior to completing permanent stabilization measures. A person with knowledge of erosion and stormwater control, including the standards and conditions in any applicable permits, shall conduct the inspections.
2. **Maintenance:** All measures shall be maintained in an effective operating condition until areas are permanently stabilized. If Best Management Practices (BMPs) need to be maintained or modified, additional BMPs are necessary, or other corrective action is needed, implementation must be completed within 7 calendar days and prior to any storm event (rainfall).
3. **Documentation:** A log summarizing the inspections and any corrective action taken must be maintained on-site. The log must include the name(s) and qualifications of the person making the inspections, the date(s) of the inspections, and major observations about the operation and maintenance of erosion and sedimentation controls, material storage areas, and vehicle access points to the site. Major observations must include BMPs that need maintenance, BMPs that failed to operate as designed or proved inadequate for a particular location, and locations where additional BMPs are needed. For each BMP requiring maintenance, BMP needing

- The exit shall be maintained in a condition that will prevent tracking of sediment onto public rights-of-way.
- When the control pad becomes ineffective, the stone shall be removed along with the collected soil material. The entrance should then be reconstructed.
- Areas that have received mud-tracking or sediment deposits shall be swept or washed. Washing shall be done on an area stabilized with aggregate, which drains into an approved sediment-trapping device (not into storm drains, ditches, or waterways).
- Temporary Seed and Mulch:
 - Mulched areas should be inspected after rain events to check for rill erosion.
 - If less than 90% of the soil surface is covered by mulch, additional mulch shall be applied in bare areas.
 - In applications where seeding and mulch have been applied in conjunction with erosion control blankets, the blankets must be inspected after rain events for dislocation or undercutting.
 - Mulch shall continue to be reapplied until 95% of the soil surface has established temporary vegetative cover.
- Stabilized Temporary Drainage Swales:
 - Sediment accumulation in the swale shall be removed once the cross section of the swale is reduced by 25%.
 - The swales shall be inspected after rainfall events. Any evidence of sloughing of the side slopes or channel erosion shall be repaired and corrective action should be taken to prevent reoccurrence of the problem.
 - In addition to the stabilized lining of the channel (i.e. erosion control blankets), stone check dams may be needed to further reduce channel velocity.

After Construction

1. **Inspection:** After construction, it is the responsibility of the owner or assigned heirs to comply with the inspection and maintenance procedures outlined in this section. All measures must be maintained in effective operating condition. A person with knowledge of erosion and stormwater control, including the standards and conditions in all applicable permits, shall conduct the inspections.
2. **Specific Inspection and Maintenance Tasks:** The following is a list of permanent erosion control and stormwater management measures and the inspection and maintenance tasks to be performed after construction.
 - A. Vegetated Areas:
 - Inspect vegetated areas, particularly slopes and embankments, early in the growing season or after heavy rains to identify active or potential erosion problems.

E. Bio-Retention Soil Filters

- Check the impoundment embankments for sloughing or erosion. The vegetation should be well established and maintained. Remove any trees or shrubs growing in the impoundment or on the inside of the pond embankments.
- Check the outlet control structure for sediment accumulation or other blockages.
- Look for sediment deposits in the sediment forebays or along edges of the pond where stormwater may enter into the soil filter. If the sediments leave less than 1 foot to the top of the berm between the forebay and the impoundment, remove the sediments and reline the forebay with stones. If the sediments extend more than 1/8 of the width of the soil filter, remove the sediments and restore the vegetation.
- Monitor the time it takes for the stormwater takes to drain from the bio-retention area. For a 1 inch or larger rainfall event, the pond should be drained in 24 to 48 hours. If the timeframe to drain the pond approaches 48 hours, the filter media should be reworked or replaced.

F. Roof Drip Strips

- Check the drip strips for sloughing or erosion. The stone surface should be free from debris and sediments. Remove any debris accumulation on and vegetation growing in the drip strip.
- Check the outlet control structure for sediment accumulation or other blockages.
- Look for sediment deposits in the drip strips and remove stone and sediments and replace stone to the require depth.
- Monitor the time it takes for the stormwater takes to drain from the drip strip. For a 1 inch or larger rainfall event, the pond should be drained in 24 to 48 hours. If the timeframe to drain the pond approaches 48 hours, the filter media under the stone should be reworked or replaced.

G. StormCrete Porous Pavement

- Check the surface of the porous pavement for accumulation of fines and debris.
- Twice per year, vacuuming of the porous pavement surface with a truck mounted regenerative air or vacuum sweeper equipment.
- Sweep the entire parking lot in the spring to remove winter sand from the surface of the lot and contributing drainage area.
- Twice per year, observe the performance of the porous pavement during a rainfall event to see if the flow is assimilated into the porous pavement surface. Check for accumulation of sediments near the catch basin inlet for the

EXHIBIT G-4

Stormwater Inspection and Maintenance Form

Bates College
Lewiston, Maine

Bates College 55 Campus Ave.

Inspector: _____

Date: _____

Maintenance Personnel: _____

SITE AREA	Inspection		Maintenance	
	Date	Comments	Date	Corrective Action
Roofline Drip Strips at Building Perimeter				
Rain Garden Adjacent to Walkway				
Underdrain Soil Filters Along Bardwell Street				
Underdrain Filter Systems- Along Franklin Street				
StormCrete Porous Pavement				
Catch Basins/Manholes				
Overflow Structures				
Vegetated areas adjacent to pavement runoff				
Lawn Vegetation				
Slope Vegetation				
Parking Lot Sweeping				

Sediment/Debris Disposal

Location of Sediment/Debris Accumulation	Location of Sediment/Debris Disposal	Date of Disposal

H

**TRANSPORTATION AND
PARKING ASSESSMENT**

EXHIBIT H

**Transportation and Parking Assessment
Bates College Housing Project
Lewiston, Maine**

Prepared for:

Bates College

July 2014

Prepared by:

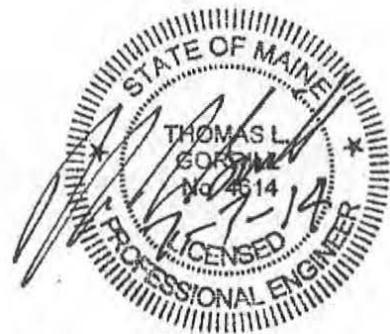


Gorrill-Palmer Consulting Engineers, Inc.

Traffic and Civil Engineering Services

PO Box 1237
15 Shaker Road
Gray, ME 04039

(207) 657-6910
Fax: (207) 657-6912
E-mail: mailbox@gorrillpalmer.com



Executive Summary

The following Executive Summary is prepared for the reader's convenience, but is not intended to be a substitute for reading the full report. Gorrill-Palmer Consulting Engineers, Inc. has been retained by Bates College to prepare this transportation and parking assessment for the proposed residential housing project on the corner of Campus Ave and Central Avenues.

The following is a summary of the findings and conclusions of the assessment:

1. The project is relocating the existing campus store and student mail/package center as well as replacing existing housing units from several locations on campus. The project will not increase enrollment. The proposed project is estimated to redistribute 61 AM and 67 PM peak hour vehicle trip ends from other locations on campus to this site. Since there is no anticipated increase in trips on the campus, in our opinion this level of trip generation will not require a MaineDOT Traffic Movement Permit. These trips are a redistribution of internal trips since no new trips are being generated on campus by the project.
2. The proposed project is estimated to generate 177 and 263 pedestrian trip ends in the AM and PM peak hours respectively. Similar to the vehicular trips, these trips are a redistribution of internal trips on the campus.
3. The capacity analyses shows that level of service for the study area intersections should continue to operate at a level of service A for both the AM and PM peak hours.
4. The sight lines entering and exiting the proposed driveways meet MaineDOT standards. While parking restricts sight lines at times in an urban area, we recommend that parking be restricted on the westerly side of Central from the proposed parking lot entrance a distance of 100 feet toward Campus Avenue and from the proposed parking lot southerly to the adjacent driveway a distance of 50 feet. Parking should be restricted within 20 feet of the proposed driveway on Bardwell Street.
5. Gorrill-Palmer Consulting Engineers, Inc. obtained MaineDOT Crash history to determine if there were any high crash locations in the project vicinity. Based on the summaries, there are no HCLs.
6. Bates College has carefully considered the pedestrian accommodations for the project. Bates is requesting the City approve partial discontinuance of Franklin Street as an enhanced safety measure for pedestrians. This will minimize the potential for conflicts for pedestrian traffic between the proposed buildings. The sidewalks will be reconstructed on Campus Avenue, Bardwell Street and Central Avenue with six foot esplanades. Two crosswalks are planned on Campus Avenue; one just west of Franklin Street and one just west of Bardwell Street. Curb extensions are planned for all four quadrants of the intersections of Campus Ave both Bardwell and Franklin Streets as well as crosswalk striping, signs, lighting, and handicap ramps to improve safety. Access to

Based on these findings, it is the opinion of Gorrill-Palmer Consulting Engineers, Inc. that the proposed project can be accommodated on the adjacent roadway network.

I. *Project Description*

Bates College is planning to construct two buildings to house 250 students, a 6,000 sf college store including a new bookstore and student gathering place, and a 3,700 sf mailroom. The project is replacing existing housing units at several locations on campus and will not increase enrollment. There are two buildings and associated parking proposed as part of the project: 65 Campus fronts Campus Avenue between Central and Franklin and 55 Campus also fronts Campus between Franklin and Bardwell. The existing 50 space parking lot on the site will be removed and replaced with two parking lots, one behind each building with a combined total of 69 spaces and includes 3 handicapped spaces. Spaces for 3 delivery trucks and one tractor trailer truck will be provided behind 65 Campus. Bates College is planning to close Franklin Street to thru traffic as part of the project. Curb extensions are planned on Campus Avenue at Franklin and at Bardwell to facilitate safe pedestrian crossings. A copy of the proposed site plan is included in the Appendix to this report.

II. *Traffic Assessment*

Gorrill-Palmer Consulting Engineers, Inc. based the traffic assessment on the following information:

- Two buildings to house 250 students, a 6,000 sf college store including a new bookstore and student gathering place, and a 3,700 sf mailroom. The actual number of housing units may be less than this but was used in the study to be conservative. No enrollment increase is planned as a result of the project.
- Crash data for 2010-2012 provided by the Maine Department of Transportation.
- Turning movement counts completed by our office on Thursday, April 3rd from 7:00-9:30 AM; 10:30 to 1:00 PM; 1:30 to 3:30 PM; and again from 4:00 to 6:00 PM at the following intersections:
 - Central Avenue and Campus Avenue
 - College Street and Campus Avenue
 - Campus at Franklin
 - Campus at Bardwell
 - Vale at Franklin
 - Vale at Bardwell

Seasonal Adjustment

The MaineDOT utilizes highway classifications of I, II, or III for state and local roadways. Type I roadways are defined as urban roadways, or those roads that typically see commuter traffic and experience little fluctuation from week to week throughout the year. Type II

Trip Generation

Proposed Housing - Bates College is planning to construct two buildings to house 250 students, a 6,000 sf college store including a new bookstore and student gathering place, and a 3,700 sf mailroom. The project is replacing existing housing units at several locations on campus and will not increase enrollment. The post office and bookstore are also being relocated to the site, thus there is no net trip generation increase on the Bates Campus. To estimate the vehicular traffic which will be generated at the new student housing, the Institute of Transportation Engineers (ITE), Ninth Edition, Land Use Code (LUC) – 550, University/College was utilized for the proposed housing component of the project. The trip generation was calculated based on a 250 student population. Based on this information, the forecast vehicular trip generation for the weekday peak hours of the generator is summarized as follows:

- AM Peak Hour – 42 trip ends
- PM Peak Hour – 42 trip ends

The large majority of the trips associated with the dorm will be pedestrians which will be estimated in Section III of this report, Pedestrian Circulation, by subtracting the vehicular trips estimated above from the trip generation determined for an apartment building. All students will be required to park in the Merrill lot as discussed in the Parking Management section of the report (Section IV). Thus, the figures above should be conservative (high).

Proposed Bookstore - As part of the project Bates will be relocating the existing bookstore to a 6,000 sf area on the site which will also include a student gathering area. GPCEI utilized ITE Land Use Code 820, Shopping Center, to conservatively estimate the amount of pedestrian and vehicle trip ends combined which results in an estimate of 28 and 90 trip ends in the morning and afternoon peak hours respectively. Based on discussion with campus staff and our own observations of campus environment, we anticipate virtually all these trips to be pedestrian but have utilized 10% of these totals as vehicular trips to be conservative.

Proposed Package Center Post Office - The final use to be considered in the trip generation estimate is the relocated package and post office. Bates College furnished traffic information for their existing facility in Chase Hall during the academic year which is summarized below:

- Deliveries for the package center and office services: approximately 4 per hour or 8 trip ends
- 4 tractor trailer deliveries for the package center per year
- 60 patrons during the peak hour or 120 trip ends of which only 5 per day use a vehicle to visit the center. We anticipate that virtually all the patrons will continue to arrive on foot, however we have assumed 4 vehicle trip ends.
- Office Services has approximately 4 visits an hour or 8 trip ends. We have assumed half of these to be vehicular trip ends.

The analysis was completed with the Synchro and SimTraffic analysis software. Levels of service rankings are similar to the academic ranking system where an 'A' represents little control delay and an 'F' represents significant delay. The following table summarizes the relationship between delay and level of service for an unsignalized intersection:

Level of Service Criteria for Unsignalized Intersections

Level of Service	Control Delay per Vehicle (sec)
A	Up to 10.0
B	10.1 to 15.0
C	15.1 to 25.0
D	25.1 to 35.0
E	35.1 to 50.0
F	Greater than 50.0

The following tables summarize the results of the capacity analysis. A copy of the output from the analysis is provided in Appendix B.

Level of Service – Bardwell at Campus (Unsignalized)

Approach	AM Postdevelopment	PM Postdevelopment
Campus - EB	A	A
Campus - WB	A	A
Bardwell - NB	A	A

Level of Service – Franklin at Campus (Unsignalized)

Approach	AM Postdevelopment	PM Postdevelopment
Campus - EB	A	A
Campus - WB	A	A
Driveway (Across from Franklin) SB	A	A

Level of Service – Campus at Central (Unsignalized)

Approach	AM Postdevelopment	PM Postdevelopment
Campus - EB	A	A
Campus - WB	A	A
Central - NB	A	A
Central - SB	A	A

from the proposed parking lot southerly to the adjacent driveway a distance of 50 feet. Parking should be restricted within 20 feet of the proposed driveway on Bardwell Street.

As identified, the sight lines for the driveway exceeds Maine DOT requirements. Gorrill-Palmer Consulting Engineers, Inc. recommends that all plantings, which will be located within the right of way, not exceed 3 feet in height and be maintained at or below that height. Signage should not interfere with sight lines. In addition, we recommend that during construction, when heavy equipment is entering and exiting into the site, that appropriate measures, such as signage and flag persons, be utilized in accordance with the Manual on Uniform Traffic Control Devices.

Crash Data

Gorrill-Palmer Consulting Engineers, Inc. obtained the crash data from MaineDOT for the period of 2010-2012. Crash information is attached.

In order to evaluate whether a location has a crash problem, MaineDOT uses two criteria to define High Crash Location (HCL). Both criteria must be met in order to be classified as an HCL.

1. A critical rate factor of 1.00 or more for a three-year period. (A Critical Rate Factor {CRF} compares the actual crash rate to the rate for similar intersection in the state. A CRF of less than 1.00 indicates a rate of less than average) and;
2. A minimum of eight crashes over the same three-year period.

Based on the information provided by MaineDOT, there are no high crash locations within the immediate area of the site. However, the intersection of Campus and Central Avenues did have six collisions with a critical rate factor of 2.09. The intersection is controlled by stop signs on each approach (4-way stop). While this intersection is not a HCL, Gorrill Palmer Consulting Engineers, Inc. prepared a collision diagram which is included with this report. The diagram shows 3 collisions occurred in 2010, 1 in 2011 and 2 in 2012. All collisions occurred as a result of failure to stop by one of the vehicles. The City may want to consider installation of a flashing beacon to supplement the stop signs.

III. *Pedestrian Circulation*

Bates College is a pedestrian campus and supports policies to minimize the number of cars on campus. The proposed project will generate significant pedestrian traffic who will be crossing Campus Avenue throughout the day.

safety measure for pedestrians. This will minimize the potential for conflicts for pedestrian traffic between the proposed buildings. The sidewalks will be reconstructed on Campus Avenue, Bardwell Street and Central Avenue with six foot esplanades. Two crosswalks are planned on Campus Avenue; one just west of Franklin Street and one just west of Bardwell Street. Curb extensions are planned for all four quadrants of the intersections of Campus Ave both Bardwell and Franklin Streets as well as crosswalk striping, signs, lighting, and handicap ramps. Access to the buildings is planned to channelize pedestrians to the crosswalks and the esplanades are intended to assist in this effort.

In alignment with the Cities of Lewiston Auburn Complete Streets Policy and Lewiston's Bicycle-Pedestrian Committee, we recommend that Campus Ave be signed and striped for sharing the roadway with bicyclists since there is not adequate width to accommodate both parking and bike lanes. We recommend that Central Avenue be re-striped to accommodate 4 foot bike lanes with appropriate pavement markings.

IV. Parking

There are currently 50 off street spaces on the site for the existing uses. The proposed redevelopment of the site will result in an additional 19 off street spaces and a net loss of 6 spaces on street. A summary of the parking additions and losses associated with the project is presented below:

Affected Area	Predevelopment	Postdevelopment	Net change
So side of Campus Ave	17	22	5
Franklin Street	24	12	-12
Bardwell Street	15	21	6
Central	13	7	-6
On site parking	50	69	19
Total on and off Street	119	131	12

The on-site parking is planned to be reserved for faculty and staff. Students will be required to park at the Merrill parking lot and will be informed of this prior to their arrival on Campus. The *Bates College Parking Study* completed in June of 2014, found that the on campus parking supply was 930 which will increase to 949 upon completion of this project. The total parking demand was determined to be 1053 during the fall and 911 during the winter. While the fall demand exceeds the off street parking, by 104 spaces during the winter months, these additional vehicles can be easily accommodated on street. The study did find that while there are local shortage of supply, such as in the vicinity of the Olin Center, that the Merrill lots, which have 334 spaces, are generally no more than half full. The study concluded that localize parking shortages should be addressed through implementation of a parking management plan and formation of a transportation committee prior to constructing additional parking since additional parking increases storm water runoff, removes vegetation, and is inconsistent with Bates initiative to be a green campus.

**PRELIMINARY GEOTECHNICAL
ENGINEERING SERVICES**

EXHIBIT I

REPORT

February 27, 2014
13-0500 S

Preliminary Geotechnical
Engineering Services
Proposed Student Housing
South of Campus Avenue
Bates College Campus
Lewiston, Maine

PREPARED FOR:

Bates College – Physical Plant
Attention: Chris Streifel
147 Russell Street
Lewiston, Maine 04240

PREPARED BY:

S.W.COLE ENGINEERING, INC.
286 Portland Road
Gray, Maine 04039
207-657-2866



S.W.COLE
ENGINEERING, INC.

- *Geotechnical Engineering*
- *Construction Materials Testing*
- *GeoEnvironmental Services*
- *Ecological Services*

www.swcole.com

13-0500 S

February 27, 2014

Bates College – Physical Plant
Attention: Chris Streifel
147 Russell Street
Lewiston, Maine 04240

Subject: Preliminary Geotechnical Engineering Services
Proposed Student Housing
South of Campus Avenue
Bates College Campus
Lewiston, Maine

Dear Chris:

In accordance with our Agreement, dated November 8, 2013, we have performed subsurface explorations for the subject project in Lewiston, Maine. This report summarizes our findings and preliminary geotechnical recommendations relative to foundations and earthwork associated with the proposed construction. The contents of this report are subject to the limitations set forth in Attachment A.

1.0 INTRODUCTION

1.1 Scope and Purpose

The purpose of our services was to obtain subsurface information at the site in order to develop preliminary geotechnical recommendations relative to foundations and earthwork associated with the proposed construction. Our scope of services included the making of seven test boring explorations, soils laboratory testing, a geotechnical analysis of the subsurface findings and preparation of this report.

1.2 Proposed Construction

Based on the information provided to us, we understand that Bates College is planning an expansion of the campus south of Campus Avenue. The expansion is proposed in an area of five blocks generally bound by Campus Avenue, Central Avenue, Vale

GPS. Several exploration locations were subsequently reestablished using taped measurements from existing features due to equipment access. The approximate exploration locations are shown on the "Exploration Location Plan" attached as Sheet 1. Logs of the explorations are attached as Sheets 2 through 14. A key to the notes and symbols used on the logs is attached as Sheet 15. Elevations noted on the logs were estimated based on topographic information shown on Sheet 1.

2.2 Testing

The borings were performed using a combination of hollow-stem auger, cased wash-boring and rod probing techniques. The soils were sampled at 2 to 5 foot intervals using Standard Penetration Test (SPT) methods. Pocket Penetrometer Testing (PPT) was performed on split spoon samples of stiffer silty clays. Shelby tube sampling and in-situ Vane Shear Tests (VST) were performed in softer silty clays. SPT blow counts, PPT readings and VST results are shown on the logs.

Soil samples obtained from the explorations were returned to our laboratory for further classification and testing. Laboratory testing includes grain size analysis, soil moisture content, Atterberg Limits, and one-dimensional consolidation testing. Atterberg Limits and moisture content test results are noted on the logs. Grain size analysis and one-dimensional consolidation test results are attached as Sheets 16 through 21.

3.0 SITE AND SUBSURFACE CONDITIONS

3.1 Surficial Conditions

The site is located on the southerly side of the Bates College campus. The first phase of the campus expansion is located along Campus Avenue between Central Avenue and Nichols Street. The site is generally occupied by existing two to three-story residential and office buildings with associated driveways, walkways, parking lots, and landscape areas. The site is relatively flat with some lower laying areas. Existing grades generally vary from about elevation to 230 to 243 feet (project datum). Existing site features are shown on the Exploration Location Plan, attached as Sheet 1.

3.2 Subsurface Conditions

Underlying a surficial layer of pavement or topsoil, the explorations encountered a soils profile generally consisting of fill overlying glaciomarine sands, silts, and clays. The

4.0 EVALUATION AND RECOMMENDATIONS

4.1 General Findings

Based on the subsurface findings, the proposed construction appears feasible from a geotechnical standpoint. The principle geotechnical considerations are as follows:

- Existing fills and relic organics are unsuitable for support of the proposed buildings and floor slabs. Planning should include removal of all unsuitable material below proposed buildings and floor slabs and replacement with properly compacted structural fill.
- Softer silty clay of varying thickness was encountered at some borings. The softer silty clay soil is compressible and will consolidate as loads from new fills and buildings are applied. We anticipate much of the consolidation related settlement will occur during construction, particularly as new fills are placed to raise site grades. As such, we recommend that the building pads be filled to final grade prior to excavating for foundations.
- In addition to being compressible, the softer clay soils are relatively low strength. Based on the findings at the preliminary borings and our understanding of proposed building grades, conventional spread footing foundations with on-grade floor slabs appear feasible. Basement foundations for the college store may encroach on the lower strength silty clay soils, as such a reinforced concrete mat foundation or grouted-rammed aggregate piers may be needed depending upon final basement elevation.
- The subsurface conditions are variable across the site over relatively short horizontal distances. Additional design phase test borings and test pits should be performed to further explore the subsurface conditions including existing fills and softer silty clay soils. Consideration should be given to performing shear wave velocity testing at the proposed building sites to further define Seismic Site Class.
- Sheet piling or other methods of braced excavation will likely be needed for construction of basements and installation of deeper utilities. Open cut excavations may be feasible in areas not adjacent to existing streets, utilities and buildings.

deleterious materials beneath the proposed building footprints and backfilling with compacted structural fill.

4.3.1 Foundation Type

Based on the subsurface findings and our understanding of the proposed construction, we offer the following preliminary foundation considerations. These considerations should be reevaluated once final proposed building location, finish grade, and structural loading have been developed.

College Store Building: Based on the findings at the explorations and assumed basement floor elevation (BFE) of 228 feet, support of the proposed college store building on spread footing foundations appears feasible. Spread footings should bear on at least 12-inches of compacted Crushed Stone, fully wrapped in non-woven geotextile fabric, overlying undisturbed native glaciomarine soils. Spread footing foundation design should consider an allowable soil bearing capacity of 1.5 ksf or less. If footings penetrate deeper than elevation 226 feet, we recommend consideration of a mat foundation or spread footings supported on grouted-rammed aggregate piers.

East and West Dormitory Buildings: Based on the findings at the explorations and our assumed finished floor elevation (FFE) of 240 and 242 feet, support of these buildings on spread footing foundations appears feasible. Spread footings should bear on at least 12-inches of compacted Crushed Stone, fully wrapped in non-woven geotextile fabric, overlying undisturbed native glaciomarine soils or compacted structural fill overlying native glaciomarine soils. Spread footing foundation design should consider an allowable soil bearing capacity of 3.0 ksf or less.

4.3.2 Settlement Analysis

We have evaluated settlement due to consolidation of the softer compressible silty clay soils encountered beneath portions of each of the proposed buildings. Our analysis has been based on the subsurface findings, the results of laboratory consolidation testing, and our assumed finish building elevations as presented herein. Additionally, we have assumed a typical building column load of 300 kips and a typical perimeter foundation wall load of 4 k/ft. We estimate consolidation related settlements as follows:

We recommend that additional exploration and analysis include:

- Test borings at the building corners and interior locations to help further characterize the extent and compressibility of softer clays beneath portions of the site.
- Test borings within proposed paved areas for pavement design and subgrade preparation.
- Test pits in the proposed building footprints to evaluate thickness and composition of existing fills.
- Crosshole shear wave velocity testing to assess seismic site class, as deemed necessary by the structural engineer.
- Settlement analysis with final design foundation configurations and structural loads.

5.0 CLOSURE

It has been a pleasure to be of assistance to you with this phase of your project. We look forward to working with you as the project progresses.

Sincerely,

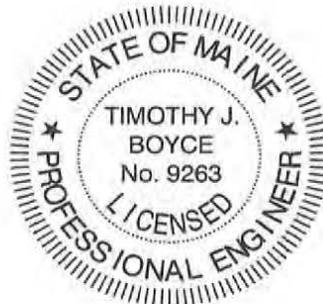
S. W. Cole Engineering, Inc.

Evan M. Walker, P.E.
Geotechnical Engineer



Timothy J. Boyce, P.E.
Senior Geotechnical Engineer

EMW/TJB:pfk



J

SOLID WASTE

EXHIBIT J-1

Bates

Facility Services

5/19/2014

Maine Department of Environmental Protection
Dan Courtemanche
17 State House Station
Augusta, ME 04333-0017

Re: Commercial Solid Waste Generation
Bates College Campus Avenue Housing Project
Maine Department of Environmental Protection Review

To Whom It May Concern:

Based on averages from 280 College, Rand Hall, and Parker Hall, I estimate that we will be hauling approximately 250 lbs. of trash daily (500 lbs total) from each of the new dorms on Campus Ave. We will be hauling the waste to Mid-Maine Waste Action Corp. in Auburn which I believe will have no problem handling the additional capacity. It should be noted that the new dorms are not increasing the enrollment of the college and therefore the overall waste generation from the campus should be unchanged after the completion of this project.

If you need any other information please let me know.

Sincerely,



Robert Leavitt
Assistant Director of Maintenance and Operations

RL/cjs

C: Chris Streifel
P. Wichroski

EXHIBIT J-3



CONSIGLI
Est. 1905

June 2, 2014

Mr. Chris Streifel
Bates College
147 Russell Street
Lewiston, ME 04240

RE: Campus Avenue Housing Project – Building Demolition Debris

Dear Mr. Streifel:

This letter serves to address how Consigli currently plans to dispose of the debris generated from the upcoming Campus Avenue Housing project.

Through an agreement with a waste management vendor (TBD), dumpsters will be transported to the appropriate recycling or disposal center. The size of the containers and their location on the site will vary due to project conditions. The materials intended to be recycled or diverted through the vendor are included on the table below:

Building Demolition

Material	tons recycled
Mixed Debris (C & D)	150

Please refer to the attached letter from Pine Tree Waste verifying the location for disposal of this debris.

If you have any questions, please do not hesitate to call.

Sincerely,
Consigli Construction Co., Inc.

David Thomas
Project Executive



Consigli Construction
15 Franklin Street
Portland, Me 04101

June 2, 2014

RE: Ability to Serve for Bates College Campus Avenue Student Housing Project

Dear Jan,

This letter is to confirm that Pine Tree Waste Inc. located in Mechanic Falls, Maine, has the capabilities to pick up, and dispose of annual volumes of (CDD) construction demolition debris as well as CDD material generated by proposed construction at the Building Construction project at Bates College Student Housing Project. The end site for the different materials will be:

Mixed Debris

Juniper Ridge Landfill
2828 Bennoch Road
Alton, Me 44088
MDEP Permit # S-020700-WD-N-A

Masonry/ Concrete w/rebar &w/o
Gendron & Gendron
Alfred Plourd Hwy
Lewiston, Me

Clean Wood ReEnergy
Alfred Plourd Hwy
Lewiston, Me

Cardboard/paper/plastic
Casella Recycling
Alfred Plourd Hwy
Lewiston, ME

Gypsum Board CPRC
Pleasant Hill Road
Scarborough

Metals
Maine Metals Schnitzer North East
Washington Street
Auburn, Maine

This letter is not a quote for service. It is a statement of capabilities. The sole purpose of this letter is to communicate the willingness and capabilities that Pine Tree Waste Inc. has towards providing this service as requested. If you have any questions or concerns, please do not hesitate to give me a call.

Sincerely,

Bill Bennett
Pine Tree Waste Inc.
87 Pleasant Hill Road
Scarborough, ME 04074
Office: 883-9777
Fax: 883-1954
William.bennett@casella.com

K

FINANCIAL CAPACITY

FINANCIAL CAPACITY

Financial Capacity

A. Estimated Costs

The construction cost for the site and facility have been estimated as shown below:

	Item	Estimated Costs
1	Site Work	\$ 3,111,000
2	55 Campus Ave	\$ 11,832,000
3	65 Campus Ave	\$ 14,117,000
	Total Construction	\$ 29,060,000

B. Financing

The project will be funded by Bates College with funds that have been raised for this project and through financing. Attached is a statement from Bates College regarding the commitment and funding sources. There is also a letter and statement of account from the Maine Health and Higher Educational Facilities Authority regarding the bond financing that is available for this project.

Bates

EXHIBIT K-2

May 12, 2014

Maine Department of Environmental Protection
Dan Courtemanche
17 State House Station
Augusta, ME 04333-0017

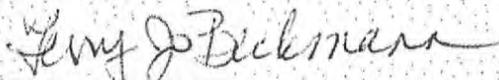
Re: Bates College Campus Avenue Housing Project
Maine Department of Environmental Protection Review

To Whom It May Concern:

Bates College has committed funding to proceed with the construction of a new student housing facility as described in the application materials. The total cost of the proposed project is estimated at \$29,000,000. Bates College has financing that is available for this project.

As permits are issued, construction will be authorized.

Sincerely,



Terry J. Beckmann
Vice President for Finance and Administration and Treasurer

TB/pjw

C: Chris Streifel
Doug Ginevan

GENERAL NOTES

1. BELOW GRADE UTILITY INFORMATION IS BASED ON INFORMATION PROVIDED BY EACH UTILITY. LOCATION OF PUBLIC UTILITIES SHOWN IS ONLY APPROXIMATE AND MAY NOT BE COMPLETE. PRIVATE UNDERGROUND UTILITIES SUCH AS, BUT NOT LIMITED TO, SEWER LINES, WATER LINES AND BURIED ELECTRICAL SERVICE ENTRANCES ARE NOT SHOWN. THE CONTRACTOR SHALL ASCERTAIN THE LOCATION AND SIZE OF EXISTING UTILITIES IN THE FIELD WITH THE RESPECTIVE UTILITY REPRESENTATIVE PRIOR TO COMMENCING WORK. REFER TO SPECIFICATION SECTION 01050. UTILITY CONTACTS ARE AS FOLLOWS.

Table with utility contact information including Electric (Central Maine Power), Cable Television (Time Warner Cable), Water/Sewer/Drain (City of Lewiston Public Works), Telephone (Fairpoint), Dig Safe (1-800-DIGSAFE), and Gas (Unutil).

ALL UTILITY COVERS OR SIMILAR STRUCTURES TO MATCH THE NEW PAVEMENT GRADE AND THE RELOCATION OF UTILITY POLES WILL BE PERFORMED BY THE APPROPRIATE UTILITY OR ITS AUTHORIZED REPRESENTATIVE. THE CONTRACTOR SHALL COOPERATE WITH THE UTILITY IN EVERY WAY TO EXPEDITE SUCH ADJUSTMENTS. CONTRACTOR TO COORDINATE ALL UTILITY RELOCATION AND INSTALLATION WITH THE APPROPRIATE UTILITY COMPANY.

2. ALL TEST PITS SHALL BE EXCAVATED PRIOR TO CONSTRUCTION LAYOUT AT THE APPROXIMATE LOCATIONS NOTED ON THE PLANS AND ACTUAL LOCATIONS OF SUBSURFACE UTILITIES SHALL BE REPORTED TO THE ENGINEER. WHERE APPROPRIATE, MINOR ADJUSTMENTS TO THE ALIGNMENTS OF PROPOSED LINES SHALL BE MADE TO ACCOMMODATE EXISTING UTILITIES. ALL ADJUSTMENTS (NORTHINGS, EASTINGS AND ELEVATIONS) MUST BE DEPICTED IN THE RECORD AS-BUILT DRAWING DATA SUBMITTED TO THE ENGINEER FOR INCLUSION IN THE RECORD AS-BUILT DRAWINGS.

3. COMPACTION TESTS SHALL BE PERFORMED IN ACCORDANCE WITH SPECIFICATION SECTION 02250. ANY SETTLEMENT OCCURRING WITHIN ONE YEAR OF SUBSTANTIAL COMPLETION OF THE PROJECT WILL BE CORRECTED BY THE CONTRACTOR IN A MANNER ACCEPTABLE TO THE ENGINEER AT NO ADDITIONAL EXPENSE TO THE OWNER.

4. BORING LOGS, PROBE LOGS AND THE GEOTECHNICAL REPORT ARE INCLUDED IN APPENDIX "A" OF THE SPECIFICATIONS.

5. ALL EXISTING COMBINED SEWER AND STORM DRAIN LINES ENCOUNTERED DURING CONSTRUCTION ARE TO REMAIN IN SERVICE EXCEPT AS OTHERWISE INDICATED. ANY EXISTING SEWERS, STORM DRAIN LINES OR CULVERTS DAMAGED DURING CONSTRUCTION AND NOT TO BE REPLACED SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.

6. FOR ABBREVIATIONS SEE SPECIFICATIONS SECTION 01070.

7. DO NOT SCALE DRAWINGS UNLESS OTHERWISE NOTED. WRITTEN DIMENSIONS AND STATIONING SHALL PREVAIL. REPORT ANY DISCREPANCIES IMMEDIATELY TO THE ENGINEER.

8. ALL STRUCTURES AND PIPELINES LOCATED ADJACENT TO THE TRENCH EXCAVATION SHALL BE PROTECTED AND FIRMLY SUPPORTED BY THE CONTRACTOR UNTIL THE TRENCH IS BACKFILLED. INJURY TO ANY SUCH STRUCTURES CAUSED BY, OR RESULTING FROM, THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. ALL UTILITIES REQUIRING REPAIR, RELOCATION OR ADJUSTMENT AS A RESULT OF THE PROJECT SHALL BE COORDINATED THROUGH THE RESPECTIVE UTILITY.

9. IN THOSE INSTANCES WHERE POWER OR TELEPHONE POLE SUPPORT IS REQUIRED, THE CONTRACTOR SHALL PROVIDE A MINIMUM 48-HOUR NOTIFICATION TO CENTRAL MAINE POWER, TIME WARNER, OR FAIR POINT RESPECTIVELY. NO ADDITIONAL PAYMENT WILL BE PROVIDED FOR TEMPORARY BRACING OF UTILITIES. THE CONTRACTOR SHALL COORDINATE RELOCATION OF TELEPHONE POLES WITH CENTRAL MAINE POWER.

10. CONTRACTOR SHALL INSTALL AND MAINTAIN PERMANENT AND TEMPORARY TRAFFIC CONTROL DEVICES AS NECESSARY AND IN A MANNER CONSISTENT WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.).

11. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING TRAFFIC FLOW AT ALL TIMES. THE CONTRACTOR IS REQUIRED TO SUBMIT A TRAFFIC CONTROL PLAN TO THE OWNER AND ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE POLICE DEPARTMENT AND FIRE DEPARTMENT ARE TO BE NOTIFIED AT LEAST 24 HOURS IN ADVANCE OF ANY STREET CLOSING OR DETOUR. REFER TO SPEC. SECTION 01570.

12. THE OWNER SHALL BE RESPONSIBLE FOR OBTAINING ANY NECESSARY RIGHTS-OF-WAY AND EASEMENTS. THE CONTRACTOR SHALL VERIFY THAT THE NECESSARY EASEMENTS HAVE BEEN SECURED BY THE OWNER. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO BE FAMILIAR WITH THE APPLICABLE PROVISIONS OF EACH EASEMENT AS THEY APPLY TO THE WORK PRIOR TO BIDDING AND ABIDE BY THOSE PROVISIONS DURING CONSTRUCTION. COPIES OF ANY SUCH RIGHTS-OF-WAY AND EASEMENTS ARE AVAILABLE FOR REVIEW FROM BATES COLLEGE.

13. OPEN TRENCHES IN THE ROADWAY MUST BE BACK FILLED AT THE END OF THE WORKDAY, UNLESS PERMISSION TO LEAVE THEM OPEN IS GIVEN IN WRITING BY THE CITY OF LEWISTON. ALL STREETS MUST BE PAVED PRIOR TO THE WINTER SHUTDOWN. TEMPORARY PAVEMENT SHALL BE CONSIDERED INCIDENTAL.

14. CONTRACTOR SHALL CONTROL DUST TO A TOLERABLE LIMIT AS OUTLINED IN SPECIFICATION SECTION 01562. CONTRACTORS SHALL NOT TRACK OR SPILL EARTH AND DEBRIS ON PUBLIC STREETS OUTSIDE THE PROJECT AREA. STREETS OPENED TO THE PUBLIC SHALL BE KEPT SWEEP AND FREE OF DEBRIS.

15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESETING ALL EXISTING PROPERTY MONUMENTATION THAT IS DISTURBED BY HIS OPERATIONS AT NO EXPENSE TO THE OWNER. THIS WORK IS TO BE DONE BY A LAND SURVEYOR LICENSED IN THE STATE OF MAINE. PROPERTY BOUNDS FOUND ARE SHOWN ON LAYOUT PLANS, THIS MAY NOT BE INCLUSIVE OF ALL BOUNDS THAT EXIST IN THE PROJECT AREA. IF ANY ADDITIONAL BOUNDS ARE FOUND, THE CONTRACTOR SHALL DOCUMENT THE LOCATION AND NOTIFY THE ENGINEER.

16. SUITABLE EXCAVATED MATERIAL MAY BE INCORPORATED IN THE PROJECT. EXCESS MATERIAL WILL BECOME PROPERTY OF THE CONTRACTOR. SILT FENCE AND STABILIZATION OF EXCESS SOIL IS ALSO REQUIRED. THE CONTRACTOR SHALL DISPOSE OF UNSUITABLE AND EXCESS MATERIAL IN ACCORDANCE WITH THE APPLICABLE SECTIONS OF THE CONTRACT DOCUMENTS AND ALL STATE, FEDERAL AND LOCAL REGULATIONS.

17. CONSTRUCT HANDICAP RAMPS AT ENDS OF ALL SIDEWALKS, FOR DRIVEWAYS OR ROADWAYS AND AS OTHERWISE SHOWN ON THE PLANS. SIDEWALKS AND HANDICAP RAMPS SHALL CONFORM TO THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT (ADA).

18. THE CONTRACTOR IS TO TAKE SPECIAL CARE NOT TO DAMAGE TREES WITHIN THE CONSTRUCTION AREA UNLESS THEY ARE NOTED TO BE REMOVED.

19. APPROXIMATE LIMIT OF WORK ON PLANS IS INTENDED TO SHOW GENERAL SCOPE OF THE WORK. ACTUAL LIMITS OF THE WORK ARE TO BE DETERMINED IN THE FIELD TO MEET PROJECT NEEDS AS DETERMINED BY THE OWNER AND THE ENGINEER.

20. PAVEMENT IS TO BE SAWCUT AT ALL SIDE ROADS, PAVED DRIVES, PAVED SIDEWALKS, AS WELL AS THE BEGINNING AND END OF THE PROJECT LIMITS. PAVEMENT GRINDING BEHIND THE SAW CUT WILL BE REQUIRED AT ALL LOCATIONS. SEE DWG 3.1 FOR DETAIL.

21. SAW CUT LINES FOR PAVED DRIVEWAY & SIDEWALK MATCHES ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY SAW CUT LOCATIONS FOR DRIVEWAY & WALKWAY MATCHES.

22. CURB CUT LOCATIONS FOR DRIVEWAYS SHALL BE FIELD VERIFIED. ADJUST CURB CUTS IF APPROPRIATE.

23. THE CONTRACTOR SHALL NOTIFY RESIDENTS 48 HOURS IN ADVANCE WHEN CONSTRUCTION WILL DISRUPT ACCESS TO THE RESIDENTS PROPERTY, INCLUDING DRIVEWAYS, WALKWAYS, ETC. DURING NOTIFICATION, CONTRACTOR SHALL VERIFY WITH OWNER ANY KNOWN BOUNDARY MARKER LOCATIONS.

24. EXISTING SIGNS THAT ARE IMPACTED BY THIS PROJECT SHALL BE RESET PER CITY STANDARDS AND IN ACCORDANCE WITH MDOT AND M.U.T.C.D. ADDITIONAL SIGNS MAY BE REQUIRED AS DIRECTED BY THE ENGINEER.

25. THE INVERTS INDICATED ARE THE INVERTS AT THE INSIDE FACE OF THE MANHOLE.

26. FLOWS FROM DEWATERING EFFORTS SHALL NOT BE DISCHARGED TO SANITARY OR COMBINED SEWERS.

EXISTING SITE NOTES:

- 1. THE LOCATIONS OF UNDERGROUND UTILITIES AND STRUCTURES, AS SHOWN ON THE DRAWINGS, ARE APPROXIMATE AND MAY NOT BE COMPLETE. THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE BASED ON PREVIOUS CONSTRUCTION DESIGN PLANS, WHICH ARE AVAILABLE FOR INSPECTION AT THE ENGINEER'S OFFICE. NO GUARANTEE IS MADE THAT UTILITIES OR STRUCTURES WILL BE ENCOUNTERED WHERE SHOWN OR THAT ALL UNDERGROUND UTILITIES AND STRUCTURES ARE SHOWN. ALL LOCATIONS AND SIZES OF EXISTING UTILITIES AND STRUCTURES SHALL BE VERIFIED IN THE FIELD WITH TEST PITS AS REQUIRED PRIOR TO BEGINNING CONSTRUCTION OF NEW FACILITIES OR PIPING THAT MAY BE AFFECTED. THE CONTRACTOR WILL REALIGN NEW PIPE LOCATIONS AS REQUIRED TO CONFORM TO EXISTING LINES AND AS APPROVED BY THE ENGINEER.
- 2. CONTRACTOR TO NOTE THAT, IN GENERAL, ALL EXISTING CONDITION INFORMATION ON THE DRAWINGS ARE SHOWN WITH A LIGHTER LINE WEIGHT AND WITH A SLANTED TYPE TEXT.
- 3. UNLESS OTHERWISE NOTED, THERE IS NO KNOWN ASBESTOS WITHIN THE AREA OF WORK. IF THE PRESENCE OF ASBESTOS IS DISCOVERED, THE CONTRACTOR SHALL NOTIFY THE OWNER AND THE ENGINEER IMMEDIATELY. DISPOSAL OF ASBESTOS SHALL BE IN ACCORDANCE WITH STATE OF MAINE REGULATIONS.

DEMOLITION NOTES:

- 1. REFER TO THE EXISTING SITE PLAN, DWG C1.1 AND C1.2, FOR ADDITIONAL INFORMATION REGARDING EXISTING FACILITIES. REFER TO DWG C2.1 AND C2.2 FOR LIMITS OF WORK.
- 2. REFER TO SPECIFICATION SECTION 01010, WHICH CONTAINS INFORMATION ON CONSTRAINTS OF CONSTRUCTION SEQUENCING.
- 3. ALL PIPING, EQUIPMENT AND MATERIALS TO BE DEMOLISHED AND/OR REMOVED FROM SERVICE SHALL BE COORDINATED WITH THE OWNER AND ENGINEER BEFORE COMMENCING THAT WORK. EXISTING PIPES AND CONDUIT DESIGNATED AS "ABANDONED" MAY BE REMOVED IF THE CONTRACTOR SO CHOOSES. IF ABANDONED PIPE CONTACTS WITH PROPOSED NEW PIPING, THEN A PORTION OF THE ABANDONED PIPE SHALL BE REMOVED AND THE NEW ENDS OF ABANDONED PIPE CAPPED, OR PLUGGED WITH CONCRETE.
- 4. ALL EXISTING PIPING AND UTILITIES WHICH ARE BENEATH PROPOSED STRUCTURES, AND ARE TO BE ABANDONED, SHALL BE REMOVED TO A MINIMUM OF 5 FEET OUTSIDE OF THE STRUCTURE. PIPE AND UTILITIES BENEATH PROPOSED STRUCTURES THAT ARE TO REMAIN SHALL BE CONCRETE ENCASED, UNLESS OTHERWISE INDICATED.
- 5. SEVERING OF EXISTING UTILITIES FOR ABANDONMENT, OR REMOVAL OF A SEGMENT FROM SERVICE, SHALL BE PERFORMED IN SUCH A MANNER AS TO ALLOW THE REMAINING ACTIVE SEGMENT TO CONTINUE IN ITS INTENDED SERVICE. CAP ACTIVE SEGMENTS WITH APPROPRIATE FITTINGS, JOINT RESTRAINT, ETC. TO ENSURE THEIR INTEGRITY. PLUG ENDS OF ABANDONED PIPE SEGMENTS WITH CONCRETE UNLESS SPECIAL CIRCUMSTANCES DICTATE PLUGGING ABANDONED PIPES WITH BLIND FLANGES, RESTRAINED MECHANICAL JOINT PLUGS, ETC. AS APPROPRIATE.
- 6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING AND DISPOSING OF ALL DEMOLISHED PIPING, EQUIPMENT AND MATERIALS. DISPOSAL SHALL BE IN ACCORDANCE WITH ALL STATE AND LOCAL REGULATIONS. THE OWNER RESERVES THE RIGHT TO RETAIN ANY SUCH PIPING, EQUIPMENT AND MATERIALS FOR DEMOLITION FOR HIS USE. SUCH MATERIALS TO BE RETAINED SHALL BE PROPERLY STORED IN AN ON-SITE LOCATION. COORDINATE LOCATION AND MATERIALS TO BE SALVAGED WITH THE OWNER/ENGINEER.
- 7. THE CONTRACTOR SHALL KEEP A RECORD OF DEMOLITION AS PART OF THE PROJECT RECORD DOCUMENTS IN ACCORDANCE WITH SPECIFICATION SECTION 01720.
- 8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE APPROPRIATE DISPOSAL OF FLOWS RESULTING FROM PRECIPITATION AND HIS DEWATERING OPERATIONS.
- 9. CONTRACTOR IS REFERRED TO SPECIFICATION SECTION 01050 FOR COORDINATION WITH OTHERS.

SITE GRADING NOTES:

- 1. STRIPPING OF TOPSOIL (LOAM) SHALL BE IN ACCORDANCE WITH SPECIFICATION SECTION 02115. REFER TO DWG C2.1, FOR LIMIT OF WORK AND STRIPPING.
- 2. CONTRACTOR SHALL MINIMIZE CLEARING OPERATIONS. CLEARING AND GRUBBING SHALL BE IN ACCORDANCE WITH SPECIFICATION SECTION 02115. CLEARING LIMITS SHALL BE AS INDICATED ON THE DRAWINGS, BUT AT ALL TIMES WITHIN PROPERTY LINES OR EASEMENTS. ALL GRUBBINGS AND EXCESS EXCAVATED MATERIAL ARE THE PROPERTY OF THE CONTRACTOR AND WILL BE DISPOSED OF AT A SITE PROVIDED BY THE CONTRACTOR IN COMPLIANCE WITH ALL STATE AND LOCAL LAWS.
- 3. ALL AREAS THAT ARE EXCAVATED, FILLED OR OTHERWISE DISTURBED BY THE CONTRACTOR AND ARE NOT TO BE PAVED OR FILLED WITH GRAVEL OR RIPRAP SHALL BE LOAMED, GRADED, LIMED, FERTILIZED, SEEDED AND MULCHED. ALL AREAS TO RECEIVE 4" OF TOPSOIL EXCEPT AREAS ADJACENT TO STREAMS, WHICH WILL REQUIRE 6" OF TOPSOIL.
- 4. THE CONTRACTOR SHALL NOT HAVE ANY RIGHT OF PROPERTY IN ANY MATERIALS TAKEN FROM ANY EXCAVATION. SUITABLE EXCAVATED MATERIAL MAY BE INCORPORATED IN THE PROJECT, WITH EXCESS MATERIAL DISPOSED OF AT A LOCATION PROVIDED BY THE CONTRACTOR. THESE PROVISIONS SHALL IN NO WAY RELIEVE THE CONTRACTOR OF HIS OBLIGATIONS TO PROPERLY DISPOSE OF AND REPLACE ANY MATERIAL DETERMINED BY THE ENGINEER TO BE UNSUITABLE FOR BACKFILLING. THE CONTRACTOR SHALL DISPOSE OF UNSUITABLE AND EXCESS MATERIAL IN ACCORDANCE WITH THE APPLICABLE SECTIONS OF THE CONTRACT DOCUMENTS.
- 5. THE CONTRACTOR SHALL PROVIDE PROPER EROSION CONTROL AND DRAINAGE MEASURES IN ALL AREAS OF WORK. SOIL EROSION CONTROL SHALL BE PROVIDED PRIOR TO BEGINNING EXCAVATION WORK. EROSION CONTROL FENCE SHALL BE INSTALLED AT THE DOWN GRADIENT PERIMETER OF THE ACTUAL LIMITS OF GRUBBING AND/OR GRADING, AND AS SHOWN ON THE DRAWINGS. EROSION CONTROL MEASURES SHOWN ON THE DRAWINGS ARE A MINIMUM. CONTRACTOR SHALL TAKE ALL OTHER NECESSARY MEASURES. EROSION CONTROL FENCE SHALL ALSO BE INSTALLED AT THE DOWN GRADIENT PERIMETER OF THE TOPSOIL STOCKPILES. ALL DISTURBED EARTH SURFACES SHALL BE STABILIZED IN THE SHORTEST PRACTICAL TIME AND TEMPORARY EROSION CONTROL DEVICES SHALL BE EMPLOYED UNTIL SUCH TIME AS ADEQUATE SOIL STABILIZATION HAS BEEN ACHIEVED. TEMPORARY STORAGE OF EXCAVATED MATERIALS SHALL BE STABILIZED IN A MANNER THAT WILL MINIMIZE EROSION. ALL INSTALLED EROSION CONTROL FACILITIES SHALL BE REMOVED AT THE END OF THE PROJECT. REFER TO SPECIFICATION SECTION 02270.
- 6. ALL STORM DRAINAGE INLETS SHALL BE PROTECTED BY HAY BALE FILTERS TO PREVENT ENTRY OF SEDIMENT FROM RUNOFF WATERS DURING CONSTRUCTION. CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL AND DISPOSAL OF ALL COLLECTED SEDIMENT, AND THAT WHICH COLLECTS IN THE STORM DRAIN SYSTEM.
- 7. TEST PIT AND/OR BORING LOGS FOR THE PROJECT SITE ARE INCLUDED IN APPENDIX A OF THE SPECIFICATIONS.
- 8. ALL ELEVATIONS REFER TO THE NATIONAL GEODETIC VERTICAL DATUM. ORIENTATION IS GRID NORTH MAINE STATE PLANE COORDINATE SYSTEM. PROJECT BENCH MARK WILL BE PROVIDED BY THE ENGINEER.
- 9. CONTRACTOR SHALL CONTROL DUST ON THE CONSTRUCTION SITE TO A REASONABLE LIMIT, AS DETERMINED BY THE ENGINEER, AND AS OUTLINED IN SPECIFICATION SECTION 01562.
- 10. CONTRACTOR SHALL NOT TRACK OR SPILL EARTH, DEBRIS OR OTHER CONSTRUCTION MATERIAL ON PUBLIC OR PRIVATE STREETS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE IMMEDIATE ASSOCIATED CLEAN UP.
- 11. ALL CATCH BASINS, MANHOLES, VALVE PITS, VALVE BOXES AND OTHER BURIED FACILITIES WITH SURFACE ACCESS SHALL BE ADJUSTED TO MATCH FINAL GRADES, UNLESS OTHERWISE INDICATED.
- 12. WHEREVER PROPOSED STRUCTURES ARE LOCATED PARTLY WITHIN A PAVED AREA AND PARTLY IN A NON-PAVED AREA, CONCRETE PAVED APRON 2 FT WIDE SHALL BE SUPPLIED AROUND THE PROPOSED COVER. PAVEMENT SHALL SLOPE AWAY FROM THE COVER.
- 13. CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL DEBRIS AND EXCESS EXCAVATED MATERIAL FROM WITHIN THE CONSTRUCTION LIMIT OF WORK, TO SUITABLE SITE PROVIDED BY THE CONTRACTOR IN COMPLIANCE WITH ALL STATE AND LOCAL REGULATIONS. ANY EXCESS SUITABLE MATERIAL MAY REMAIN ON SITE AT THE REQUEST OF THE OWNER.
- 14. CONTRACTOR SHALL REMOVE AND REPLACE, OR REPAIR, ALL CURBS, SIDEWALKS, PAVEMENT AND OTHER ITEMS DAMAGED BY HIS CONSTRUCTION ACTIVITIES TO AT LEAST THEIR ORIGINAL CONDITION, TO THE SATISFACTION OF THE OWNER AND ENGINEER.
- 15. WHERE EXISTING PAVEMENT IS REMOVED AND REPLACED, MATCH EXISTING GRADES TO THE EXTENT POSSIBLE. COORDINATE FINE GRADING WITH THE ENGINEER.
- 16. ALL ROAD AND PARKING AREA SURFACES SHALL PITCH 1/4 INCH PER FOOT MINIMUM UNLESS OTHERWISE NOTED. ALL VEGETATED AREAS ABOVE MEAN HIGH WATER ELEVATION THAT ARE EXCAVATED, FILLED OR OTHERWISE DISTURBED BY THE CONTRACTOR AND ARE NOT TO BE PAVED OR FILLED WITH RIPRAP SHALL BE LOAMED, GRADED, LIMED, FERTILIZED, SEEDED AND MULCHED AT NO ADDITIONAL EXPENSE TO THE OWNER.
- 17. INITIAL PAVING SHALL BE CONDUCTED WITHIN TWO WEEKS OF COMPLETION OF PLACEMENT OF FINAL BACKFILL UNLESS OTHERWISE AUTHORIZED BY ENGINEER. INITIAL PAVEMENT SHALL BE INSTALLED AND MAINTAINED BY CONTRACTOR FOR A MINIMUM PERIOD OF 2 MONTHS BEFORE FINAL PAVEMENT IS PLACED. FINAL PAVEMENT MAY BE PLACED OVER THE INITIAL PAVING PROVIDED INITIAL PAVING COURSE IS IN GOOD REPAIR. CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING AND SHIMMING THE INITIAL PAVEMENT AS NECESSARY TO ACCEPT THE FINAL PAVING COURSE. IF CONDITIONS WARRANT, THE CONTRACTOR MAY BE REQUIRED TO REMOVE AND REPLACE INITIAL PAVING PRIOR TO FINAL PAVING.

SITE LAYOUT NOTES:

- 1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LAYOUT OF ALL PROPOSED WORK AS SHOWN ON THE DRAWINGS. THE ENGINEER WILL PROVIDE TWO POINTS THAT DEFINE THE HORIZONTAL CONTROL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THIS PROVIDED LAYOUT INFORMATION THROUGHOUT THE COURSE OF CONSTRUCTION. REPORT ANY LAYOUT DISCREPANCIES IMMEDIATELY TO THE ENGINEER.
- 2. ENGINEER WILL PROVIDE HORIZONTAL CONTROL POINTS TO ESTABLISH THE CONSTRUCTION BASELINE. THE POINTS TO BE PROVIDED ARE SHOWN ON THE LAYOUT PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING AND REESTABLISHING THESE POINTS FOR THE DURATION OF THE PROJECT. CONTRACTOR SHALL LAYOUT ALL CONSTRUCTION AS SHOWN ON THE DRAWINGS USING THE CONSTRUCTION BASELINE. THE LAYOUT SHALL BE REVIEWED BY THE ENGINEER PRIOR TO CONSTRUCTION. ANY DEVIATIONS FROM THE ROADWAY OR UTILITY LAYOUTS MUST BE APPROVED BY THE ENGINEER/OWNER.
- 3. REFER TO THE SITE PIPING AND SITE GRADING DRAWINGS FOR ADDITIONAL LAYOUT INFORMATION.
- 4. IN GENERAL, THE GIVEN STRUCTURE LOCATIONS ARE TO THE OUTSIDE FACE OF THE STRUCTURE FOUNDATION WALL, NOT FOOTINGS. REFER TO THE ARCHITECTURAL AND STRUCTURAL DRAWINGS FOR BUILDING AND STRUCTURE DIMENSIONS. RADI SHOWN FOR ROADS ARE TO EDGE OF PAVEMENT.
- 5. PLACE CRUSHED STONE MOWING STRIP AROUND THOSE STRUCTURES AS INDICATED ON THE DRAWINGS. SEE DWG C3.0 FOR DETAIL.
- 6. THE LOCATION AND LIMITS OF ALL ON-SITE WORK AND STORAGE AREAS SHALL BE REVIEWED/COORDINATED WITH, AND ACCEPTABLE TO, THE OWNER AND ENGINEER. THE CONTRACTOR SHALL LIMIT HIS ACTIVITIES TO THESE AREAS.
- 7. BOLLARD LOCATIONS SHOWN ARE APPROXIMATE. REFER TO THE ARCHITECTURAL AND STRUCTURAL DRAWINGS FOR THE PRECISE LOCATION OF BOLLARDS NEAR NEW STRUCTURES. COORDINATE BOLLARD LOCATION WITH THE ENGINEER. SEE DWG C3.0 FOR DETAIL.

SITE PIPING NOTES:

- 1. ALL PIPE LINES SHALL SLOPE UNIFORMLY BETWEEN ELEVATIONS INDICATED ON THE DRAWINGS. NO CRESTS IN PIPING WILL BE PERMITTED. ALL HORIZONTAL AND VERTICAL BENDS IN PRESSURIZED LINES SHALL BE SUITABLY RESTRAINED WITH THRUST BLOCKS OR RETAINER GLANDS (RETAINER GLANDS ALLOWED FOR DUCTILE IRON PIPE ONLY). SEE DWG C3.0 FOR THRUST BLOCK DETAILS. PROVIDE ALL BENDS (HORIZONTAL AND VERTICAL) AS REQUIRED TO MEET THE GRADES AND ALIGNMENT INDICATED ON THE DRAWINGS.
- 2. THE CONTRACTOR SHALL ASCERTAIN THE LOCATION AND SIZE OF EXISTING PIPING AND UTILITIES IN THE FIELD BY TEST PIT EXCAVATION PRIOR TO COMMENCING INSTALLATION OF ANY OF THE NEW PIPING AFFECTED. WHERE NEW PIPE CONNECTS TO EXISTING PIPING OR STRUCTURAL PENETRATION, CONTRACTOR SHALL VERIFY ELEVATION BY TEST PIT, AS REQUIRED. PRIOR TO INSTALLATION OF ANY OF THE ASSOCIATED/AFFECTED NEW PIPING, IDENTIFIED CONFLICTS WITH EXISTING PIPING AND UTILITIES WILL BE REVIEWED WITH THE ENGINEER PRIOR TO COMMENCING INSTALLATION. THE HORIZONTAL ALIGNMENT OF NEW PIPING MAY BE ADJUSTED IN THE FIELD SUBJECT TO PRIOR REVIEW AND ACCEPTANCE OF THE ENGINEER. CONTRACTOR SHALL BE RESPONSIBLE FOR ANY LAYOUT OF ALL PROPOSED WORK AS SHOWN ON THE DRAWINGS AND REPORT ANY LAYOUT DISCREPANCIES IMMEDIATELY TO THE ENGINEER.
- 3. TRENCH INSULATION SHALL BE USED WHERE DEPTH OF COVER IS LESS THAN 5 FEET. REFER TO DWG C3.0 FOR TRENCH INSULATION DETAIL.
- 4. MANHOLES AND CATCH BASINS ARE 4 FEET IN DIAMETER UNLESS OTHERWISE NOTED. THE TOP OF FRAMES SHALL BE SET FLUSH WITH FINISH GRADE, UNLESS OTHERWISE NOTED ON DRAWINGS.
- 5. REFER TO SPECIFICATION SECTION 02200 FOR PIPE AND STRUCTURE BEDDING AND BACKFILL REQUIREMENTS.
- 6. COMPACTION TESTS WILL BE PERFORMED IN ACCORDANCE WITH SPECIFICATION SECTION 02200. ANY SETTLEMENT OCCURRING WITHIN ONE YEAR OF FINAL COMPLETION OF THE WORK SHALL BE CORRECTED BY THE CONTRACTOR AT NO ADDITIONAL COST.
- 7. THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE REGULATIONS OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA).
- 8. REFER TO SECTION 01050 OF THE SPECIFICATIONS FOR INFORMATION REGARDING COORDINATION WITH OTHERS, INCLUDING RESPONSIBILITIES AND RELATED COSTS.
- 9. WHERE NEW PIPING IS TO BE CONNECTED TO EXISTING PIPING, THE CONTRACTOR SHALL FURNISH AND INSTALL ALL ADAPTERS, FITTINGS, AND ADDITIONAL PIPE AS REQUIRED TO COMPLETE THE CONNECTION. CONTRACTOR SHALL VERIFY LOCATION, ELEVATION, ORIENTATION AND MATERIAL OF CONSTRUCTION. TEST PITS SHALL BE USED AS REQUIRED.
- 10. ALL EXISTING UTILITIES ENCOUNTERED DURING CONSTRUCTION ARE TO REMAIN IN SERVICE UNLESS OTHERWISE NOTED ON THE DEMOLITION PLAN, DWG C1.2. CONTRACTOR SHALL RE-SHAPE INVERTS AS REQUIRED WHEN CONNECTING INTO EXISTING MANHOLES.
- 11. CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL AND DISPOSAL OF ALL DEMOLITION MATERIALS IN ACCORDANCE WITH SPECIFICATION SECTION 02050.
- 12. WHERE POSSIBLE, WATER LINES SHOULD BE INSTALLED OVER WASTEWATER LINES. A MINIMUM SEPARATION OF 18 INCHES BETWEEN THE BOTTOM OF THE WATER LINE AND THE TOP OF THE WASTEWATER LINE SHALL BE MAINTAINED, IF POSSIBLE. WHERE A WATER LINE CROSSES UNDER A WASTEWATER LINE, A FULL LENGTH OF PIPE SHALL BE CENTERED ABOVE THE WATER LINE SO THAT BOTH JOINTS WILL BE AS FAR FROM THE WATER LINE AS POSSIBLE.
- 13. ALL STRUCTURES AND PIPELINES LOCATED ADJACENT TO ANY TRENCH EXCAVATION SHALL BE PROTECTED AND FIRMLY SUPPORTED BY THE CONTRACTOR UNTIL THE TRENCH IS BACKFILLED. DAMAGE TO ANY SUCH STRUCTURES CAUSED BY OR RESULTING FROM THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. ALL UTILITIES REQUIRING REPAIR, RELOCATION OR ADJUSTMENT AS A RESULT OF THE PROJECT SHALL BE COORDINATED THROUGH THE OWNER.
- 14. PIPING ON THE SITE PIPING PLAN HAS BEEN SHOWN BROKEN FOR CLARITY ONLY. PIPE BREAKS DO NOT INDICATE RELATIVE ELEVATIONS OF PIPING.
- 15. ELECTRICAL CONDUIT RUNS ARE INDICATED ON THE UTILITY AND SITE ELECTRICAL DRAWINGS.
- 16. FORCE MAINS SHALL SLOPE UNIFORMLY BETWEEN ELEVATIONS INDICATED ON THE DRAWINGS. NO CRESTS IN NEW PIPING WILL BE PERMITTED UNLESS OTHERWISE NOTED ON THE DRAWINGS. ALL BENDS SHALL BE SUITABLY RESTRAINED BY CAST-IN-PLACE CONCRETE THRUST BLOCKS. DUCTILE IRON RETAINER GLANDS MAY BE USED IN LIEU OF THRUST BLOCKS ON DUCTILE IRON FORCE MAINS ONLY. THE NUMBER OF JOINTS ON EACH SIDE OF THE BENDS REQUIRING RETAINER GLANDS SHALL BE DETERMINED BY STANDARDS SET FORTH BY THE DUCTILE IRON PIPE RESEARCH ASSOCIATION. TEST PRESSURE FOR THE PRESSURE AND LEAKAGE TEST SHALL BE 100 PSI OR GREATER AS OUTLINED IN SPECIFICATION SECTION 02755.
- 17. ALL UNDERGROUND UTILITIES ENCOUNTERED DURING CONSTRUCTION WILL BE IN SERVICE OR OUT OF SERVICE. IF OUT OF SERVICE WHEN ENCOUNTERED, THE CONTRACTOR SHALL REMOVE COMPLETELY AND DISPOSE OF DEBRIS. IF A UTILITY IS TO BE TAKEN OUT OF SERVICE AS A RESULT OF THIS PROJECT, THAT UTILITY WILL ALSO BE REMOVED COMPLETELY AFTER THE UTILITY IS DISCONTINUED. THE ONLY EXCEPTION TO THIS IS ASBESTOS PIPE. ASBESTOS PIPE THAT IS ENCOUNTERED WILL BE LEFT UNDISTURBED AND ABANDONED IN PLACE. ALL WORK COVERED BY THIS NOTE WILL BE INCIDENTAL TO THE COST OF THE PIPE INSTALLATION AND SHALL BE PERFORMED AT NO ADDITIONAL COST TO THE OWNER.
- 18. WHERE SEWER MAINS AND HOUSE SERVICE CONNECTIONS CROSS WATER MAINS THEY SHALL BE INSTALLED UNDER THE WATER MAIN WHENEVER POSSIBLE. A MINIMUM SEPARATION OF 18 INCHES BETWEEN THE BOTTOM OF THE WATER LINE AND TOP OF THE SEWER SHALL BE MAINTAINED. WHERE SITE CONDITIONS DICTATE LESS THAN THE ABOVE REFERENCED 18 INCH SEPARATION THE FOLLOWING CONSTRUCTION METHODS ARE REQUIRED: THE WATER MAIN SHALL BE RELOCATED TO PROVIDE THE 18" SEPARATION, AND ONE FULL LENGTH OF PIPE SHALL BE CENTERED AT THE POINT OF CROSSING TO LOCATE JOINTS AS FAR AS POSSIBLE FROM THE WATER MAIN. THE CONTRACTOR SHALL RELOCATE THE WATER MAIN TO THE SATISFACTION OF THE LEWISTON DEPARTMENT OF PUBLIC WORKS.
- 19. PROVIDE 2" RIGID INSULATION OVER WATER AND SEWER MAINS AND SERVICES WHEN COVER IS LESS THAN 6.0 FEET AND WHERE DIRECTED BY THE ENGINEER (SEE DETAILS).
- 20. WHERE APPROPRIATE, MINOR ADJUSTMENTS TO THE ALIGNMENT OF PROPOSED UTILITIES SHALL BE MADE TO ACCOMMODATE EXISTING UTILITIES. COORDINATE ADJUSTMENTS WITH ENGINEER.
- 21. CONTRACTOR SHALL COORDINATE LOCATION OF SEWER SERVICE WYES TO AVOID CONFLICT BETWEEN SEWER SERVICES, WATER LINES AND SERVICES, AND NEW STORM DRAINAGE PIPING.
- 22. A MINIMUM VERTICAL OR HORIZONTAL SEPARATION OF 6 INCHES BETWEEN THE WATER LINES (INCLUDING SERVICES) AND STORM DRAINAGE PIPES AND STRUCTURES, SHALL BE MAINTAINED. 2" RIGID INSULATION SHALL BE INSTALLED WHEN THE SEPARATION IS LESS THAN 4 FEET. 4" RIGID INSULATION SHALL BE INSTALLED FOR UNDER 18" SEPARATION.
- 23. PIPE RESTRAINT FOR WATER MAINS: ALL BENDS, TEES, REDUCERS, HYDRANTS, AND PLUGS SHALL BE RESTRAINED BY USING CONCRETE THRUST BLOCKS AND "GRIP RINGS" OR OTHER METHOD AS SHOWN ON THE DRAWINGS. ANCHOR PIPES SHALL BE USED FOR ALL HYDRANT BRANCHES EXCEPT IN LOCATIONS WHERE GATE VALVES CAN NOT BE CONNECTED DIRECTLY TO THE, IN WHICH CASE "GRIP RINGS" OR THE RDDS SHALL BE USED.
- 24. WATER LINE AND APPURTENANCE CONSTRUCTION SHALL BE IN ACCORDANCE WITH CITY OF LEWISTON REGULATIONS. SEWER MANHOLE AND PIPELINE CONSTRUCTION AND TESTING SHALL BE IN ACCORDANCE WITH CITY OF LEWISTON REQUIREMENTS.
- 25. WATER MAIN WILL BE TESTED AT 1.5 TIMES THE WORKING PRESSURE OR 200 PSI WHICHEVER IS GREATER. TEST DURATION WILL BE TWO HOURS. REFER TO SPECIFICATION SECTION 02610.
- 26. ALL SEWER PIPE SHALL BE PVC MEETING THE REQUIREMENTS OUTLINED IN THE SPECIFICATIONS UNLESS OTHERWISE INDICATED ON THE DRAWINGS.

CIVIL ABBREVIATIONS

Table of Civil Abbreviations with columns for symbol, description, and notes. Includes symbols for diameter, number, approval, building, catch basin, center, cubic feet per second, cast iron, centerline, corrugated metal pipe, cleanout, concrete, corner, cubic yard, demolition, drain manhole, ductile iron, drain, drawing, elevation, electric manhole, force main, feet, gas, hydrant, inch, influent, invert, pounds, maximum, manhole, minimum, monitoring well, north, and north arrow.

LEGEND table with columns for EXISTING and PROPOSED symbols and descriptions. Includes symbols for property/row line, setback line, easement line, centerline, edge of pavement, curb, edge of gravel, edge of concrete, contour, building, stone wall, treeline, chain link fence, stockade fence, barb wire fence, retaining wall, guardrail, sewer, sewer force main, gas, water, storm drain, underdrain, culvert, underground electric, overhead electric, iron pipe/rebar, drillhole, monument, survey control point, spot elevation, sewer manhole, drainage manhole, catch basin, electric manhole, telephone manhole, gate valve, curb stop, yard hydrant, hydrant, utility pole, utility pole w/ guy, utility pole w/ light, light pole, bollard, flagpole, coniferous tree, deciduous tree, shrub, edge of water, stream, edge of wetlands, floodplain, wetlands, drainage flow, drainage swale, pavement markings, sign, mailbox, temporary bench mark, test pit, test boring, test probe, monitoring well, limit of work, silt fence, riprap, railroad, matchline, rock outcrop.

PROJECT OWNER

PRESIDENT AND TRUSTEES OF BATES COLLEGE
2 ANDREWS WAY
LEWISTON, ME 04240

PROPERTY INCLUDED IN DEVELOPMENT

Table with columns for TAX MAP, PARCEL #, and area in ACRES. Lists parcels 173 LOT 5, 173 LOT 6, 173 LOT 7, 173 LOT 8, 173 LOT 10, 194 LOT 113, and 194 LOT 136.

Table comparing ALLOWED and PROPOSED coverage percentages. Shows 75% allowed vs 55% campus and 65% campus proposed for max impervious cover, and 75% allowed vs 17.3% and 16.5% proposed for max lot coverage.

PROPOSED USE:

INSTITUTIONAL - STUDENT HOUSING AND CAMPUS STORE AND MAIL FACILITY.

NOTE:

- 1. IF DEVELOPMENT HAS NOT OCCURRED, AS DEFINED WITHIN THE SCOPE OF THE ZONING AND LAND USE CODE WITHIN TWO YEARS, DEVELOPMENT REVIEW APPROVAL SHALL EXPIRE, PURSUANT TO ARTICLE XIII, SECTION II.
- 2. PRIOR TO A CERTIFICATE OF OCCUPANCY BEING ISSUED A PROFESSIONAL ENGINEER MUST PROVIDE A STAMPED STATEMENT INDICATING THAT THE STORMWATER IMPROVEMENTS HAVE BEEN COMPLETED IN ACCORDANCE WITH THE APPROVED PLANS.

BATES COLLEGE

55 & 65 CAMPUS AVENUE HOUSING

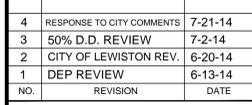
LEWISTON, MAINE 04240

KEY PLAN



LEGEND

Table with 4 columns: NO., REVISION, DATE, and description. Lists revisions 1 through 4, including responses to city comments, 50% D.D. review, and DEP review.



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AnnBeha Architects

33 Kingston Street Boston, MA 02111
p:617.338.3000 f:617.482.9097

Job Number: 01313.01
Project: BATES COLLEGE

Drawn By: MRL Checked By: JBW
Date: 6.20.2014

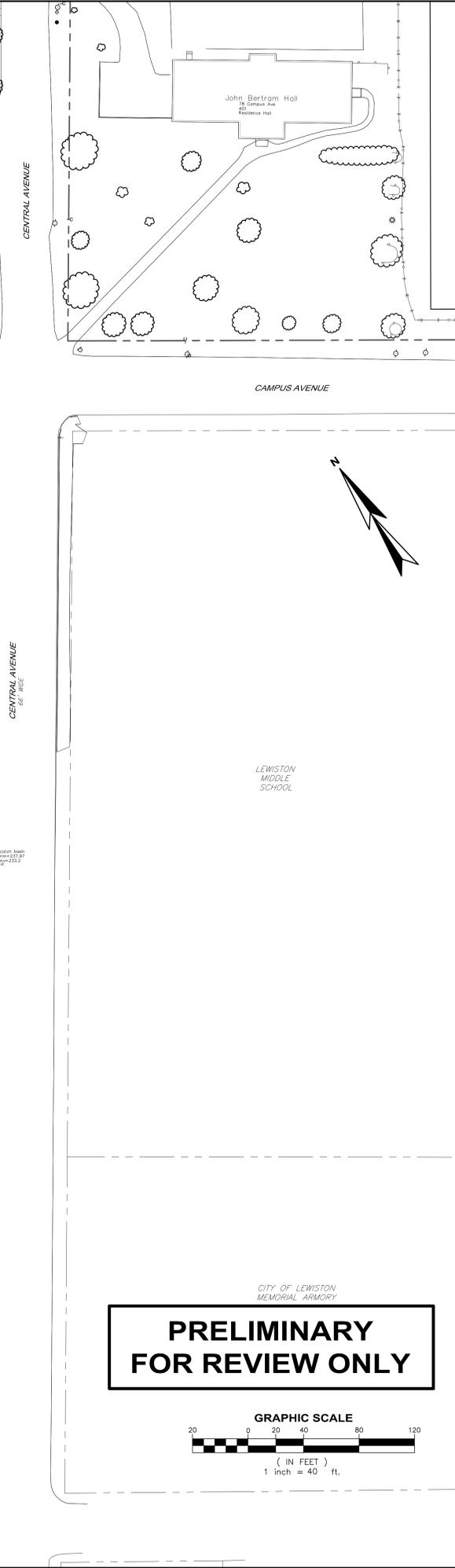
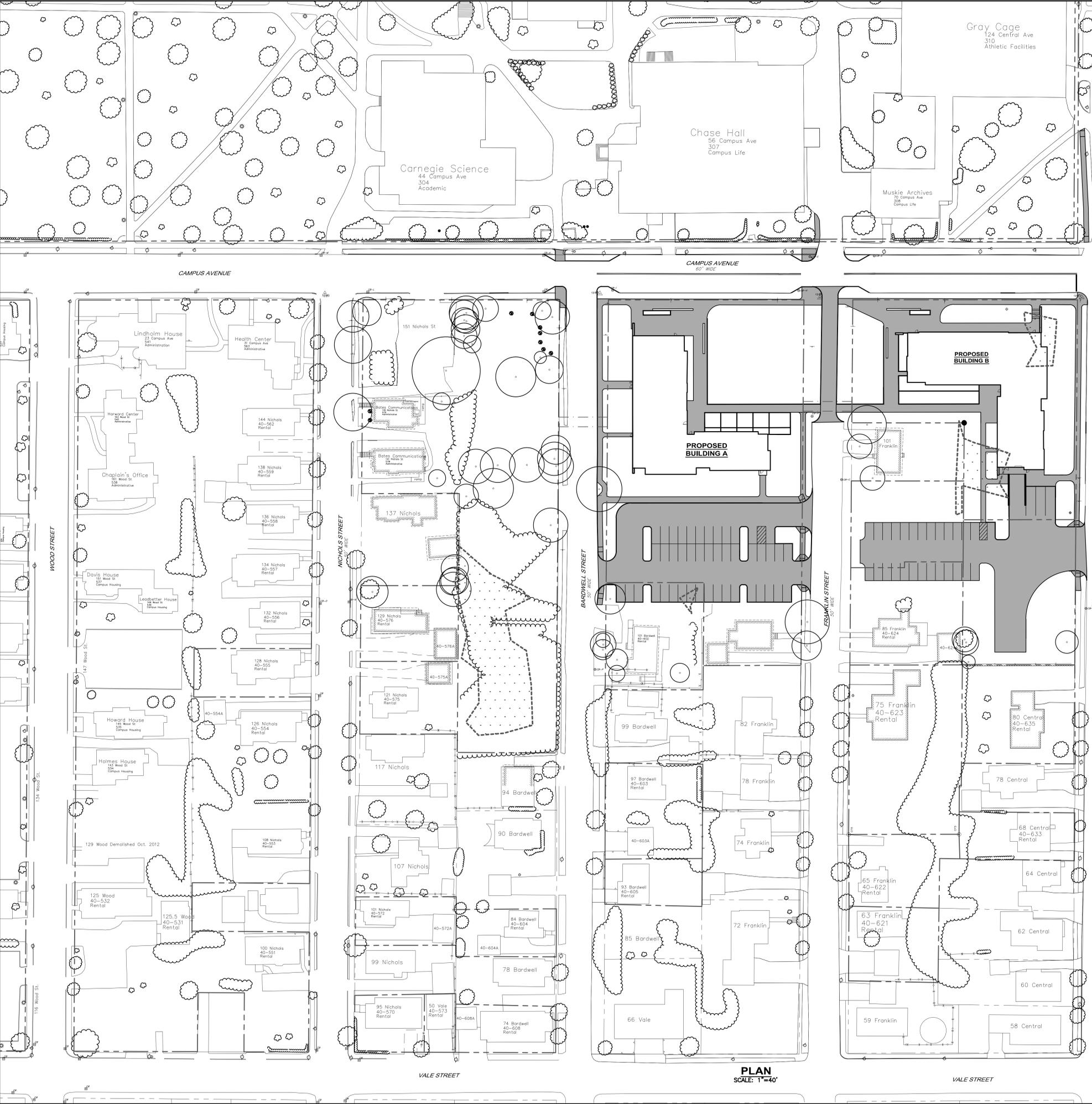
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GENERAL NOTES, LEGEND AND ABBREVIATIONS

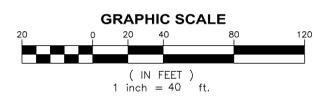
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BATES COLLEGE
 55 & 65 CAMPUS AVENUE HOUSING
 LEWISTON, MAINE 04240
 KEY PLAN
 LEGEND

NO.	REVISION	DATE
4	RESPONSE TO CITY COMMENTS	7-21-14
3	50% D.D. REVIEW	7-2-14
2	CITY OF LEWISTON REV.	6-20-14
1	DEP REVIEW	6-13-14

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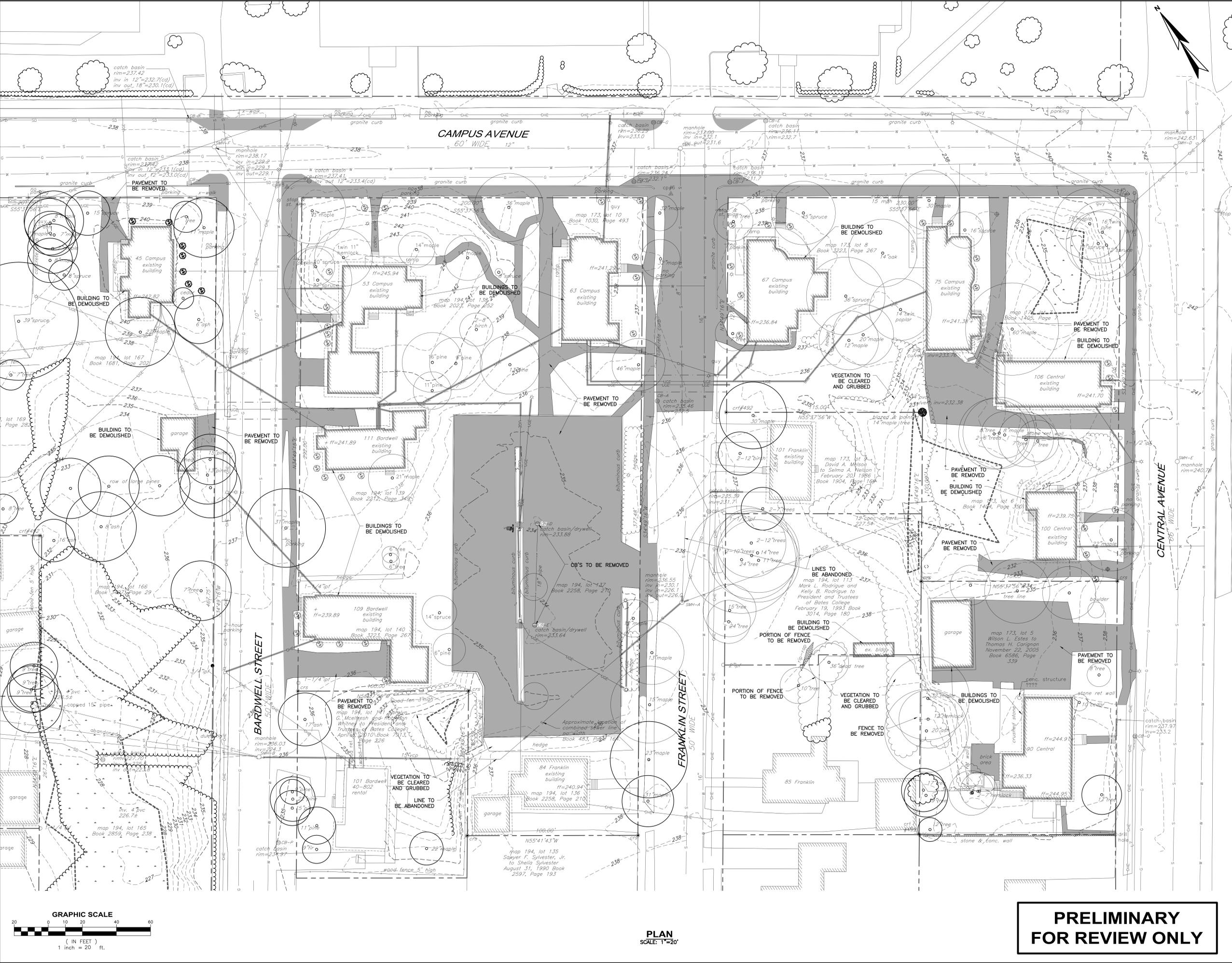
JAN B S WEGMAN
 No. 5852
 LICENSED PROFESSIONAL ENGINEER
 STATE OF MAINE
 7/21/2014

AnnBeha Architects
 33 Kingston Street Boston, MA 02111
 p:617.338.3000 f:617.482.9097

Job Number: 01313.01
 Project: BATES COLLEGE
 Drawn By: MRL Checked By: JBW
 Date: 06.20.2014
 Scale: 1" = 40'
 Drawing Title: **SITE LOCATION PLAN**

C1.1

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BATES COLLEGE
 55 & 65 CAMPUS AVENUE HOUSING

LEWISTON, MAINE 04240

KEY PLAN

LEGEND

4	RESPONSE TO CITY COMMENTS	7-21-14
3	50% D.D. REVIEW	7-2-14
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NO.	REVISION	DATE

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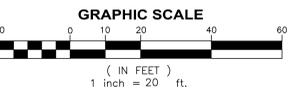


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 Date: 07.20.2014
 Scale: 1" = 20'

EXISTING CONDITIONS & DEMOLITION PLAN

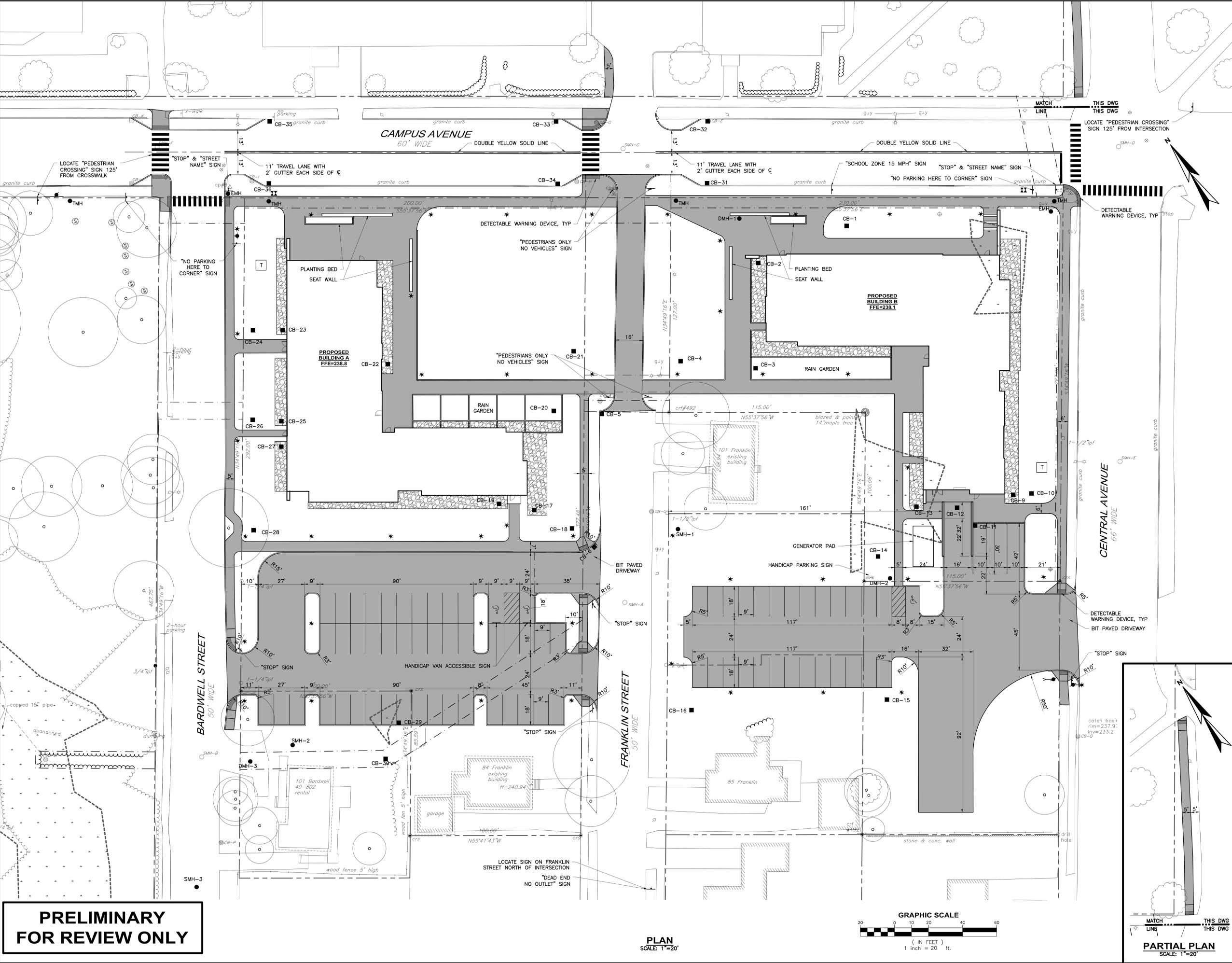
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PLAN SCALE: 1" = 20'

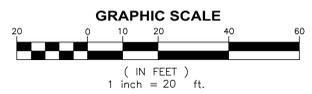
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PLAN
 SCALE: 1"=20'



PARTIAL PLAN
 SCALE: 1"=20'

4	RESPONSE TO CITY COMMENTS	7-21-14
3	50% D.D. REVIEW	7-2-14
2	CITY OF LEWISTON REV.	6-20-14
1	DEP REVIEW	6-13-14
NO.	REVISION	DATE

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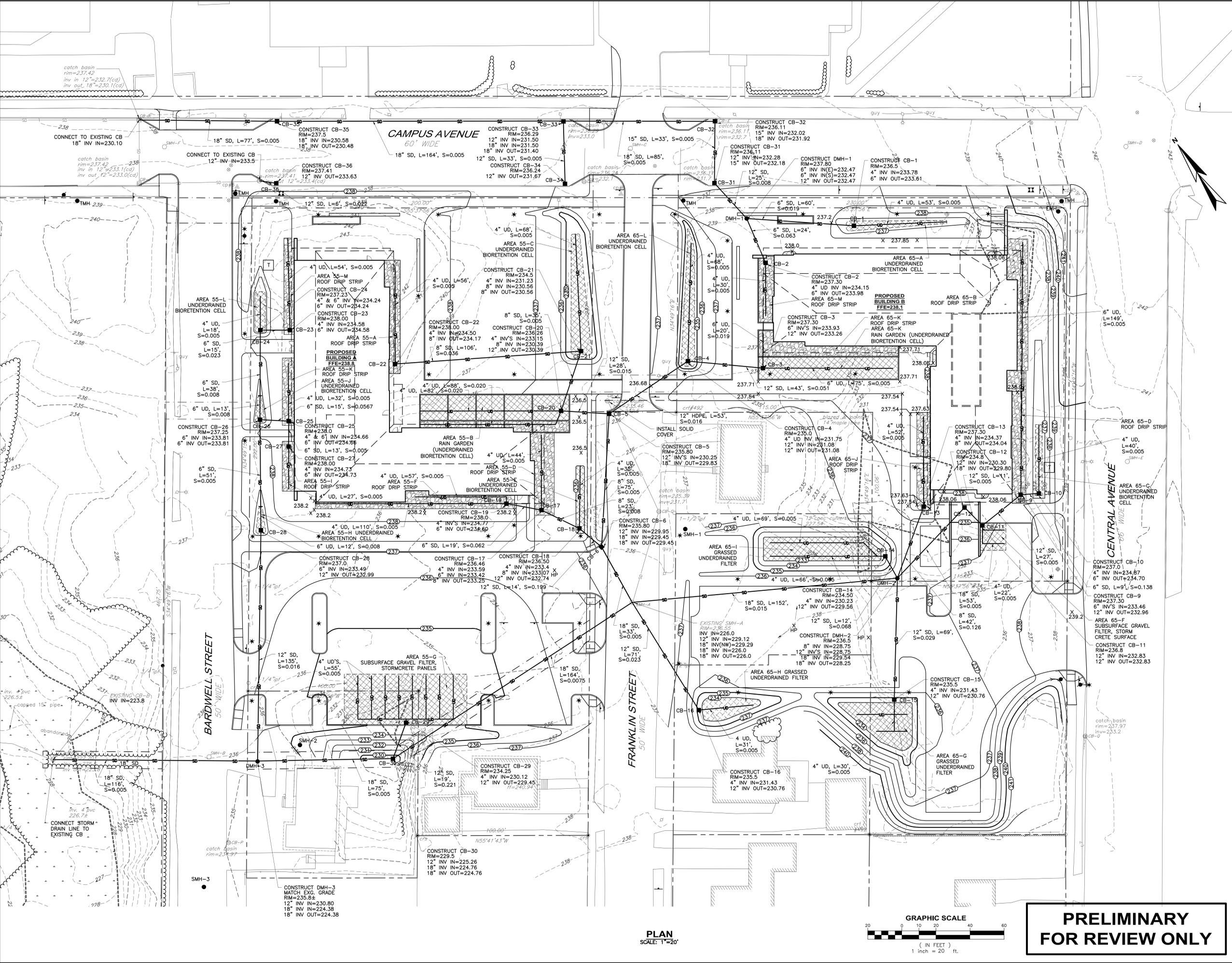


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Job Number: 01313.01
 Project: BATES COLLEGE
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 Scale: 1"=20'
 Drawing Title:
**PROPOSED CONDITIONS
 SITE PLAN**

C2.0

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BATES COLLEGE
 55 & 65 AVENUE HOUSING

LEWISTON, MAINE 04240

KEY PLAN

LEGEND

4	RESPONSE TO CITY COMMENTS	7-21-14
3	50% D.D. REVIEW	7-21-14
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1	DEP REVIEW	6-13-14
NO.	REVISION	DATE

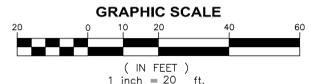
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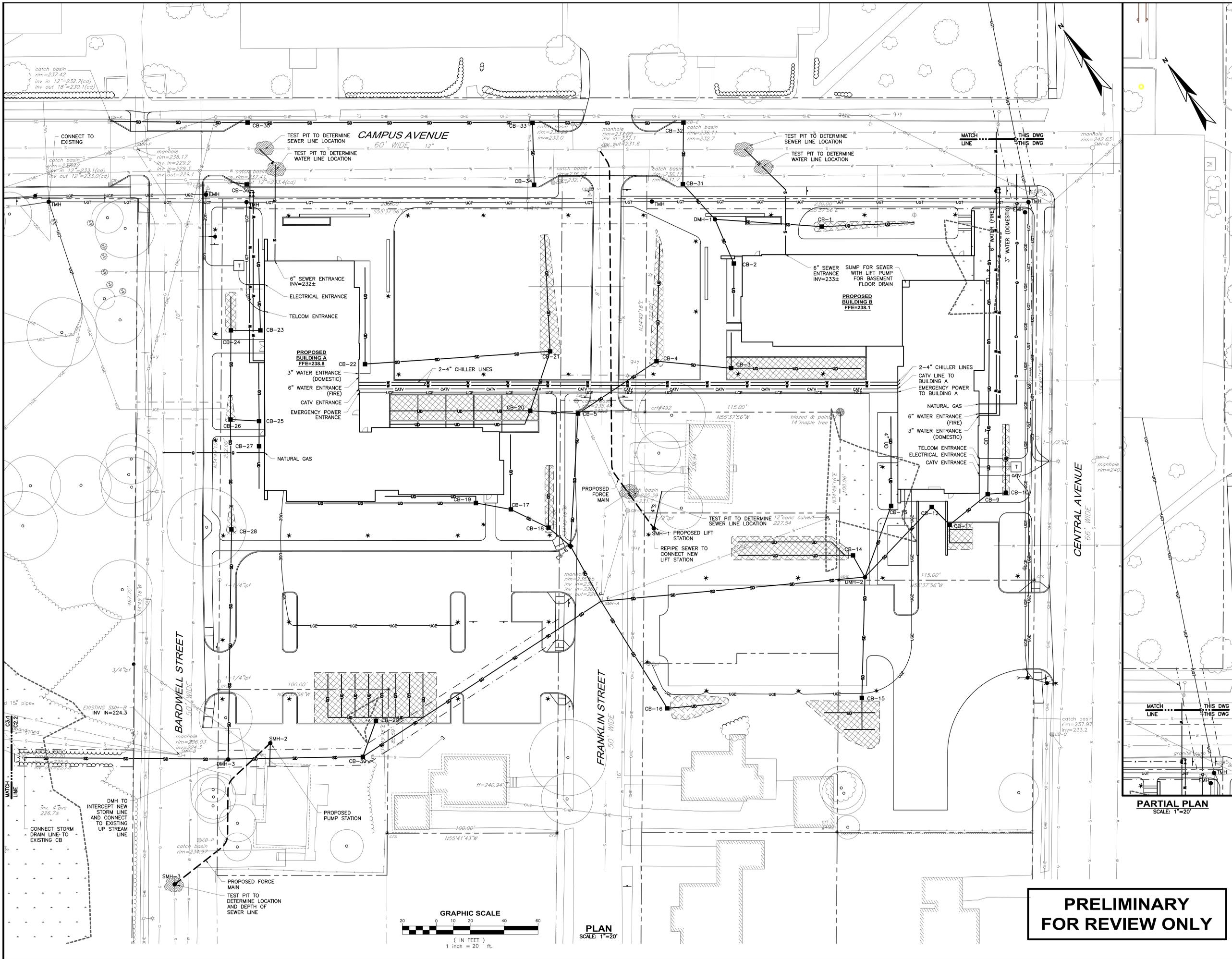
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 Project: BATES COLLEGE
 Drawn By: MRL Checked By: JBW
 Date: 07.21.2014
 Scale: 1"=20'
 Drawing Title: **PROPOSED GRADING & DRAINAGE PLAN**

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C2.1



PLAN
 SCALE: 1"=20'



BATES COLLEGE
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KEY PLAN

LEGEND

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1	DEP REVIEW	6-13-14
NO.	REVISION	DATE

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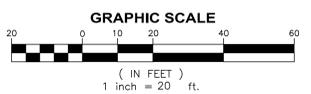
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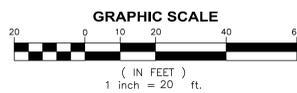
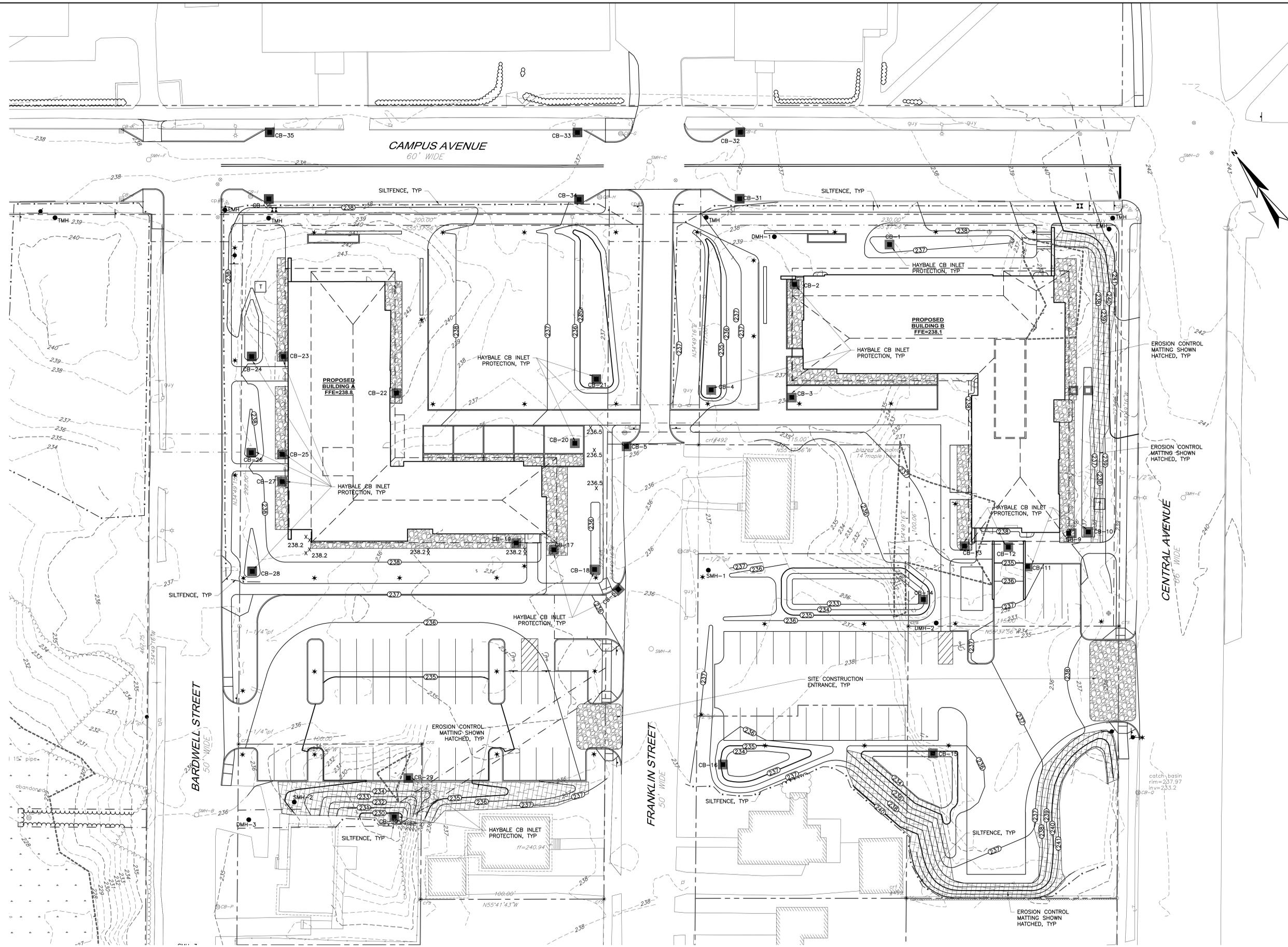
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Project: BATES COLLEGE
Drawn By: MRL Checked By: JBW
Date: 07.02.2014
Scale: 1"=20'
Drawing Title:
PROPOSED SITE UTILITIES PLAN

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PLAN SCALE: 1"=20'

PARTIAL PLAN
SCALE: 1"=20'



PLAN
SCALE: 1"=20'

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4	RESPONSE TO CITY COMMENTS	7-21-14
3	50% D.D. REVIEW	7-2-14
2	CITY OF LEWISTON REV.	6-20-14
1	DEP REVIEW	6-13-14
NO.	REVISION	DATE

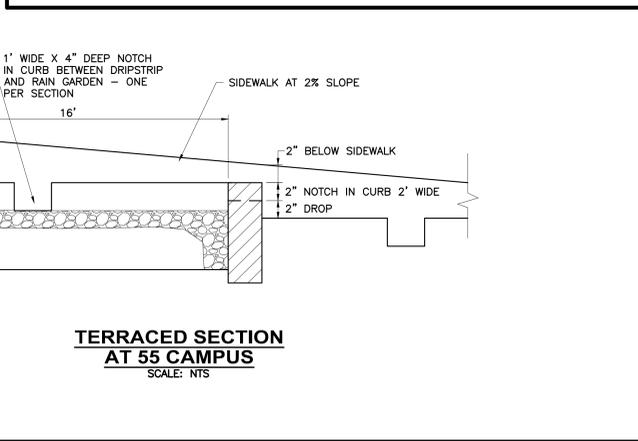
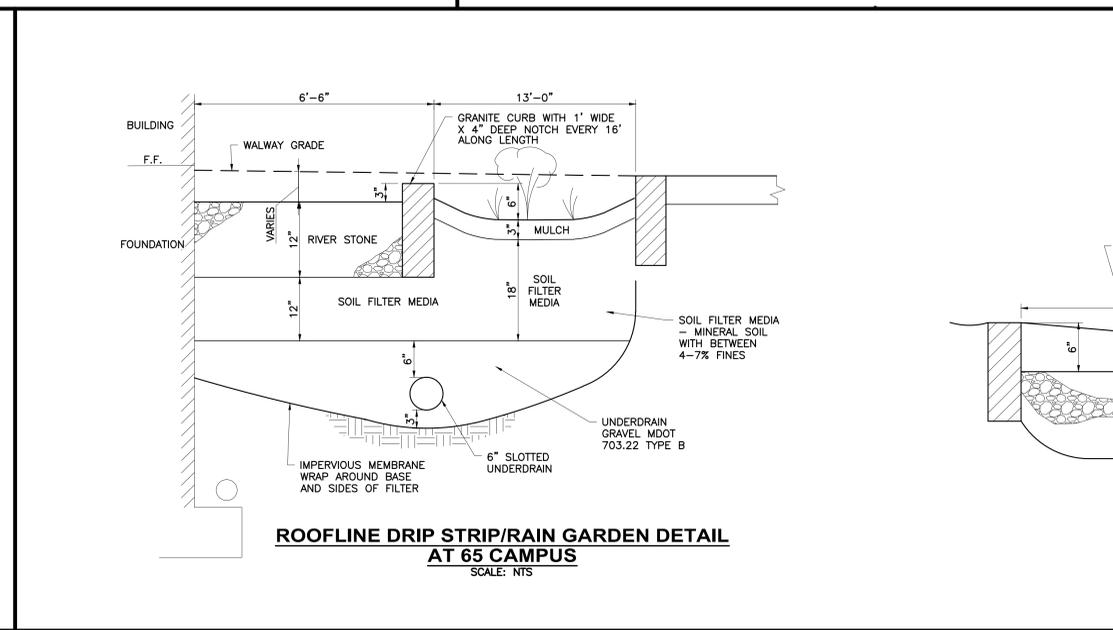
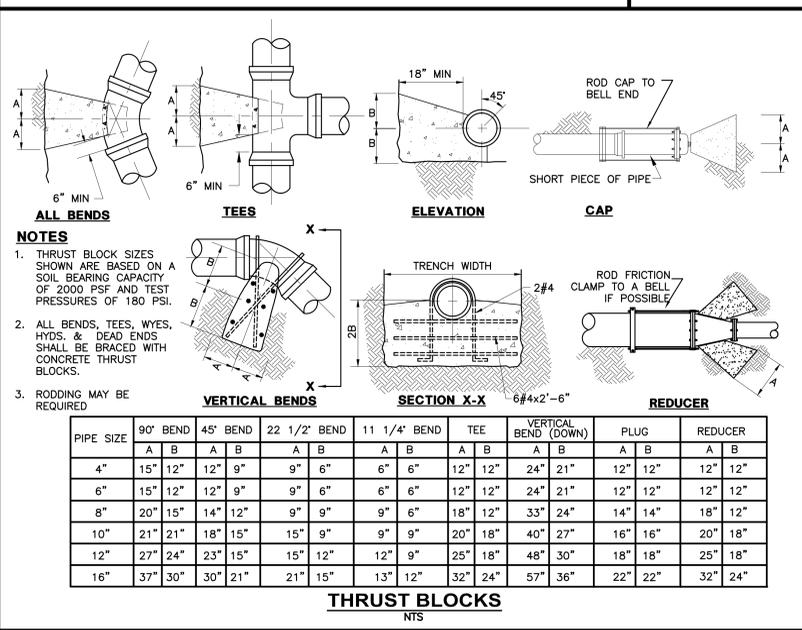
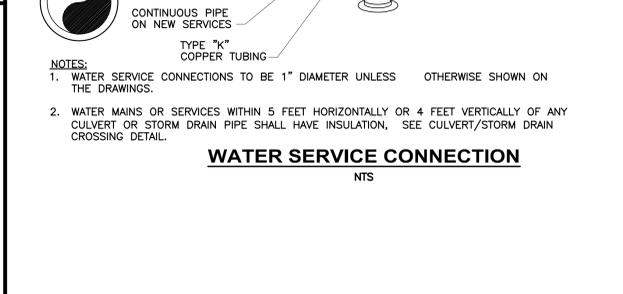
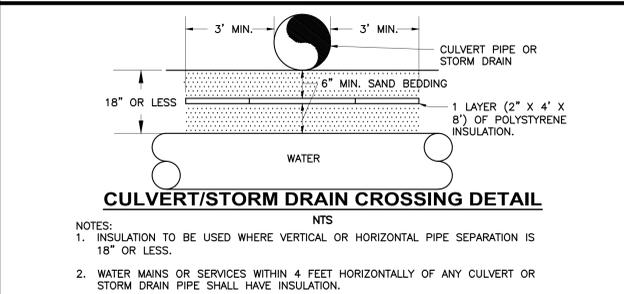
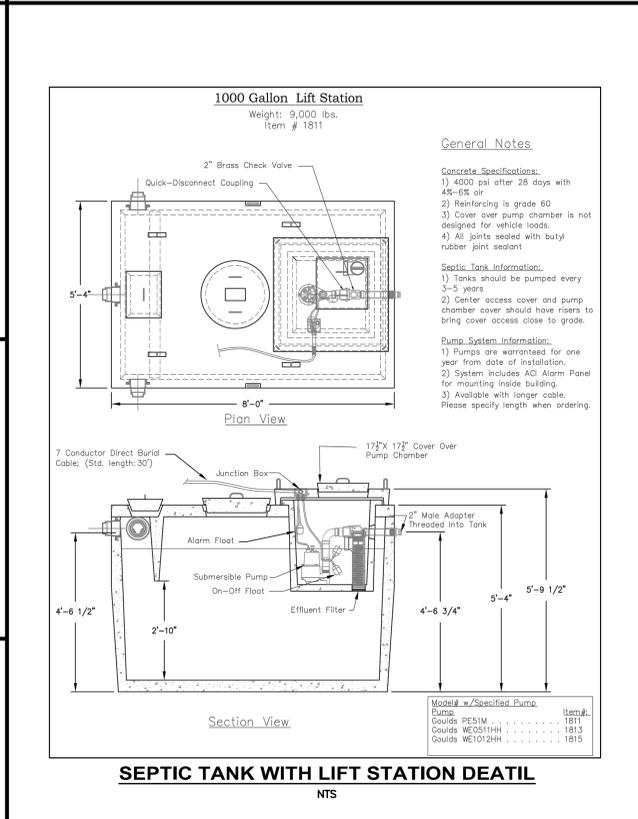
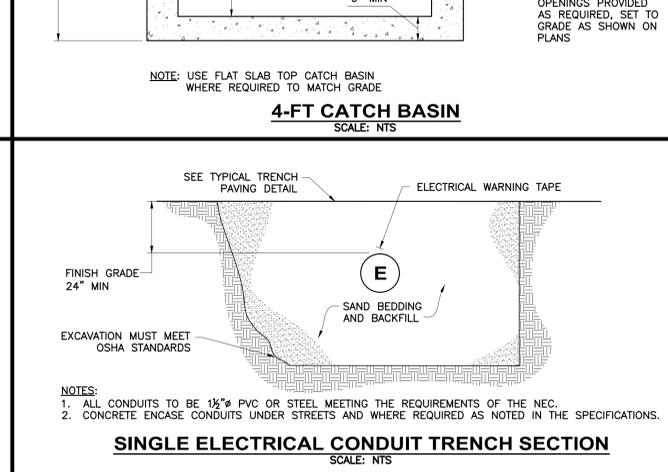
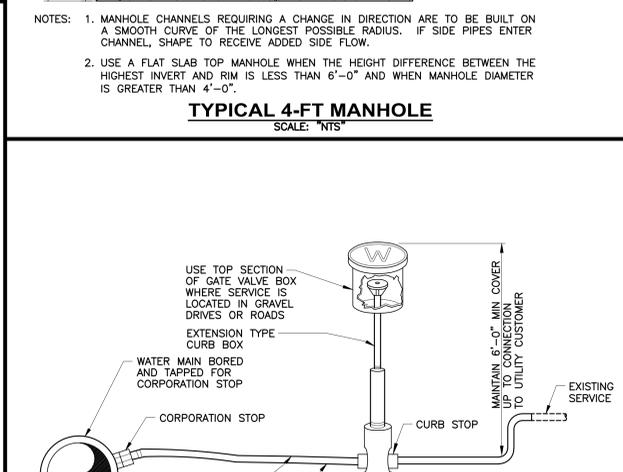
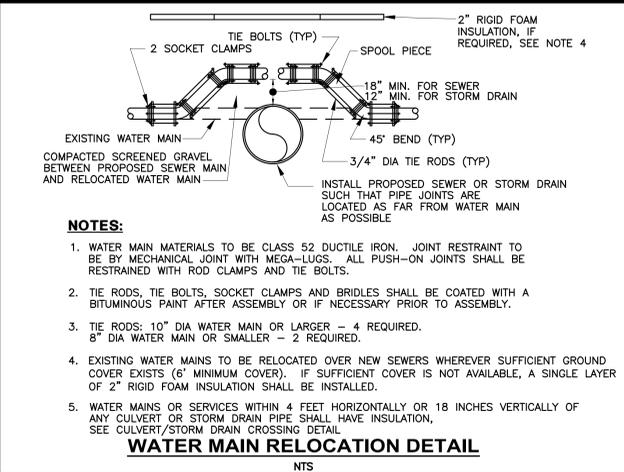
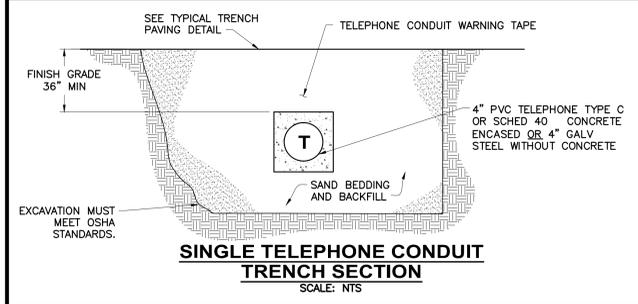
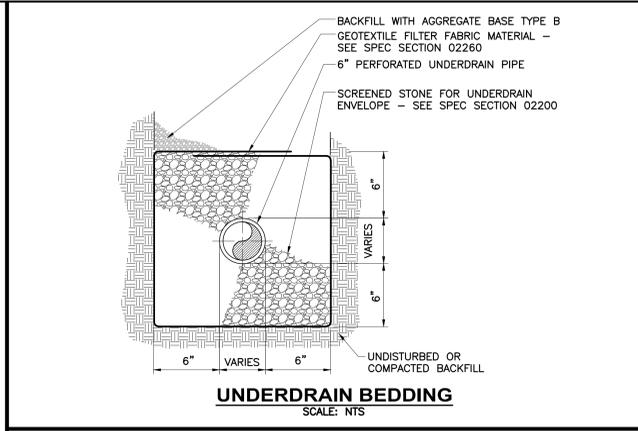
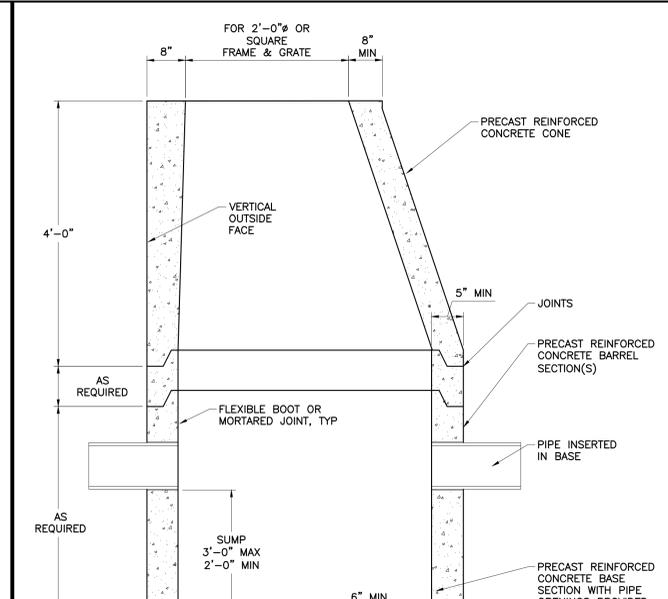
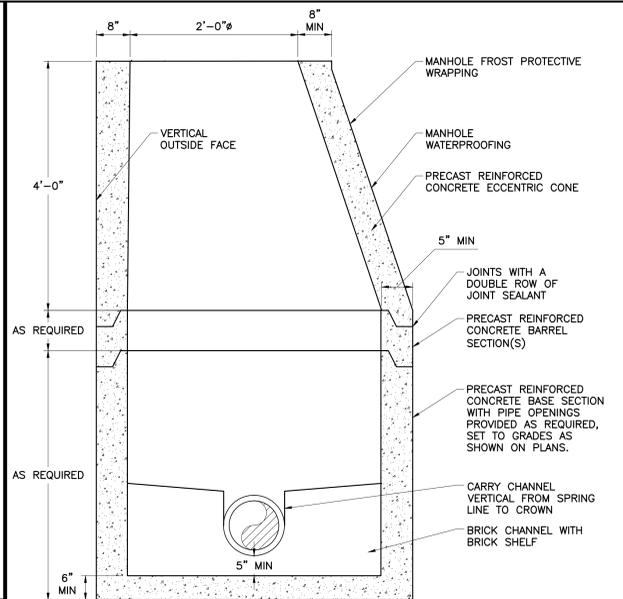
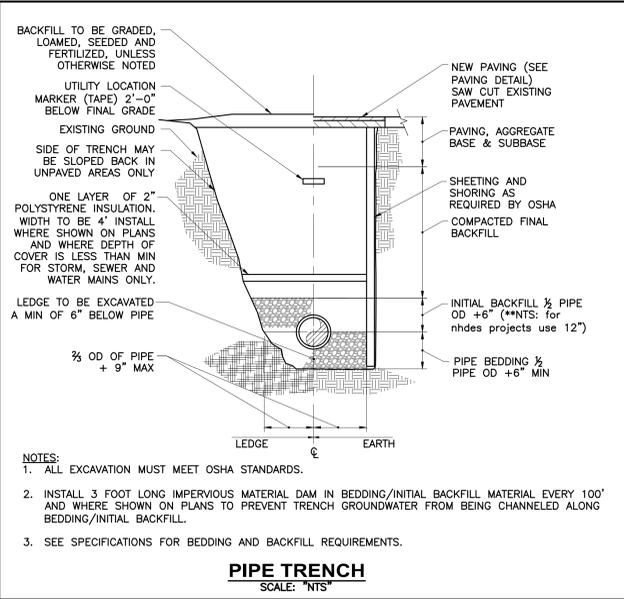
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Project: BATES COLLEGE
Drawn By: MRL Checked By: JBW
Date: 07.02.2014
Scale: 1"=20'
Drawing Title:
SOIL EROSION & SEDIMENT CONTROL PLAN

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BATES COLLEGE
55 & 65 CAMPUS AVENUE HOUSING

LEWISTON, MAINE 04240

KEY PLAN

LEGEND

4	RESPONSE TO CITY COMMENTS	7-21-14
3	50% D.D. REVIEW	7-2-14
2	CITY OF LEWISTON REV.	6-20-14
1	DEP REVIEW	6-13-14
NO.	REVISION	DATE

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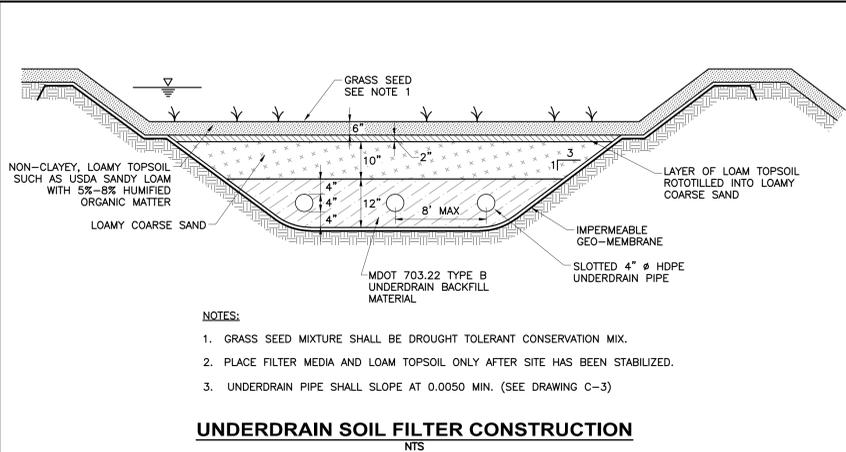
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Job Number: 01313.01
Project: BATES COLLEGE
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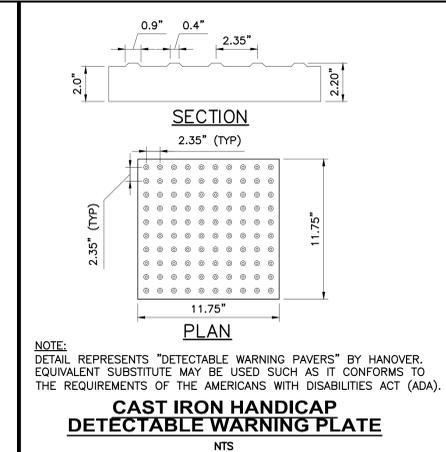
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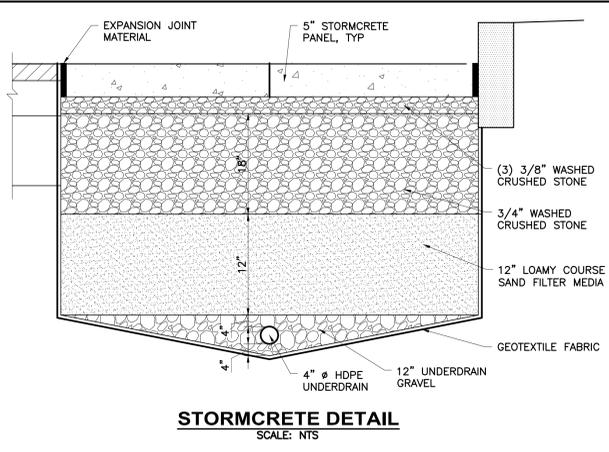
- NOTES:**
- GRASS SEED MIXTURE SHALL BE DROUGHT TOLERANT CONSERVATION MIX.
 - PLACE FILTER MEDIA AND LOAM TOPSOIL ONLY AFTER SITE HAS BEEN STABILIZED.
 - UNDERDRAIN PIPE SHALL SLOPE AT 0.0050 MIN. (SEE DRAWING C-3)

UNDERDRAIN SOIL FILTER CONSTRUCTION
NTS

- POND CONSTRUCTION NOTES:**
- CONSTRUCTION OVERSIGHT:** INSPECTION OF THE FILTER BASINS SHALL BE PROVIDED FOR EACH PHASE OF CONSTRUCTION BY THE DESIGN ENGINEER WITH REQUIRED REPORTING TO THE DEP. AT A MINIMUM, INSPECTIONS WILL OCCUR:
 - AFTER PRELIMINARY CONSTRUCTION OF THE FILTER GRADES AND ONCE THE UNDERDRAIN PIPES ARE INSTALLED BUT NOT BACKFILLED;
 - AFTER THE DRAINAGE LAYER IS CONSTRUCTED AND PRIOR TO THE INSTALLATION OF THE FILTER MEDIA;
 - AFTER THE FILTER MEDIA HAS BEEN INSTALLED AND SEEDED;
 - AFTER ONE YEAR TO INSPECT HEALTH OF THE VEGETATION AND MAKE CORRECTIONS.
 - TESTING AND SUBMITTALS:** ALL MATERIAL USED FOR THE CONSTRUCTION OF THE FILTER BASIN WILL BE APPROVED BY THE DESIGN ENGINEER AFTER TESTS BY A CERTIFIED LABORATORY SHOW THAT THEY ARE PASSING DEP SPECIFICATIONS. THE CONTRACTOR SHALL IDENTIFY THE LOCATION OF THE SOURCE OF EACH COMPONENT OF THE FILTER MATERIAL. ALL RESULTS OF FIELD AND LABORATORY TESTING SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR CONFIRMATION. THE CONTRACTOR SHALL:
 - SUBMIT SAMPLES OF EACH TYPE OF MATERIAL TO BE USED FOR THE FILTER MATERIAL AND SAMPLES OF THE UNDERDRAIN BEDDING MATERIAL. SAMPLES MUST BE A COMPOSITE OF THREE DIFFERENT LOCATIONS (GRABS) FROM THE STOCKPILE OR PIT FACE. SAMPLE SIZE REQUIRED WILL BE DETERMINED BY THE TESTING LABORATORY.
 - PERFORM A SIEVE ANALYSIS CONFORMING TO ASTM C136 (STANDARD TEST METHOD FOR SIEVE ANALYSIS OF FINE AND COARSE AGGREGATES; 1996A) ON THE UNDERDRAIN BEDDING MATERIAL.
 - PERFORM A PERMEABILITY TEST ON THE SOIL FILTER MEDIA MIXTURE CONFORMING TO ASTM D2434 WITH THE MIXTURE COMPACTED TO 90- 92% OF MAXIMUM DRY DENSITY BASED ON ASTM D698.



CAST IRON HANDICAP DETECTABLE WARNING PLATE
NTS



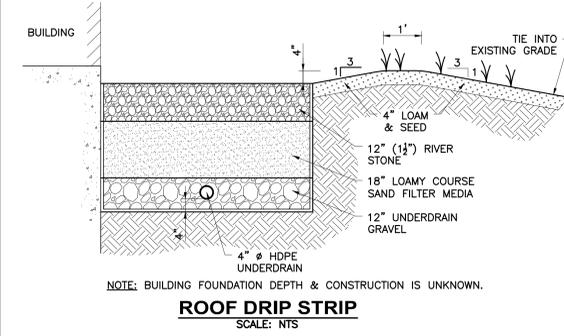
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BATES COLLEGE
55 & 65 CAMPUS AVENUE HOUSING

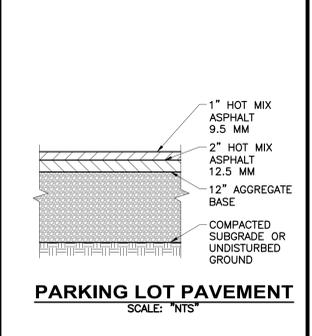
LEWISTON, MAINE 04240

KEY PLAN

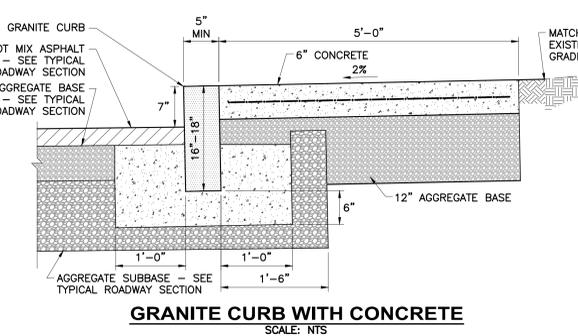
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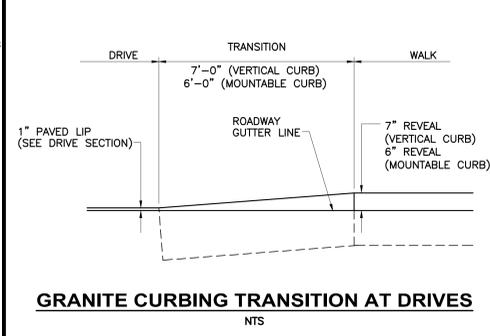
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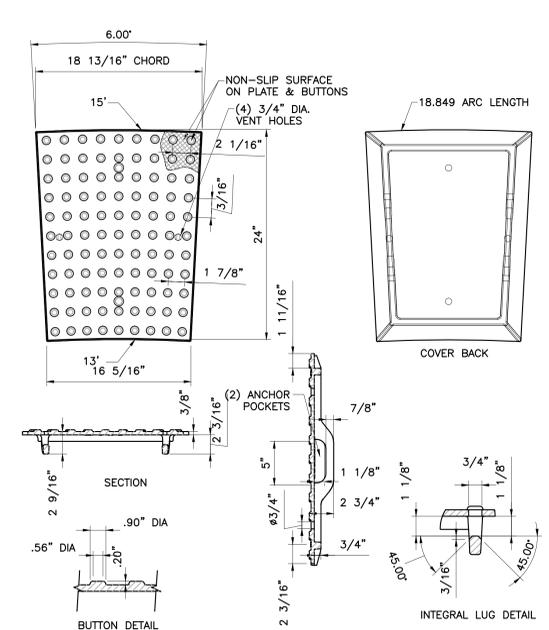
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GRANITE CURB WITH CONCRETE
SCALE: NTS

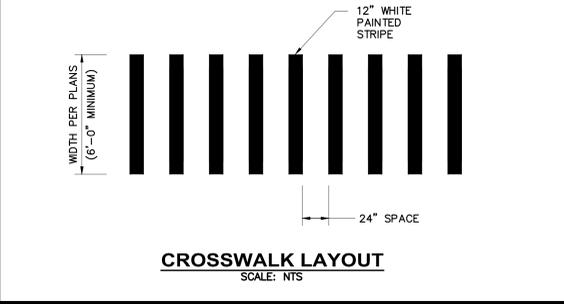


GRANITE CURBING TRANSITION AT DRIVES
NTS

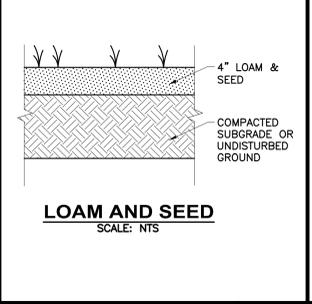


15 FT RADIUS CAST IRON DETECTABLE WARNING PLATE
NTS

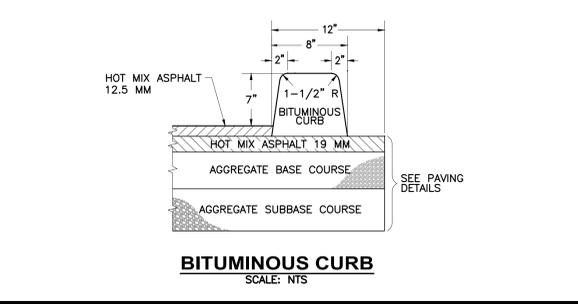
- NOTES:**
- REFER TO RECTANGULAR DETECTABLE WARNING DEVICE DETAIL. THIS DRAWING FOR EMBEDMENT AND CONCRETE SURROUND DETAILS.
 - TAKES FIFTEEN (15) PLATES TO COMPLETE FULL 90° TURN.
 - DETECTABLE WARNING PLATES TO BE BEDDED IN CONCRETE.



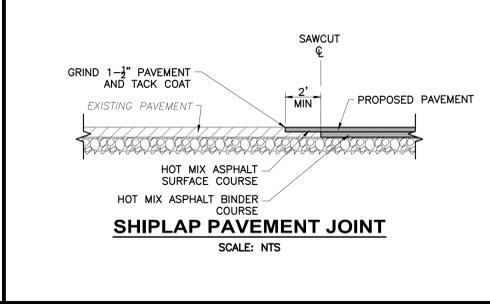
CROSSWALK LAYOUT
SCALE: NTS



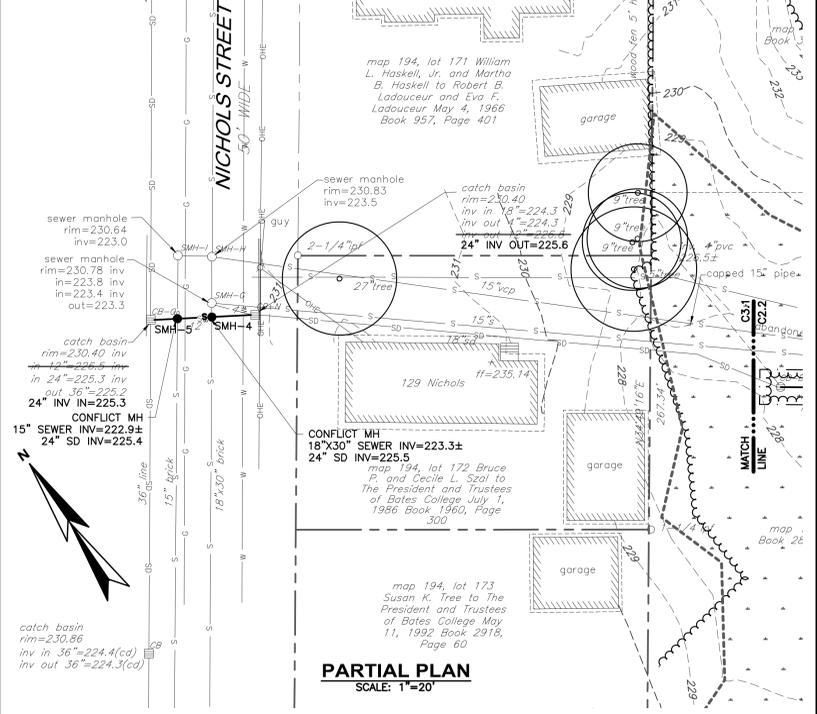
LOAM AND SEED
SCALE: NTS



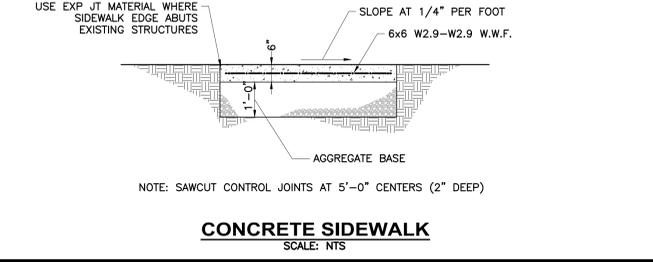
BITUMINOUS CURB
SCALE: NTS



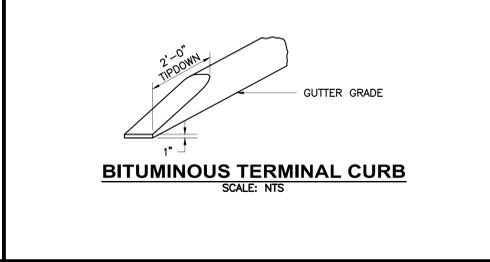
SHIPLAP PAVEMENT JOINT
SCALE: NTS



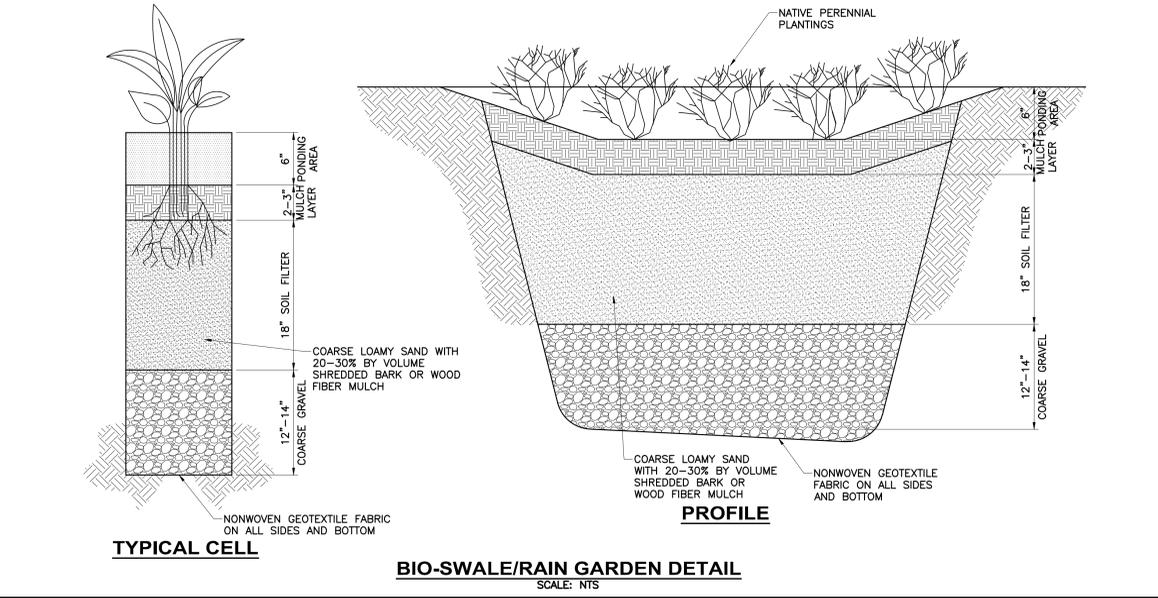
PARTIAL PLAN
SCALE: 1"=20'



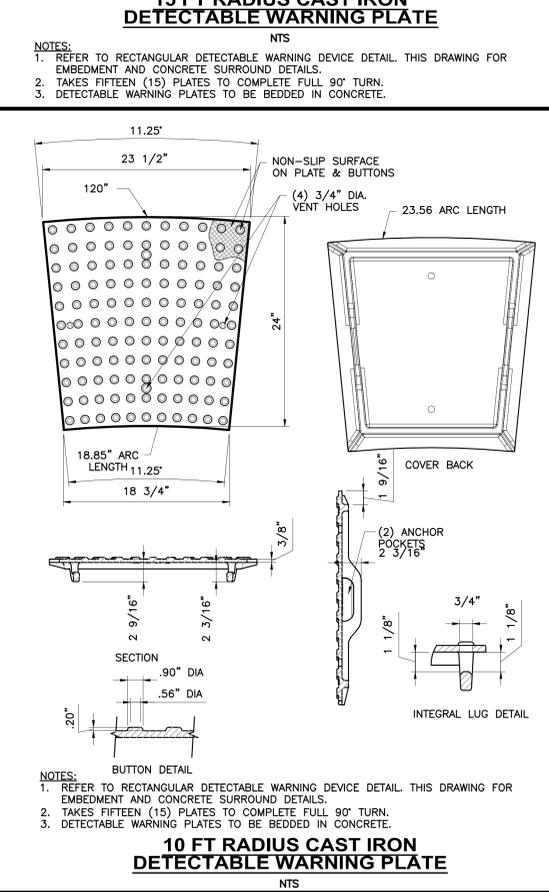
CONCRETE SIDEWALK
SCALE: NTS



BITUMINOUS TERMINAL CURB
SCALE: NTS



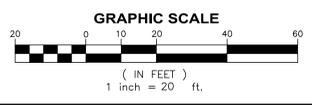
BIO-SWALE/RAIN GARDEN DETAIL
SCALE: NTS



10 FT RADIUS CAST IRON DETECTABLE WARNING PLATE
NTS

- NOTES:**
- REFER TO RECTANGULAR DETECTABLE WARNING DEVICE DETAIL. THIS DRAWING FOR EMBEDMENT AND CONCRETE SURROUND DETAILS.
 - TAKES FIFTEEN (15) PLATES TO COMPLETE FULL 90° TURN.
 - DETECTABLE WARNING PLATES TO BE BEDDED IN CONCRETE.

PRELIMINARY FOR REVIEW ONLY



4	RESPONSE TO CITY COMMENTS	7-21-14
3	50% D.D. REVIEW	7-2-14
2	CITY OF LEWISTON REV.	6-20-14
1	DEP REVIEW	6-13-14
NO.	REVISION	DATE

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Job Number: 01313.01
Project: BATES COLLEGE
Drawn By: MRL Checked By: JBW
Date: 07.02.2014
Scale: 1"=20'
Drawing Title: DETAILS II

C3.1

EROSION AND SEDIMENTATION CONTROL NOTES

THIS PLAN HAS BEEN DEVELOPED AS A STRATEGY TO CONTROL SOIL EROSION AND SEDIMENTATION DURING AND AFTER CONSTRUCTION. THIS PLAN IS BASED ON THE STANDARDS AND SPECIFICATIONS FOR EROSION PREVENTION IN DEVELOPING AREAS AS CONTAINED IN THE "MAINE EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES", MAINE DEPARTMENT OF ENVIRONMENTAL PROTECTION DATED MARCH 2003.

THE PROPOSED LOCATIONS OF SILTATION AND EROSION CONTROL STRUCTURES ARE SHOWN ON THE SITE PLAN.

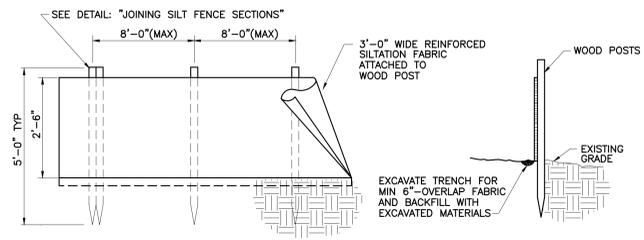
- ALL SEDIMENT AND EROSION CONTROL MEASURES SHALL BE DONE IN ACCORDANCE WITH THE "MAINE EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES", MAINE DEPARTMENT OF ENVIRONMENTAL PROTECTION, DATED MARCH 2003.
- THOSE AREAS UNDERGOING ACTUAL CONSTRUCTION WILL BE MAINTAINED IN AN UNTREATED OR UNVEGETATED CONDITION FOR THE MINIMUM TIME REQUIRED. IN GENERAL AREAS TO BE VEGETATED SHALL BE PERMANENTLY STABILIZED WITHIN 15 DAYS OF FINAL GRADING AND TEMPORARILY STABILIZED WITHIN 30 DAYS OF INITIAL DISTURBANCE OF THE SOIL.
- SEDIMENT BARRIERS (SILT FENCE, STONE CHECK DAMS, ETC.) SHOULD BE INSTALLED PRIOR TO ANY SOIL DISTURBANCE OF UPGRADIENT DRAINAGE AREAS.
- INSTALL SILT FENCE AT TOE OF SLOPES TO FILTER SILT FROM RUNOFF. SEE SILT FENCE DETAIL FOR PROPER INSTALLATION. SILT FENCE WILL REMAIN IN PLACE PER NOTE #5.
- ALL EROSION CONTROL STRUCTURES WILL BE INSPECTED, REPLACED AND/OR REPAIRED EVERY 7 DAYS AND IMMEDIATELY FOLLOWING ANY SIGNIFICANT RAINFALL OR SNOW MELT OR WHEN NO LONGER SERVICEABLE DUE TO SEDIMENT ACCUMULATION OR DECOMPOSITION. SEDIMENT DEPOSITS MUST BE REMOVED WHEN THEY REACH APPROXIMATELY ONE HALF THE HEIGHT OF THE BARRIER. SEDIMENT CONTROL DEVICES SHALL REMAIN IN PLACE AND BE MAINTAINED BY THE CONTRACTOR UNTIL AREAS UPSLOPE ARE PERMANENTLY STABILIZED.
- NO SLOPES, EITHER PERMANENT OR TEMPORARY, SHALL BE STEEPER THAN TWO HORIZONTAL TO ONE VERTICAL (2 TO 1) UNLESS STABILIZED WITH RIPRAP OR OTHER STRUCTURAL MEANS.
- IF FINAL SEEDING AND SODDING IS NOT EXPECTED PRIOR TO THE ANTICIPATED DATE OF THE FIRST KILLING FROST, USE TEMPORARY ANNUAL RYEGRASS SEEDING AND MULCHING ON ROUGH GRADED SUBSOIL TO PROTECT THE SITE AND DELAY PERMANENT LOAMING, FINE GRADING, AND SEEDING OR SODDING UNTIL SPRING.
- WHEN FEASIBLE, TEMPORARY SEEDING OF DISTURBED AREAS THAT HAVE NOT BEEN FINISH GRADED SHALL BE COMPLETED 30 DAYS PRIOR TO THE FIRST KILLING FROST.
- DURING THE CONSTRUCTION PHASE, INTERCEPTED SEDIMENT WILL BE RETURNED TO THE SITE AND REGRADED ONTO OPEN AREAS. POST SEEDING SEDIMENT, IF ANY, WILL BE DISPOSED OF IN AN ACCEPTABLE MANNER.
- REVEGETATION MEASURES WILL COMMENCE UPON COMPLETION OF CONSTRUCTION EXCEPT AS NOTED ABOVE. ALL DISTURBED AREAS NOT OTHERWISE STABILIZED WILL BE GRADED, SMOOTHED, AND REVEGETATED.
- ALL TEMPORARY EROSION CONTROL MEASURES SHALL BE REMOVED ONCE THE SITE IS STABILIZED.
- STABILIZATION SCHEDULE BEFORE WINTER:
 - SEPTEMBER 15** ALL DISTURBED AREAS MUST BE SEEDING AND MULCHED. ALL SLOPES MUST BE STABILIZED, SEEDING AND MULCHED. SLOPES 3:1 OR GREATER TO BE STABILIZED WITH EROSION CONTROL MATTING AND SEEDING. ALL DISTURBED AREAS TO BE PROTECTED WITH AN ANNUAL GRASS MUST BE SEEDING AT A SEEDING RATE OF 3 POUNDS PER 1,000 SQUARE FEET AND MULCHED.
 - OCTOBER 1** ALL GRASS-LINED DITCHES AND CHANNELS MUST BE STABILIZED WITH MULCH OR EROSION CONTROL BLANKET.
 - NOVEMBER 15** ALL STONE-LINED DITCHES AND CHANNELS MUST BE CONSTRUCTED AND STABILIZED. SLOPES THAT ARE COVERED WITH RIPRAP MUST BE CONSTRUCTED BY THAT DATE.
 - DECEMBER 1** ALL DISTURBED AREAS WHERE THE GROWTH OF VEGETATION FAILS TO BE AT LEAST THREE INCHES TALL OR AT LEAST 75% OF THE DISTURBED SOIL IS COVERED BY VEGETATION, MUST BE PROTECTED FOR OVER-WINTER.

EROSION CONTROL - WINTER CONSTRUCTION

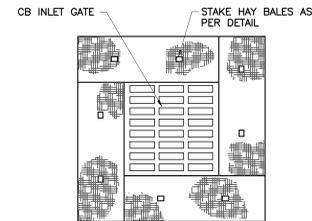
- WINTER CONSTRUCTION PERIOD DEFINED: NOVEMBER 1 THROUGH APRIL 15
- WINTER EXCAVATION AND EARTHWORK SHALL BE DONE SUCH THAT NO MORE THAN 1 ACRE OF THE SITE IS WITHOUT STABILIZATION AT ANY ONE TIME.
- EXPOSED AREA SHOULD BE LIMITED SUCH THAT THE AREA CAN BE MULCHED IN ONE DAY PRIOR TO ANY SNOW EVENT.
- CONTINUATION OF EARTHWORK OPERATIONS ON ADDITIONAL AREAS SHALL NOT BEGIN UNTIL THE EXPOSED SOIL SURFACE ON THE AREA BEING WORKED HAS BEEN STABILIZED SUCH THAT NO LARGER AREA OF THE SITE IS WITHOUT EROSION CONTROL PROTECTION AS LISTED IN ITEM 2 ABOVE.
- AN AREA SHALL BE CONSIDERED TO HAVE BEEN STABILIZED WHEN EXPOSED SURFACES HAVE BEEN EITHER MULCHED WITH STRAW AT A RATE OF 100 LB. PER 1,000 SQUARE FEET (WITH OR WITHOUT SEEDING) OR DORMANT SEEDING, MULCHED AND ADEQUATELY ANCHORED BY AN APPROVED ANCHORING TECHNIQUE. IN ALL CASES, MULCH SHALL BE APPLIED SUCH THAT SOIL SURFACE IS NOT VISIBLE THROUGH THE MULCH.
- BETWEEN THE DATES OF OCTOBER 15 AND APRIL 1ST, LOAM OR SEED WILL NOT BE REQUIRED. DURING PERIODS OF ABOVE-FREEZING TEMPERATURES, THE SLOPES SHALL BE FINE GRADED AND EITHER PROTECTED WITH MULCH OR TEMPORARILY SEEDING AND MULCHED UNTIL SUCH TIME AS THE FINAL TREATMENT CAN BE APPLIED. IF THE DATE IS AFTER NOVEMBER 1ST AND IF THE EXPOSED AREA HAS BEEN LOAMED, FINAL GRADED AND IS SMOOTH, THEN THE AREA MUST BE STABILIZED WITH MULCH. IF CONSTRUCTION CONTINUES DURING FREEZING WEATHER, ALL EXPOSED AREAS SHALL BE GRADED BEFORE FREEZING AND THE SURFACE TEMPORARILY PROTECTED FROM EROSION BY THE APPLICATION OF MULCH. SLOPES SHALL NOT BE LEFT EXPOSED OVER THE WINTER OR ANY OTHER EXTENDED TIME OF WORK SUSPENSION UNLESS TREATED IN THE ABOVE MANNER. UNTIL SUCH TIME AS WEATHER CONDITIONS ALLOW DITCHES TO BE FINISHED WITH THE PERMANENT SURFACE TREATMENT, EROSION SHALL BE CONTROLLED BY THE INSTALLATION OF BALES OF HAY OR STONE CHECK DAMS IN ACCORDANCE WITH THE STANDARD DETAILS.
- THE APPLICATION OF MULCH TO FINE GRADED AREAS WILL BE STABILIZED AS FOLLOWS:
 - A) BETWEEN THE DATES OF NOVEMBER 1ST AND APRIL 15TH ALL MULCH SHALL BE ANCHORED BY EITHER PEG LINE, MULCH NETTING, ASPHALT EMULSION, CHEMICAL TACK OR WOOD CELLULOSE FIBER.
 - B) MULCH NETTING SHALL BE USED TO ANCHOR MULCH IN ALL DRAINAGEWAYS WITH A SLOPE GREATER THAN 3% FOR SLOPES EXPOSED TO DIRECT WINDS AND FOR ALL OTHER SLOPES GREATER THAN 8%.
 - C) MULCH NETTING SHALL BE USED TO ANCHOR MULCH IN ALL AREAS WITH SLOPES GREATER THAN 15%. AFTER OCTOBER 1ST, THE SAME APPLIES FOR ALL SLOPES GREATER THAN 8%.
- AFTER NOVEMBER 1ST THE CONTRACTOR SHALL APPLY MULCH AND ANCHORING ON ALL BARE EARTH AT THE END OF EACH WORKING DAY.
- DURING WINTER CONSTRUCTION PERIODS ALL SNOW SHALL BE REMOVED FROM AREAS OF MULCHING PRIOR TO PLACEMENT.

EROSION CONTROL - WETLAND NOTES

- WETLANDS AND SURFACE WATERS (EXCEPTING THOSE WHICH ARE TO BE FILLED IN ACCORDANCE WITH STATE AND FEDERAL REGULATIONS) WILL BE PROTECTED WITH SILT FENCE INSTALLED AT THE EDGE OF THE WETLAND OR THE BOUNDARY OF WETLAND DISTURBANCE.
- IF THE WORK INCLUDES CROSSING OF WETLANDS AND/OR STREAMS, THE CONTRACTOR SHALL TAKE SPECIAL PRECAUTIONS WORKING IN THESE AREAS.
- ANY WETLAND CROSSING WORK SHALL BE COMPLETED BETWEEN THE PERIOD OF MAY 1 AND SEPTEMBER 30.
- ALL EROSION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO COMMENCING CONSTRUCTION WITHIN OR ADJACENT TO WETLAND AREAS.
- WETLAND VEGETATIVE LAYERS SHALL BE REMOVED AND SALVAGED FOR RESTORATION OF THE DISTURBED AREAS.
- STORAGE AREAS FOR WETLAND MATERIALS SHALL BE PROPERLY PROTECTED AGAINST EROSION.
- SEEDING OF THE DISTURBED AREAS WITHIN WETLAND AREAS SHALL UTILIZE MIXTURES APPROPRIATE FOR WETLAND AREAS AS OUTLINED IN THE SPECIFICATIONS.

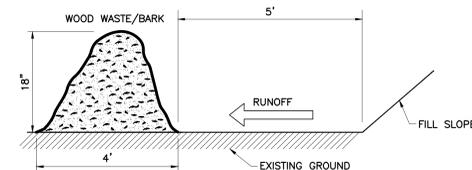


SILT FENCE INSTALLATION DETAIL
SCALE: "NTS"

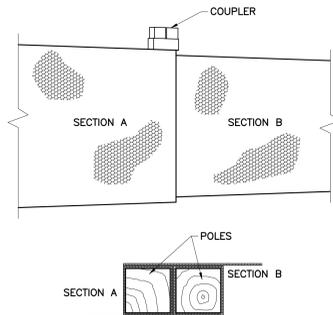


NOTE: EMBED HAYBALES MINIMUM OF 4".

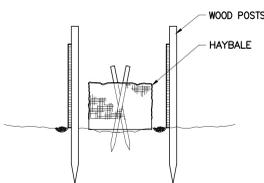
HAY BALE CB INLET PROTECTION
SCALE: "NTS"



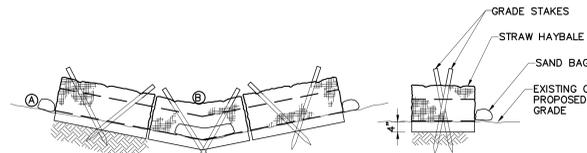
WOOD WASTE/BARK FILTER BERM
SCALE: "NTS"



JOINING SILT FENCE SECTIONS
SCALE: "NTS"

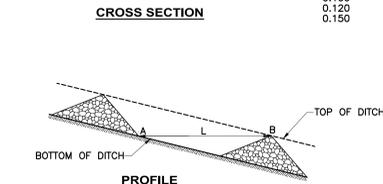


COMBINATION SILT FENCE AND HAY BALE BARRIER
SCALE: "NTS"

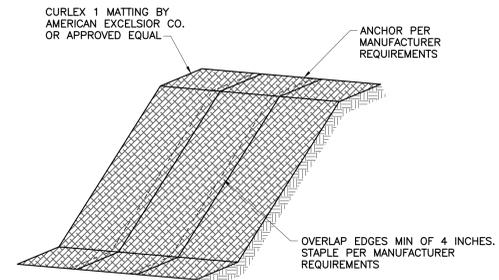


STRAW HAY BALE CHECK DAM
SCALE: "NTS"

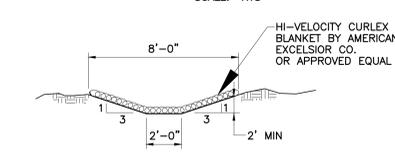
DITCH SLOPE (FT/FT)	L (FT)
0.020	100
0.030	66
0.040	50
0.050	40
0.060	25
0.100	20
0.120	17
0.150	13



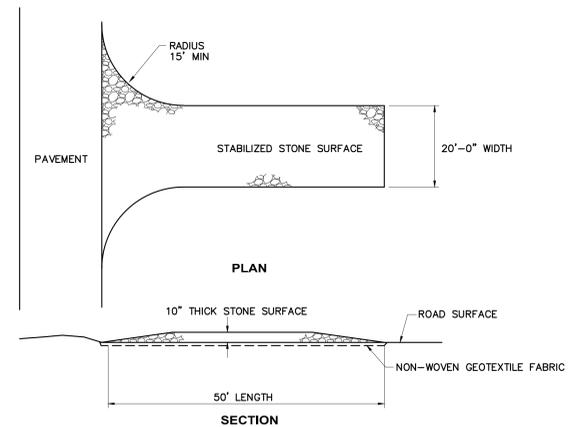
STONE CHECK DAM DETAIL
SCALE: "NTS"



INSTALL ON SLOPES 3:1 OR GREATER
EROSION CONTROL MATTING - SLOPES
SCALE: "NTS"



EROSION CONTROL MATTING - DITCHES
SCALE: "NTS"



(TEMPORARY, TO BE REMOVED PRIOR TO FINAL SITE PAVING)
STABILIZED CONSTRUCTION ENTRANCE
SCALE: "NTS"

PRELIMINARY FOR REVIEW ONLY

NO.	REVISION	DATE
4	RESPONSE TO CITY COMMENTS	7-21-14
3	50% D.D. REVIEW	7-2-14
2	CITY OF LEWISTON REV.	6-20-14
1	DEP REVIEW	6-13-14

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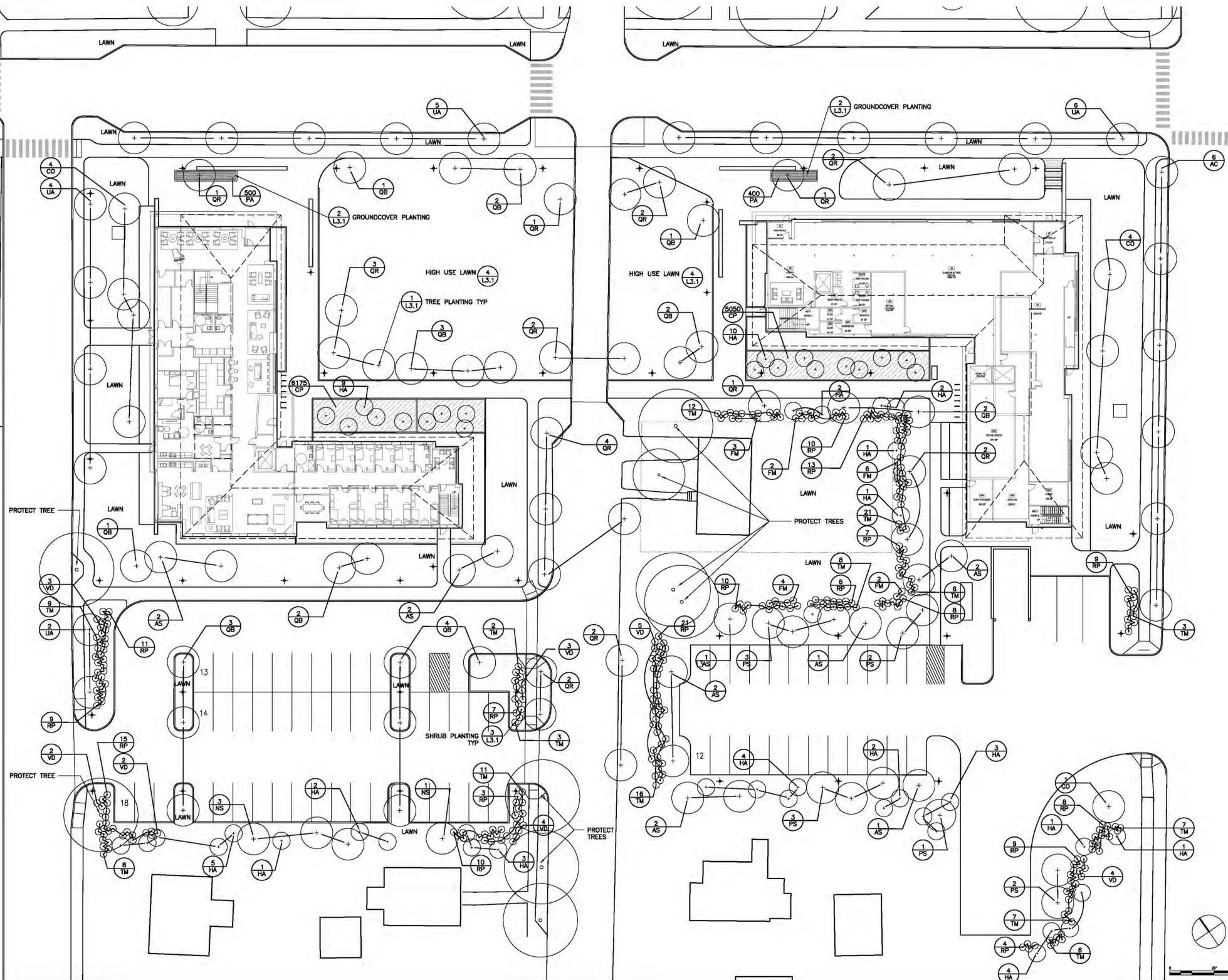
Job Number: 01313.01
Project: BATES COLLEGE
Drawn By: MRL Checked By: JBW
Date: 07.02.2014
Scale: 1"=20'
Drawing Title:
EROSION CONTROL DETAILS

PLANT SCHEDULE - BASE SCOPE						
KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE	ROOT	COMMENTS
LARGE CANOPY TREES - STANDARD						
AC	6	ACER CAMPESTRE	HEDGE MAPLE	3-3.5" CAL	B&B	
AS	13	ACER SACCHARUM	SUGAR MAPLE	3-3.5" CAL	B&B	
CO	9	CELTIS OCCIDENTALIS	COMMON HACKBERRY	3-3.5" CAL	B&B	
NS	4	NYSSA SYLVATICA	BLACK GUM	3-3.5" CAL	B&B	
OB	21	QUERCUS BICOLOR	SWAMP WHITE OAK	3-3.5" CAL	B&B	
QR	23	QUERCUS RUBRA	NORTHERN RED OAK	4-4.5" CAL	B&B	
UA	17	ULMUS 'ACCOLADE'	ACCOLADE ELM	3-3.5" CAL	B&B	
EVERGREEN TREES						
PS	11	PINUS STROBUS	EASTERN WHITE PINE	14-16" HT	B&B	

PLANT SCHEDULE - BASE SCOPE						
KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE	ROOT	COMMENTS
SHRUBS						
FM	17	FOTHERGILLA 'MOUNT AIRY'	DWARF FOTHERGILLA	24" HT	CONT	
HA	51	HAMMELIS 'ARNOLD PROMISE'	ARNOLD PROMISE WITCHHAZEL	5-6" HT	B&B	
RP	160	RHODODENDRON 'P.J.M.'	P.J.M. RHODODENDRON	30-36" HT	B&B	
TM	119	TAXUS MEDIA HICKSII	HICKS YEW	4" HT	B&B	
VD	23	VIBURNUM DENTATUM	ARROWWOOD VIBURNUM	30-36" HT	B&B	
RAIN GARDEN						
CP	11,225	CAREX PENNSYLVANICA	PENNSYLVANIA SEDGE	PLUG	CONT	6" OC
PA	900	POLYSTICHUM ACROSTICHOIDES	CHRISTMAS FERN	PLUG	CONT	6" OC

- PLANTING NOTES:
- LANDSCAPE ARCHITECT SHALL STAKE ALL TREES AND SHRUB LOCATIONS IN THE FIELD WITH THE SITE SUPER TO ADVISE ON UTILITY LOCATIONS. THIS SHALL BE PERFORMED PRIOR TO THE TREES AND SHRUBS ARRIVING ON SITE.
 - LAWN AREAS SHALL BE SODED.
 - NURSERY SOURCES: TREES SHALL BE SOURCED FROM THE FOLLOWING NURSERIES: HALKA NURSERY, MILLSTONE TOWNSHIP PA; HAMMILL NURSERY, HONEY BROOK PA; MOON NURSERY, CHESAPEAKE CITY MD. ANY OTHER SOURCES MUST BE APPROVED BY THE LANDSCAPE ARCHITECT.
 - HERBACEOUS RAIN GARDEN PLANTS SHALL BE LANDSCAPE PLUGS BY NORTH CREEK NURSERIES, LANDBERG, PA 610 225-0100. HERBACEOUS PLUGS ARE MOST EFFICIENTLY INSTALLED WITH A 2" DRILL BIT TO EXCAVATE HOLES.
 - IF THERE IS A DISCREPANCY BETWEEN THE QUANTITY OF PLANTS SCHEDULED AND THE QUANTITY OF PLANTS DRAWN, THEN THE QUANTITY OF PLANTS DRAWN SHALL DETERMINE FINAL QUANTITIES.
 - PROVIDE 8 PERCOLATION TESTS AT TREE PLANTING LOCATIONS.
 - EXISTING TREES TO REMAIN SHALL BE ENCLOSED BY A SIX FT HT CHAIN LINK FENCE AT THE DRIP LINE AND SHALL RECEIVE A 4" THICK MULCH OF AGED WOOD CHIPS. PROVIDE HAZARD PRUNING, CROWN CLEANING AND CROWN REDUCTION AS NEEDED. PROVIDE STRUCTURAL BRACING AS NEEDED.

2 PLANTING SCHEDULE



1 PLANTING PLAN

SCALE: 1"=20'-0"

PLAN

XX NO.	RELEASED FOR SITE PLAN REVIEW/XXX	REVISION	DATE

Richard Burck Associates, Inc.

LANDSCAPE ARCHITECTURE
7 DAVIS SQUARE
SOMERVILLE, MA 02144
617.623.2300 FAX 617.623.2322

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33 Kingston Street Boston, MA 02111
p:617.338.3000 f:617.482.9097

Job Number: 01313.01

Project: BATES COLLEGE

Drawn By: TM, EJ Checked By: TM

Date: 07.02.2014

Scale: AS NOTED

Drawing Title:

PLANTING PLAN

NO.	REVISION	DATE
XX	XXXXXXXXXX	XX/XX/XX

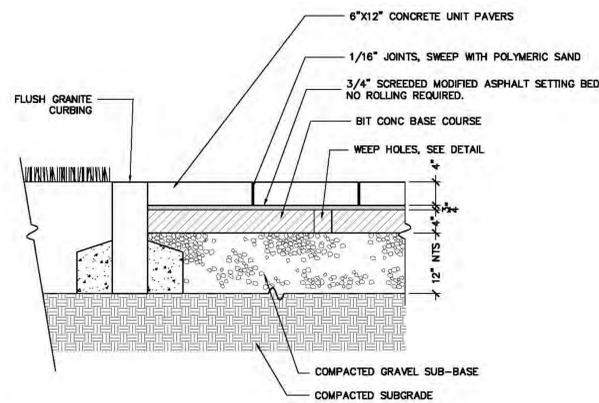
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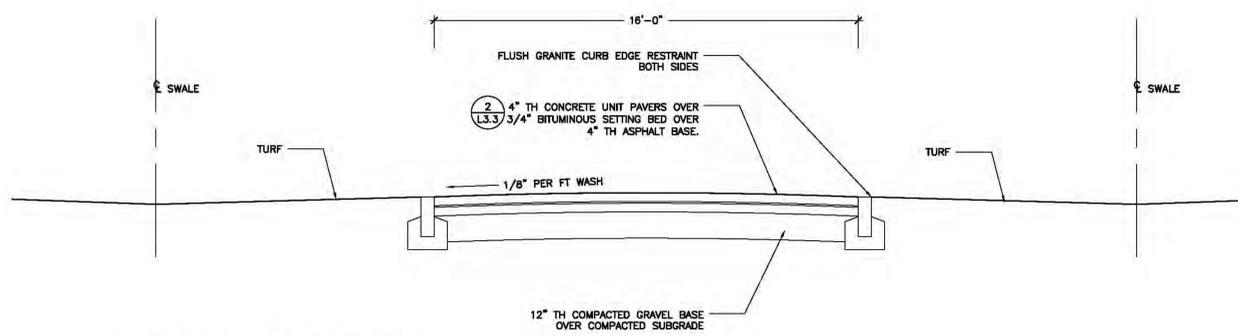
Job Number: 01313.01
Project: BATES COLLEGE
Drawn By: XX Checked By: XX
Date: 07.02.2014
Scale: SCALE

Drawing Title:
SITE DETAILS

L3.3



2 CONCRETE UNIT PAVERS
SCALE: 1" = 1'-0" SECTION



1 SECTION THROUGH FRANKLIN WALK
SCALE: 3/8" = 1'-0" SECTION





CITY OF LEWISTON

Department of Planning & Code Enforcement



TO: Planning Board
FROM: David Hediger, City Planner
DATE: July 24, 2014
RE: June 28, 2014 Planning Board Agenda Item V(a)

Request for recommendation on the acquisition of 114-118 Bartlett Street.

Pursuant to Article VII, Section 4(h) of the Zoning and Land Use Code, the board shall review and make a recommendation to the city council with regard to the acquisition and disposition of all public ways, lands, buildings and other municipal facilities.

The properties at 114 and 118 Bartlett Street are both vacant lots of approximately 5,000 each with 50' frontage located in the Downtown Residential (DR) district. They are owned by LJM, LLC with the mortgage held by Casco Bay Finance Company, LLC. In May 2013, the City Council condemned both structures due to heavy damage by fire, which were subsequently demolished by the City. Each property is currently assessed at \$17,040.

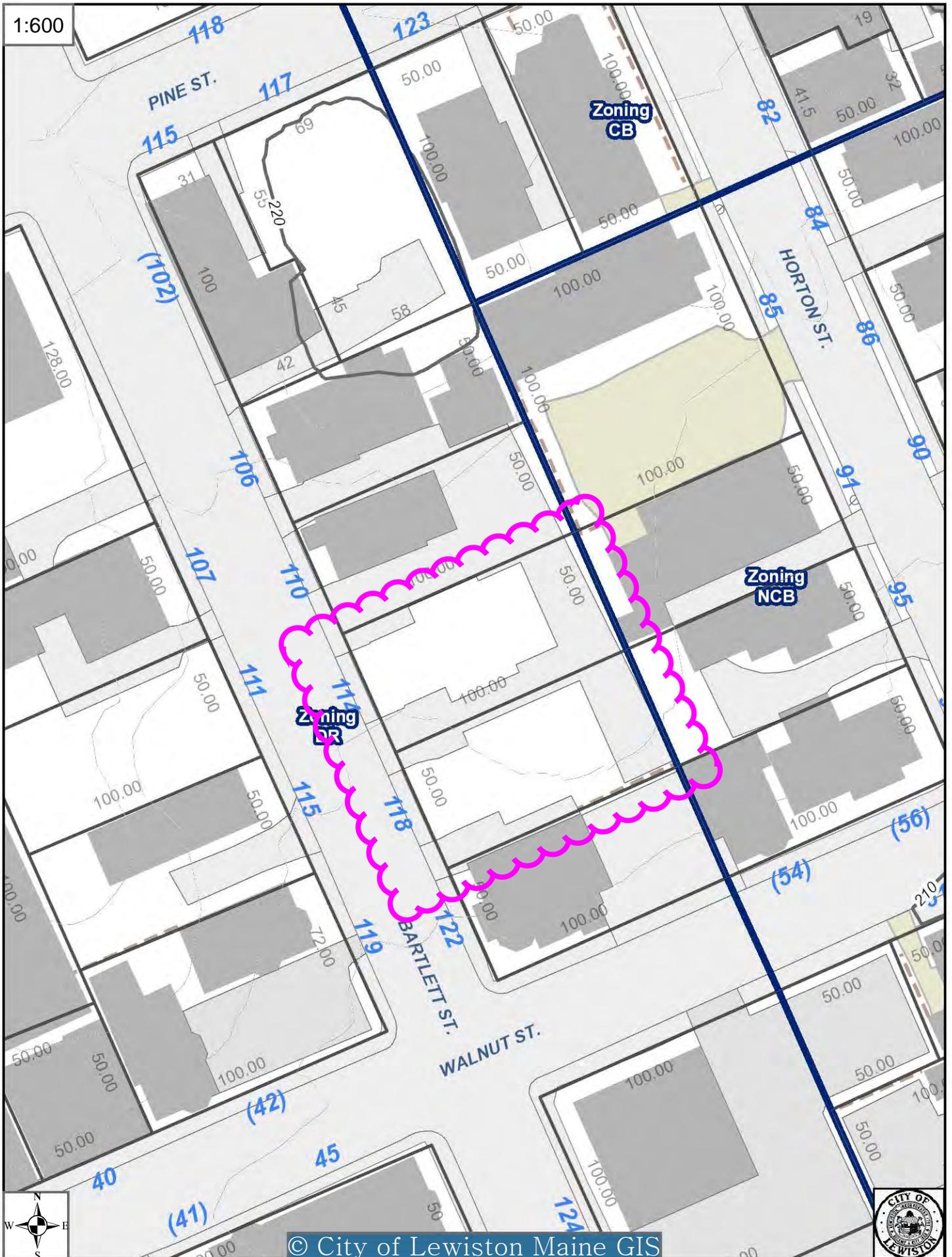
At this time, the city is owed approximately \$28,527.75 for 114 Bartlett Street and \$30,383.75 for 118 Bartlett Street for the structures demolition and attorney's fees. LJM is defunct - without assets. Representatives of LJM have offered to donate the land to the City and the mortgage holder has agreed to discharge the mortgage, eliminating any claim on the property.

Given the situation, the City will eventually acquire these properties by tax foreclosure. Accepting the donation at this time will simply speed the process and allow the City to loan and seed the property (at a cost of \$4,410) to improve its appearance.

ACTIONS NECESSARY:

1. Make a motion pursuant to Article VII, Section 4(h) of the Zoning and Land Use Code to send a favorable recommendation to the City Council for the acquisition of 114 and 118 Bartlett Street.

1:600



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114 and 118 Bartlett Street Acquisition



CITY OF LEWISTON

Department of Planning & Code Enforcement



TO: Planning Board
FROM: David Hediger, City Planner
DATE: July 24, 2014
RE: July 28, 2014 Planning Board Agenda Item V(b)

Item V(b).

Executive Session to discuss real estate negotiations, of which the premature disclosure of the information would prejudice the competitive bargaining position of the City.

A request is being made by Director of Economic and Community Development, Lincoln Jeffers for the Planning Board to go into executive session to discuss the possible acquisition and disposition of city owned land. The following motion must be made:

Make a motion that the Planning Board go into executive session pursuant to 1 M.R.S.A. § 405(6)(C) to discuss real estate negotiations, of which the premature disclosure of the information would prejudice the competitive bargaining position of the City.

The Board will then enter the adjacent meeting room to be briefed on a potential project. No notes, minutes, or votes are taken during executive session.

The Board will reconvene in the Council Chambers continue the meeting after the executive session.