

CITY OF LEWISTON
PLANNING BOARD MEETING
Monday, May 13, 2013 – 5:30 P.M.
City Council Chambers, First Floor
Lewiston City Building
27 Pine Street, Lewiston

AGENDA

I. ROLL CALL

II. ADJUSTMENTS TO THE AGENDA

III. CORRESPONDENCE

IV. PUBLIC HEARINGS:

- a) An application submitted by Stoneybrook Consultants on behalf of K.P. Realty, LLC to construct a Dunkin Donuts Shop at 743 Sabattus Street.
- b) Request by the Lewiston School Department for a capital expenditure not included in Lewiston's annual capital program to make baseline safety improvements for the school entrance of Montello Elementary School.

V. OTHER BUSINESS:

- a) Summary of Form Based Codes: Bates College Intern
- b) Staff update on Comprehensive Plan Process
- c) Any other business Planning Board Members may have relating to the duties of the Lewiston Planning Board.

VIII. READING OF THE MINUTES: Motion to adopt the draft minutes from April 22, 2013.

VII. ADJOURNMENT



CITY OF LEWISTON



Department of Planning & Code Enforcement

TO: Planning Board
FROM: David Hediger, City Planner
DATE: May 10, 2013
RE: May 13, 2013 Planning Board Agenda Item IV(a)

An application submitted by Stoneybrook Consultants on behalf of K.P. Realty, LLC to construct a Dunkin Donuts Shop at 743 Sabattus Street.

Stoneybrook Consultants, Inc. on behalf of K.P. Realty, LLC. has submitted an application for the construction of a Dunkin Donuts at 741-743 Sabattus Street. The site currently consists of one vacant lot and one lot with an existing single family dwelling. Said lots will be combined with the demolition of the dwelling. The lots combined will consist of 38,045 s.f. of area, located in the Highway Business (HB) district in which restaurants with a drive-thru are a permitted use. This Dunkin will be a two-story structure. The first floor will consist of 2,270 s.f. containing all of the restaurants services; the second floor will consist of 1,505 square feet of office space. This project is being considered pursuant to Article XIII, Section 4 of the Zoning and Land Use Code.

Staff has been working closely with the applicant's representative to address concerns and questions. The applicant has since provided revised plans and documentation referencing most of staff comments (see section 5 of the application; May 3, 2013 letter). Staff notes the following with respect to the proposed development:

- The project has been designed to meet all of the space and bulk standards of the HB district. This includes the need for a buffer along the abutting property line of the residential use owned by Mrs. Lacourse on Wildwood Drive. The applicant has met with the neighbor and has agreed upon the installation of a 6' high solid fence and planting of maple trees along the property line.
- Article XII, Section 17(d) of the Zoning and Land Use Code provides the applicant and the Board to waive the codes parking requirements when it can be clearly shown that the proposed use has a differing parking need than otherwise required. Based upon the code's definition and parking demand for a "drive-in restaurant" with office space, the site should provide 38 parking spaces. If this were a "restaurant" with office space and no drive-thru, only 15 parking spaces would be need. The proposed site plan includes 26 parking spaces, plus 16 spaces for vehicle stacking associated with the drive thru. Staff believes this is a reasonable request for the Board's consideration given that the code's definition of drive-in restaurant and corresponding parking standards appear to be intended for the classic drive-in of years past, similar to Val's on Sabattus Street. Of primary concern is adequate stacking of vehicles on site for the drive thru which appears adequate based upon existing establishments in Lewiston/Auburn and previous traffic studies.

- The site has been designed so that a typical Dunkin delivery tractor trailer with a wheel base 50' may access the site without backing out onto Wildwood Drive.
- As with most Dunkin Donut's sites, traffic is the primary concern with the development of the site. The city and the applicant have been in discussions for several months in effort to establish a plan that will be acceptable to the city, the applicant, and Maine DOT. The project is subject to a traffic movement permit (TMP) generating approximately 300 AM peak hour trips. The city has delegated authority from MDOT to review and issue TMPs. At this time, the city, the city traffic engineer at HNTB, applicant's traffic engineer, and MDOT are all in agreement that a traffic light must be installed at the intersections of Wildwood, Sabattus, and Stanley Streets. At a minimum, the operation of the traffic signal improvements will include a temporary traffic signal coordination plan with adjacent signalized intersections at Sabattus Street/Old Green Road and Sabattus Street/Bradford Street, unless otherwise directed by MaineDOT and the City of Lewiston. The intersection signalization improvements will also include pedestrian signal crossings on Sabattus Street at Stanley Street and across both Wildwood Drive and Stanley Street approaches. All improvements will be paid by the applicant. MaineDOT will, through their Traffic Signal Modification Project which will commence this summer, complete communication cable connection and system coordination of the Sabattus Street/Wildwood Drive/Stanley Street intersection to the Sabattus Street Corridor System. Given the necessity of the signal to support this development, the applicant will be required to construct the traffic signal installation providing sequenced traffic signal operation prior to receipt of a certificate of occupancy. While the overall concept of the proposed off-site improvements has been agreed upon by all reviewing departments and agencies, the draft TMP and schematic for offsite improvements have yet to be reviewed in detail. In addition, the city is still waiting for complete submission of the traffic study required as part of the TMP review process. The applicant has indicated this remaining information may be provided sometime the week of May 13th. Ideally, this information would have been completed prior to scheduling for the Boards consideration. Part of the delay has been with the applicant trying to coordinate their improvements with MDOT's planned communication cable connection and system coordination along Sabattus Street. Therefore, staff recommends that no building permits or site activity occur until the TMP, schematic, and traffic study have been reviewed to the satisfaction of the City and MDOT.
- The applicant has provided plans and documentation indicated that existing 22,575 s.f. of impervious area will not be increased upon completion of the project. The site currently drains into an existing catchbasin on site and also sheet flows to a stream at the rear of the property. Staff is pleased to see no increase in impervious area as the site is designed so stormwater will continue to discharge to the rear of the property. However, there is question as to whether stormwater is being directed to an existing sewer line instead of the intended stormwater line. City code prohibits additional stormwater from being added to a sewer line. And if stormwater is directed to an existing stormwater line, the capacity of that existing line must be proven to the city to avoid flooding the system. Similar to the traffic, the applicant and staff are in agreement on the concept of improvements to be made. However, the details related to the connections, and capacity of existing lines and structures have not been provided. . Ideally, this information would have been completed prior to scheduling; however, the uncertainty of whether the existing

drainage structure connects to a sewer line or stormwater line has delayed submission of an additional info. Lewiston Public Works will be providing assistance with respect to status of the existing structure on site in effort to move the project forward. Therefore, staff recommends that no building permits or site activity occur until revised plans and drainage analysis have been provided to the city's satisfaction.

- The area where drainage is directed has been determined to be a stream by DEP. This project will maintain the minimum required 25' buffer from the stream and is awaiting issuance of Permit-By-Rule from DEP for said activity.
- Sheet 3, erosion control note 1 references weekly inspections will be conducted for erosion control purposes. Staff is recommending that upon construction activity commencing, weekly erosion control reports are provided to city on the status of the project and any issues or corrections needed or made with respect to erosion control.
- It is staff's understanding that additional review comments noted in City of Lewiston Project Engineer Ryan Barnes memo dated May 8 2013 and David Hediger's memo dated May 9, 2013 will be addressed by the applicant at the meeting. Staff anticipates with the exception of traffic and stormwater, all other issues will be addressed. However, in the event issues remain, staff shall recommend that no building permits or site activity occur until staff's comments in the above referenced memos have been addressed to the city's satisfaction.

No other concerns have been raised by city staff. Therefore, approval is recommended with the following conditions to be noted on the plan:

1. The applicant will be required to construct the traffic signal installation providing sequenced traffic signal operation prior to receipt of a certificate of occupancy.
2. No building permits or site activity occur until the TMP, schematic, and traffic study have been reviewed to the satisfaction of the City and MDOT.
3. No building permits or site activity occur until revised plans and drainage analysis have been provided to the city's satisfaction.
4. Upon construction activity commencing, weekly erosion control reports are provided to city on the status of the project and any issues or corrections needed or made with respect to erosion control.
5. No building permits or site activity occur until staff's comments in the above referenced memos have been addressed to the city's satisfaction.
6. No building permits or site activity occur until revised plans and drainage analysis have been provided to the city's satisfaction.

ACTIONS NECESSARY

1. Make a motion to consider an application submitted by Stoneybrook Consultants on behalf of K.P. Realty, LLC to construct a Dunkin Donuts Shop at 743 Sabattus Street.
2. Obtain input on the application;
3. Make a determination that the application is complete;
4. Make finding that the application meets all of the necessary criteria contained in the Zoning and Land Use Code, including Article XIII, Section 4 of the Zoning and Land Use Code and to grant approval to K.P. Realty, LLC to construct a Dunkin Donuts Shop at 743 Sabattus Street subject to any concerns raised by the Planning Board or staff.

**City of Lewiston
Traffic Movement Permit**

Applicant: KP Realty, LLC
Project Location: 743 Sabattus Street
Lewiston PID #XXX

Project: DUNKIN' DONUTS RESTAURANT
Identification #: XXX
Permit Category: > 200 PCE
Traffic Engineer: Traffic Solutions
Attn: William Bray, P.E.
235 Bancroft Street
Portland, ME 04102

Pursuant to the provision of 23 M.R.S.A. § 704-A and Chapter 305 of the Maine Department of Transportation's Regulations, with delegated review authority granted to the City of Lewiston, the City has considered the application of KP Realty, LLC with supportive data, staff review, and other related materials on file.

PROJECT DESCRIPTION

The applicant proposes to construct a 2,500 square foot full-service Dunkin' Donuts Restaurant with drive-through window service with a 1,505 square foot office suit on a second floor level at 741 - 743 Sabattus Street. The site will be accessed via a 20' entry and a 15' wide single-lane exit onto Wildwood Drive.

Findings

Based on a review of the files and related information, the City approves the Traffic Movement Permit Application of KP Realty, LLC subject to the following conditions:

MITIGATION

The following mitigation is intended to describe that conceptually shown on *proposed Site Plan*, dated **May XX, 2013** prepared by Summit Geoengineering Services, Inc. and the Sabattus Street/Wildwood Drive/Stanley Street Traffic Signal Improvement Plan dated **May XX, 2013**, prepared by Traffic Solutions and Milone and MacBroom, Inc. Not all of the mitigation discussed herein may be shown on that or any plan.

On-Site Mitigation

A. The Applicant will construct a 3' wide 1" raised concrete island separating the entry and exit lanes to the proposed site.

Off-Site Mitigation

A. The Applicant shall design and install sequenced traffic signals at the Sabattus Street/Wildwood Drive/Stanley Street intersection. The traffic signal design and equipment specifications will be fully compatible and integrated with MaineDOT's PIN: 19007.00 Traffic

Signal Modifications Sabattus Street Project. The Applicant will be required to construct the traffic signal installation providing sequenced traffic signal operation prior to receipt of site occupancy. Operation of the traffic signal improvements will include a temporary traffic signal coordination plan with adjacent signalized intersections at Sabattus Street/Old Green Road and Sabattus Street/Bradford Street, unless otherwise directed by MaineDOT and the City of Lewiston. The intersection signalization improvements will also include pedestrian signal crossings on Sabattus Street at Stanley Street(west approach of Sabattus Street only) and across both Wildwood Drive and Stanley Street approaches. MaineDOT will, through their Traffic Signal Modification Project (PIN 19007.00), complete communication cable connection and system coordination of the Sabattus Street/Wildwood Drive/Stanley Street intersection to the Sabattus Street Corridor System.

Overall

A. Provide all necessary auxiliary signs and pavement markings to implement the improvements described herein according to State of Maine and/or National standards.

B. All plantings and signs (permanent and temporary) shall be placed and maintained such that they do not block available sight distances and do not violate the State's "Installations and Obstructions" law. No signage or plantings shall be allowed within the "clear zone" if they constitute a deadly fixed object.

If any of the supporting data or representations for which this permit is based changes in any way or is found to be incorrect/inaccurate, the applicant shall request in writing from the City a decision of what impacts those changes will have on the permit. The applicant will then be required to submit those changes for review and approval and additional mitigation as a result of those changes may be required at the expense of the applicant.

By:

David Hediger
City Planner
City of Lewiston

Date: _____



CITY OF LEWISTON

Department of Planning & Code Enforcement



TO: Stoneybrook Consultants, LLC
FROM: David Hediger, City Planner
DATE: May 1, 2013
RE: Sabattus Street Dunkin Donuts

Planning and Code Enforcement has the following comments at this time upon reviewing the application:

1. A retaining wall is proposed on site. Please confirm height. Must be designed by a PE if it exceeds 6' in height. Also confirm construction type (i.e. precast, pour concrete, etc.)
2. Proposed sign location will require a waiver from DOT given the width and number of travel lanes on Sabattus Street. No sign permits will be issued for its proposed location until a waiver is received from DOT.
3. Please clarify what the square footage of the proposed office space on the second floor shall be.
4. Article XII, Section 17 (d) references the parking requirements for a restaurant, drive-in, and office. Please reference these provisions and how they are applied to this project.
5. The eight parking spaces adjacent the building will be difficult to access during peak hours due to being blocked by drive-thru traffic. Please speak to stacking of vehicles on site and the availability of parking.
6. Where appropriate, consideration should be given to reducing the amount of impervious area by narrowing parking spaces and aisle widths to the minimum requirements of Article XII, Section 17.
7. Article XIII, Section 4(b) requires that the planning board shall determine that any traffic increase attributable to the proposed development will not result in unreasonable congestions or unsafe conditions. In making this determination, the board shall consider the analysis and recommendation provided by the applicant, DOT and the City. While there has been a great deal of discussion regarding the traffic improvement needed to support this project, additional information is need, including but not limited to, a site plan showing all proposed on and off site improvements including signals, signs, striping, pedestrian crossings, etc.; a summary traffic analysis provided by the projects traffic engineer; a draft traffic movement permit. Reference should also be made the DOT's proposed improvements to the Sabattus Street corridor and how this project shall tie into said improvements. All of this information must be reviewed to the satisfaction of the City and MDOT prior to any permits being issued for construction activity. In addition, upon receiving approval from the Planning Board, staff is recommending as a condition of approval that all offsite improvements must be completed in accordance with the approved TMP and said plans prior to a certificate of occupancy being issued for construction activity. Staff is also recommending that the applicant traffic

engineer provided a stamped statement verifying that all on and off site traffic related improvements have been completed in accordance with the approved plans and TMP. If the applicant believes the site will be completed prior to DOT's corridor improvements, then a condition of approval should be drafted for staff and the Board's consideration as to how the site may operate with traffic improvements remaining to be completed.

8. The application notes a Permit By Rule from DEP will be required for proposed soil disturbance within 75' of a stream. No building permits or site activity may occur until this activity is approved by DEP.
9. Given the sites proximity to a natural resource, the amount of fill proposed for the site, and grading of the site toward the natural resource, staff is recommending that upon construction activity commencing, weekly erosion control reports are provided to city on the status of the project and any issues or corrections needed or made with respect to erosion control.
10. The applicant must provide evidence to demonstrate financial capacity to complete the project.
11. Staff asks that the sidewalk accessing the site from Sabattus Street be possibly relocated to align with proposed crosswalks on Sabattus Street.
12. You have indicated that the amount of impervious area on site shall remain the same. However, based upon the existing and proposed grading, it appears more of the site shall drain into an existing catch basin and city stormwater line on site. This may be allowed upon Public Works approval. Specifically, Article XIII, Section 4(f)(1)(b) allows for a connection to a city storm water system if the applicant has adequately demonstrated that it has the capacity to accommodate increases in flow. A slight increase may be allowed if it is determined that the increase cannot be avoided and does not significantly impact abutting properties or city property. More information is necessary for the city to make a determination on the proposed stormwater improvements.
13. There appears to be a grading typo behind the dumpster with the 258' elevation.
14. Detail on the dumpster screen should be provided.
15. Sheet 1 legend must be revised with respect to proposed landscaping. Please confirm plantings must be 2.5" caliper upon planting.
16. Details need on proposed lighting. Lighting must be cut off fixtures. Pole lighting height should not exceed 20'.

Additional comments may be provided upon further review.

Thank you.



CITY OF LEWISTON

Department of Planning & Code Enforcement



TO: Stoneybrook Consultants, LLC
FROM: David Hediger, City Planner
DATE: May 9, 2013
RE: Sabattus Street Dunkin Donuts

Planning and Code Enforcement has the following comments at this time upon reviewing your responses dated May 3rd and received by this office May 6th.

The majority of the responses you have provided have been addressed to staffs satisfaction, with the following exceptions:

1. Sheet 1, note 6 and Sheet 2, note 5: these notes do not seem necessary at this point in the review.
2. Sheet 3, erosion control note 1: the note references weekly inspections will be conducted for erosion control purposes. Staff is recommending that upon construction activity commencing, weekly erosion control reports are provided to city on the status of the project and any issues or corrections needed or made with respect to erosion control.
3. You have noted in your May response that lighting will be cut off fixtures and pole lighting height will not exceed 20'. Since plan details are not being provided, this should be noted on Sheet 1 within the legend provided.
4. If the proposed sign location is to remain on the plan, a note should be added indicating that no sign permits will be issued for its proposed location until a waiver is received from DOT.
5. As of today, given the limited information provided at this time, additional traffic review is required by the City, the City's peer reviewer for traffic HNTB, and the Maine Department of Transportation. At a minimum, staff will be recommending to the Planning Board that no construction activity occur on site and no permits be issued until a draft traffic movement permit, schematic design for offsite improvements, and completed traffic study report has been reviewed to the satisfaction of staff.
6. Based upon the remaining concerns related to stormwater raised by Public Works, staff will be recommending to the Planning Board that no construction activity occur on site and no permits be issued until stormwater and drainage related issues have been reviewed to the satisfaction of staff.

Additional comments may be provided upon further review.

Thank you.



Department of Public Works
David A. Jones, P.E, Director

DATE: April 30, 2013

TO: David Hediger, Planning Director

FROM: Ryan Barnes, P.E., Project Engineer *RJB*

SUBJECT: 731-733 Sabattus Street

Lewiston Public Works has the following comments at this time upon reviewing the application:

At the time of this review the Signal Plan and drainage calculations had not been submitted and comments on these are not included in this review.

Application

1. The application letter states that the percentage of impervious areas are remaining the same and no stormwater treatment is required. The proposed work changes the layout of the site redirecting water to the existing stormwater pond located on the adjacent parcel, therefore stormwater calculations will be required to show that the additional flow to the stormwater pond will not cause adverse impacts to the downstream properties.
2. A cost estimate for the onsite and offsite work was not included with this application. Both cost estimates and Financial Capacity will need to be submitted.
3. A Traffic impact study and Signal plan were not included in the application and will need to be submitted for review. As previously stated at meets between the applicant and staff he signal plan shall include, video detection for all approaches, preemption, APS Equipment, connection to the MaineDOT Coordinated system, and rehabbing of all impacted pedestrian ramps. Additional comments regarding the offsite improvements and signal plan will be provided after a plan has been submitted for review.

Plans

1. The applicant should consider relocating the walkway from Sabattus Street to the site to be on the opposite side of the order board so that the walkway can be aligned

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with the crosswalk crossing Sabattus Street.

2. The applicant should consider reducing the parking spaces to be the minimum City dimensions to further reduce impervious area and run off from the site. The applicant may be able to add additional parking to meet the parking demand by reducing the stall width.
3. The site grading will add approximately 6,500 square feet of area to the stormwater pond on the adjacent parcel, the applicant shall provide calculations that show that this will not cause adverse impacts to the downstream properties.
4. The proposed drainage plan will provide a new manhole to be constructed on the existing 18" stormdrain, the applicant shall provide calculations showing that the 18" stormdrain can handle this increase in flow.
5. Level lip spreaders or other means of dissipation should be added to the end of the rip rap downspouts to prevent erosion adjacent to the stream due to the concentration of flows and increase in slopes as a result of this project.
6. Please provide grading information for the proposed wall and the curb along the top of the wall.
7. The plans include a detail for a Precast Concrete Catchbasin, however, none are proposed, if a catchbasin is installed a catchbasin hood such as a Snout will be required instead of the 90° elbow.
8. A detail for a manhole should be added to the detail sheet. The applicant should consider utilizing a doghouse structure and maintaining the bottom of the existing pipe as the channel and construct the shelf around the pipe instead of trying to make connections using short sections of pipe that could leak over time.
9. A detail for the retaining wall including type of wall and reinforcement if necessary should be added to the detail sheet.
10. A detail for a rip-rap downspout including non-woven geotextile and stone sizing should be added to the detail sheet.
11. Erosion & Sedimentation Control Note #1 makes reference to a Stormwater Permit, the application states that a Stormwater Permit is not required, the note should be revised to indicate the appropriate measures necessary since a permit will not be provided to the contractor.

Additional comments may be provided upon further review.

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Department of Public Works

David A. Jones, P.E., Director

DATE: May 8, 2013

TO: David Hediger, Planning Director

FROM: Ryan Barnes, P.E., Project Engineer 

SUBJECT: 731-733 Sabattus Street - #2

Lewiston Public Works has the following comments at this time upon reviewing the application:

At the time of this review the Signal Plan and Traffic Impact Study had not been submitted and therefore comments on these items are not included.

Application

1. A Traffic impact study and Signal plan were not included in the application and will need to be submitted for review. As previously stated the signal plan shall include, video detection for all approaches, preemption, APS Equipment, connection to the MaineDOT Coordinated system, and rehabbing of all impacted pedestrian ramps. Additional comments regarding the offsite improvements and signal plan will be provided after a plan has been submitted for review.

Plans

1. The proposed drainage plan will provide a new manhole to be constructed on the existing 18" stormdrain, the applicant shall provide calculations showing that the 18" stormdrain can handle this increase in flow from the added area and the roof.

Based on our site observations it appears that the existing catchbasin on stie connects into the sewer system which per section 74-55 (C) (1) and (2) of the City Ordinance will need to be disconnected as part of this project, and since the catchbasin is not currently connected into the 18" stormdrain this flow will need to be include in the sizing for the 18" stormdrain from the manhole to the steam.

The applicant may install a separate outlet pipe instead of connecting to the City System; however, they will be responsible for obtaining all necessary permits for the installation.

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2. The detail for the manhole shall be revised to include a brick channel.
3. The detail for the Rip Rap Swale indicates a 12" thick layer of rip rap, typically rip rap is placed at 2.25 times the d50 stone therefore this should be a 15" layer.
4. The detail for the Rip Rap Swale indicates the use of Mirafi 700x geotextile, this is a woven geotextile, typically non-woven geotextiles are used under rip rap.
5. Due to the nature of the establishment a permit will be required from the Lewiston Auburn Water Pollution Control Authority (LAWPCA) regarding the handling of grease.

Additional comments may be provided upon further review and submissions.



Stoneybrook Consultants, Inc.

456 Buckfield Road
Turner, Maine 04282
(207) 514-7491 voice
(207) 514-7492 fax

Development Review Application

741-743 Sabattus Street

prepared for
K.P. Realty, LLC

May 2013



Stoneybrook Consultants, Inc.

456 Buckfield Road
Turner, Maine 04282
(207) 514-7491 voice
(207) 514-7492 fax

April 22, 2013

David Hediger, City Planner
Department of Planning & Code Enforcement
City of Lewiston
27 Pine Street
Lewiston, ME 04240-7201

Re: Dunkin' Donuts Shop
741 - 743 Sabattus Street

Dear David:

On behalf of K.P. Realty, LLC, we are pleased to submit this application for Site Plan Review to construct a Dunkin' Donuts shop on two lots located at 741 - 743 Sabattus Street. The property is currently occupied by a single-family home but, previously, also supported a large commercial structure with associated parking areas. Those improvements can be seen on the City GIS system for lots identified as Tax Map 119, Lots 286 & 287. Existing improvements are shown on the Existing Conditions Plan included in Section 4 of this application and the Tax Map information is provided in Section 2. Both parcels are located within the Highway Business (HB) zoning district.

The parcels contain 38,045 square feet in total. There is about 177' of road frontage on Sabattus Street and about 201' of frontage on Wildwood Drive. There is a City or State storm drain line crossing the parcel with a recorded easement. There is also a City sewer main crossing the parcel that does not appear to be within a recorded easement. We are currently working with City staff to document an easement location for the sewer main.

As with most Dunkin' Donut projects, this project will generate about 300 AM peak hour trips and, therefore, will require a Traffic Movement Permit from the Maine Department of Transportation which is processed under the City's delegated review authority. We have previously submitted that Traffic application and hope to finalize that permit process with the City in the very near future.

Proposed improvements are shown on plans prepared by Summit Geoengineering Services, Inc. (Summit) which are included in Section 4 of this application. Plans are to construct a two-story building with a footprint of about 2,270 square feet. The building will be a full service Dunkin' with a drive-up window and seating for 28 customers. The second floor will have several separate office areas, storage area, a conference room and a bathroom for the owner and his management staff. With this location, the owner will have three Dunkin' shops which will employ 55 to 62 employees. This second floor area will give the owner space to manage all three locations, store employee records and meet with management staff.

The property currently has two curb cuts on Sabattus Street and one curb cut on Wildwood Drive. The plans call for elimination of all three of these existing driveways and the construction of one new drive on Wildwood Drive located about 160' south of the Sabattus Street intersection. Please note that the Summit plans show the approximate location of the Cumberland Farms store improvements and that the centerlines for the Wildwood Drive access for these two projects are expected to be separated by about 85'.

The Dunkin' site has been designed with a one-way traffic flow. Parking for 25 vehicles which includes two handicap spaces are shown. The drive-thru lane, which will allow for by-pass traffic flow, will be long enough to provide stacking for at least 16 vehicles. The site plans also show proposed sidewalks for pedestrian movements leading to the building from parking areas and from the sidewalk along Sabattus Street. Curbing is provided around the building and a concrete island (raised 1") will be installed to better direct traffic movements at the project entrance from Wildwood Drive. This concrete island

and the site drives have been designed to allow truck (WB-50) movements through the site for product deliveries.

Based upon the City GIS system and our survey information, there is about 22,575 square feet of impervious surface on the parcel. The proposed project has been designed to maintain an equal amount of impervious area. Therefore, stormwater permits are not required and stormwater treatment is not proposed. Total impervious coverage is 0.59. City code allows a maximum impervious coverage of 0.75. This project will disturb 38,045 square feet of area to construct the improvements shown. Total lot coverage within the parcel for the building improvements is 0.07. City code allows 0.50 lot coverage in the HB District. The proposed layout will meet all City setbacks and yard requirements.

We have shown connections to the City water and sewer systems on the project plans. Sewer will connect at an existing manhole onsite and water will be connected to an existing water main located in Wildwood Drive. Power will be extended underground to the building from the existing pole on Wildwood Drive and gas will be connected from the existing main also located in Wildwood Drive.

As noted above, this project has been designed to allow truck movements for a WB-50 truck to drive through the site. The driveway is wider behind the employee parking spaces to provide an area for the delivery truck to park while still allowing vehicles to exit the site. We have also shown a new enclosed dumpster pad at the rear of the building. We have provided information for the proposed location of street trees and tree buffering along the Lacourse property line. Ms. Lacourse prefers a new fence and maple trees along the driveway. We will also improve the existing slope between the properties before we install these improvements. The applicant plans to provide additional landscape improvements with shrub and landscape beds within the green space areas, but has not finalized those plans at this time.

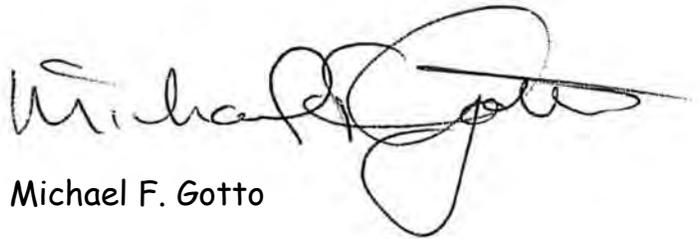
The existing City storm drain system outlets to a stream located at the southerly end of this site. The stream is regulated under the Natural Resource

Protection Act (NRPA) by the Maine Department of Environmental Protection (MDEP). Since this project proposes to disturb areas within 75' of the stream during construction, but will maintain a 25' setback from the stream, this project will qualify under the NRPA permit by rule procedures. We are preparing the required application forms for submittal to MDEP and would expect to have approvals within two to three weeks.

This Dunkin' will operate 24 hours per day 7 days per week. The total estimated cost for the new building and site improvements is about \$550,000 with construction scheduled to begin as soon as this project is approved. Occupancy of the building is planned by Fall of this year. Final landscaping may not be completed until Spring of next year. We hope you find this application complete and we will plan to attend the Planning Board meeting to answer any questions the Planning Board or staff may have.

Respectfully Yours

STONEBROOK CONSULTANTS, INC.

A handwritten signature in black ink, appearing to read "Michael F. Gotto", with a long horizontal stroke extending to the right.

Michael F. Gotto

cc: Kevin Pacheco

**KP Realty, LLC
1385 Maine Street #1
Poland, ME 04274**

To Whom It May Concern:

The signature below authorizes Stoneybrook Consultants, Inc. and William Bray, P.E. to act as the applicant's agents in the processing of the enclosed application.



for KP Realty, LLC, applicant

Kevin Pacheco, president
(print name, title)

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Section 8	Financial Capacity
Section 9	Stormwater

Section 1

Development Review Application

PROJECT DATA

The following information is required where applicable, in order to complete the application

IMPERVIOUS SURFACE AREA/RATIO

Existing Total Impervious Area	_____ 22,575 sq. ft.
Proposed Total Paved Area	_____ 20,324 sq. ft.
Proposed Total Impervious Area	_____ 22,572 sq. ft.
Proposed Impervious Net Change	_____ 0 sq. ft.
Impervious surface ratio existing	_____ 59 % of lot area
Impervious surface ratio proposed	_____ 59 % of lot area

BUILDING AREA/LOT

COVERAGE

Existing Building Footprint	_____ 1,240 sq. ft.
Proposed Building Footprint	_____ 2,270 sq. ft.
Proposed Building Footprint Net change	_____ 1,030 sq. ft.
Existing Total Building Floor Area	_____ 1,240 sq. ft.
Proposed Total Building Floor Area	_____ 3,775 sq. ft.
Proposed Building Floor Area Net Change	_____ 2,535 sq. ft.
New Building	_____ yes (yes or no)
Building Area/Lot coverage existing	_____ 3 % of lot area
Building Area/Lot coverage proposed	_____ 7 % of lot area

ZONING

Existing	_____ Highway Business _____
Proposed, if applicable	_____ _____

LAND USE

Existing	_____ residential _____
Proposed	_____ commercial _____

RESIDENTIAL, IF APPLICABLE

Existing Number of Residential Units	_____ 1 _____
Proposed Number of Residential Units	_____ 0 _____
Subdivision, Proposed Number of Lots	_____ _____

PARKING SPACES

Existing Number of Parking Spaces	_____ 0 _____
Proposed Number of Parking Spaces	_____ 25 _____
Required Number of Parking Spaces	_____ _____
Number of Handicapped Parking Spaces	_____ 2 _____

ESTIMATED COST OF PROJECT

_____ **\$550,000** _____

DELEGATED REVIEW AUTHORITY CHECKLIST

SITE LOCATION OF DEVELOPMENT AND STORMWATER MANAGEMENT

Existing Impervious Area	_____ 22,575 sq. ft.
Proposed Disturbed Area	_____ 38,045 sq. ft.
Proposed Impervious Area	_____ 22,575 sq. ft.

1. *If the proposed disturbance is greater than one acre, then the applicant shall apply for a Maine Construction General Permit (MCGP) with MDEP.*
2. *If the proposed impervious area is greater than one acre including any impervious area created since 11/16/05, then the applicant shall apply for a MDEP Stormwater Management Permit, Chapter 500, with the City.*
3. *If total impervious area (including structures, pavement, etc) is greater than 3 acres since 1971 but less than 7 acres, then the applicant shall apply for a Site Location of Development Permit with the City. If more than 7 acres then the application shall be made to MDEP unless determined otherwise.*
4. *If the development is a subdivision of more than 20 acres but less than 100 acres then the applicant shall apply for a Site Location of Development Permit with the City. If more than 100 acres then the application shall be made to MDEP unless determined otherwise.*

TRAFFIC ESTIMATE

Total traffic estimated in the peak hour-existing (Since July 1, 1997)	_____ N/A _____ passenger car equivalents (PCE)
--	--

Total traffic estimated in the peak hour-proposed (Since July 1, 1997)	_____ 300 _____ passenger car equivalents (PCE)
--	--

If the proposed increase in traffic exceeds 100 one-way trips in the peak hour then a traffic movement permit will be required.

Zoning Summary

1. Property is located in the Highway Business zoning district.
2. Parcel Area: _____ acres / 38,045 square feet(sf).

Regulations	Required/Allowed	Provided
Min Lot Area	<u>20,000</u>	<u>38,045</u>
Street Frontage	<u>150'</u>	<u>378'</u>
Min Front Yard	<u>15'</u>	<u>15'</u>
Min Rear Yard	<u>N/A</u>	<u>N/A</u>
Min Side Yard	<u>10'</u>	<u>10'/13'</u>
Max. Building Height	<u>65'</u>	<u>30'</u>
Use Designation	<u>drive-thru</u>	<u>drive-thru</u>
Parking Requirement	1 space/ per _____ square feet of floor area	
Total Parking:	/	<u>25</u>
Overlay zoning districts (if any):	/ /	
Urban impaired stream watershed?	YES <input checked="" type="radio"/> NO <input type="radio"/> If yes, watershed name _____	

DEVELOPMENT REVIEW APPLICATION SUBMISSION

Submission shall include payment of fee and fifteen (15) complete packets containing the following materials:

1. Full size plans containing the information found in the attached sample plan checklist.
2. Application form that is completed and signed.
3. Cover letter stating the nature of the project.
4. All written submittals including evidence of right, title and interest.
5. Copy of the checklist completed for the proposal listing the material contained in the submitted application.

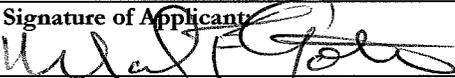
Refer to the application checklist for a detailed list of submittal requirements.

L/A's development review process and requirements have been made similar for convenience and to encourage development. Each City's ordinances are available online at their prospective websites:

Auburn: www.auburnmaine.org under City Departments/ Planning and Permitting/Land Use Division/**Zoning Ordinance**
Lewiston: <http://www.ci.lewiston.me.us/clerk/ordinances.htm> Refer to Appendix A of the Code of Ordinances

I hereby certify that I am the Owner of record of the named property, or that the owner of record authorizes the proposed work and that I have been authorized by the owner to make this application as his/her authorized agent. I agree to conform to all applicable laws of this jurisdiction. In addition, I certify that the City's authorized representative shall have the authority to enter all areas covered by this permit at any reasonable hour to enforce the provisions of the codes applicable to this permit.

This application is for development review only; a Performance Guarantee, Inspection Fee, Building Permit Application and other associated fees and permits will be required prior to construction.

Signature of Applicant 	Date: April 22, 2013
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AS AGENT

Development Review Checklist
 City of Auburn Planning and Permitting Department
 City of Lewiston Department of Planning and Code
 Enforcement



**THE FOLLOWING INFORMATION IS REQUIRED WHERE APPLICABLE TO BE
 SUBMITTED FOR AN APPLICATION TO BE COMPLETE**

PROJECT NAME: Dunkin' Donuts Shop

PROPOSED DEVELOPMENT ADDRESS and PARCEL #: 741-743 Sabattus Street, 286&2t

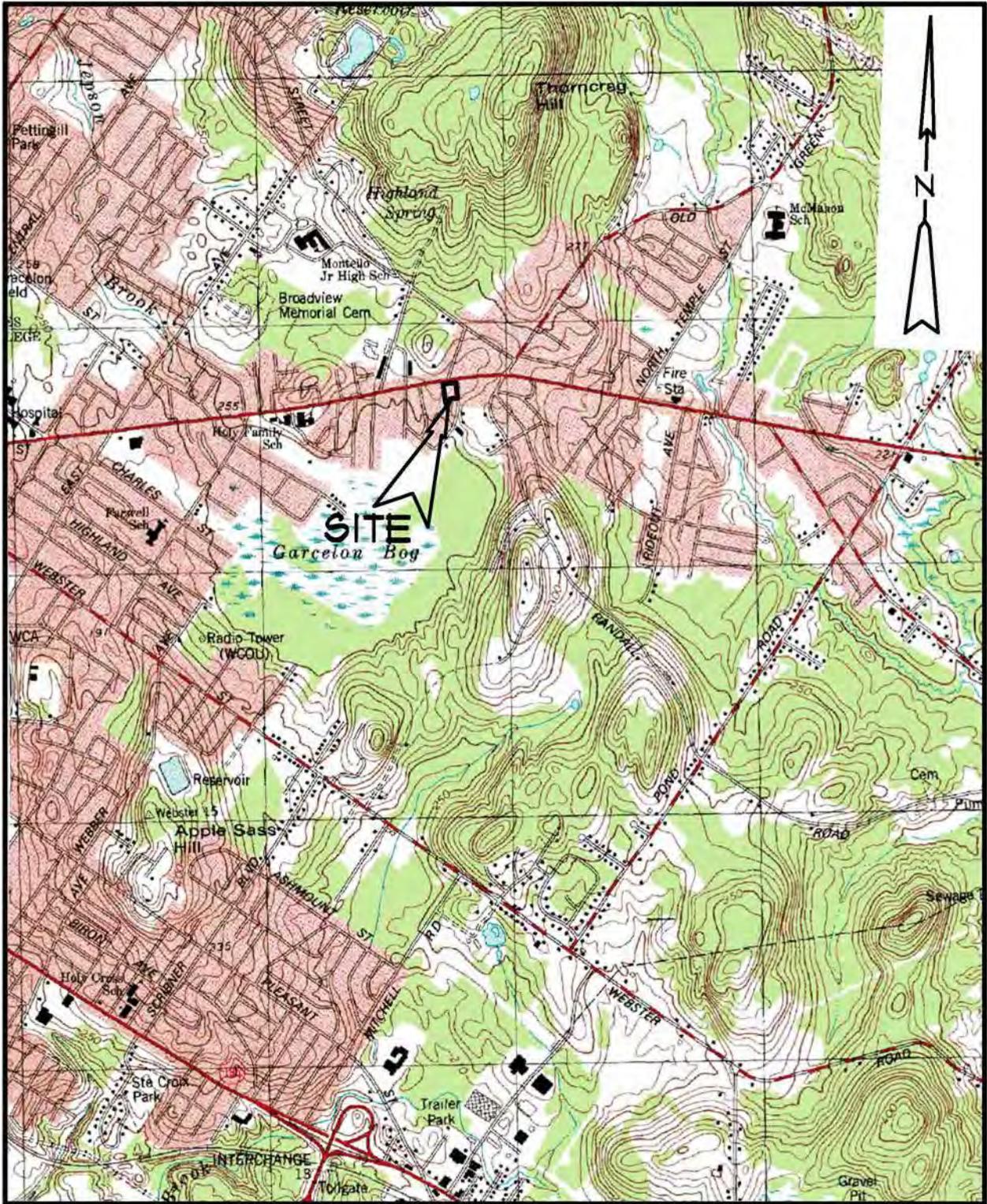
Required Information		Check Submitted		Applicable Ordinance	
		Applicant	Staff	Lewiston	Auburn
Site Plan					
	Owner's Names/Address	✓			
	Names of Development	✓			
	Professionally Prepared Plan	✓			
	Tax Map or Street/Parcel Number	✓			
	Zoning of Property	✓			
	Distance to Property Lines	✓			
	Boundaries of Abutting land	✓			
	Show Setbacks, Yards and Buffers	✓			
	Airport Area of Influence (Auburn only)				
	Parking Space Calcs				
	Drive Openings/Locations	✓			
	Subdivision Restrictions				
	Proposed Use	✓			
	PB/BOA/Other Restrictions				
	Fire Department Review				
	Open Space/Lot Coverage	✓			
	Lot Layout (Lewiston only)	✓			
	Existing Building (s)	✓			
	Existing Streets, etc.	✓			
	Existing Driveways, etc.	✓			
	Proposed Building(s)	✓			
	Proposed Driveways	✓			
Landscape Plan					
	Greenspace Requirements				
	Setbacks to Parking	✓			
	Buffer Requirements	✓			
	Street Tree Requirements	✓			
	Screened Dumpsters	✓			
	Additional Design Guidelines				

	Planting Schedule				
Stormwater & Erosion Control Plan					
	Compliance w/ chapter 500	N/A			
	Show Existing Surface Drainage	✓			
	Direction of Flow				
	Location of Catch Basins, etc.	✓			
	Drainage Calculations				
	Erosion Control Measures	✓			
	Maine Construction General Permit				
	Bonding and Inspection Fees				
	Post-Construction Stormwater Plan				
	Inspection/monitoring requirements				
	Third Party Inspections (Lewiston only)				
Lighting Plan					
	Full cut-off fixtures	✓			
	Meets Parking Lot Requirements	✓			
Traffic Information					
	Access Management	✓			
	Signage	✓			
	PCE - Trips in Peak Hour	✓			
	Vehicular Movements	✓			
	Safety Concerns	✓			
	Pedestrian Circulation	✓			
	Police Traffic	✓			
	Engineering Traffic	✓			
Utility Plan					
	Water	✓			
	Adequacy of Water Supply				
	Water main extension agreement				
	Sewer	✓			
	Available city capacity				
	Electric	✓			
	Natural Gas	✓			
	Cable/Phone	✓			
Natural Resources					
	Shoreland Zone				
	Flood Plain	✓			
	Wetlands or Streams	✓			
	Urban Impaired Stream				
	Phosphorus Check				
	Aquifer/Groundwater Protection				
	Applicable State Permits				
	No Name Pond Watershed (Lewiston only)				

	Lake Auburn Watershed (Auburn only)				
	Taylor Pond Watershed (Auburn only)				
Right Title or Interest					
	Verify	✓			
	Document Existing Easements, Covenants, etc.				
Technical & Financial Capacity					
	Cost Est./Financial Capacity	✓			
	Performance Guarantee				
State Subdivision Law					
	Verify/Check				
	Covenants/Deed Restrictions				
	Offers of Conveyance to City				
	Association Documents				
	Location of Proposed Streets & Sidewalks				
	Proposed Lot Lines, etc.				
	Data to Determine Lots, etc.				
	Subdivision Lots/Blocks				
	Specified Dedication of Land				
Additional Subdivision Standards					
	Single-Family Cluster (Lewiston only)				
	Multi-Unit Residential Development (Lewiston only)				
	Mobile Home Parks				
	Private Commercial or Industrial Subdivisions (Lewiston only)				
	PUD (Auburn only)				
A jpeg or pdf of the proposed site plan					
Final sets of the approved plans shall be submitted digitally to the City, on a CD or DVD, in AutoCAD format R 14 or greater, along with PDF images of the plans for archiving					

Section 2

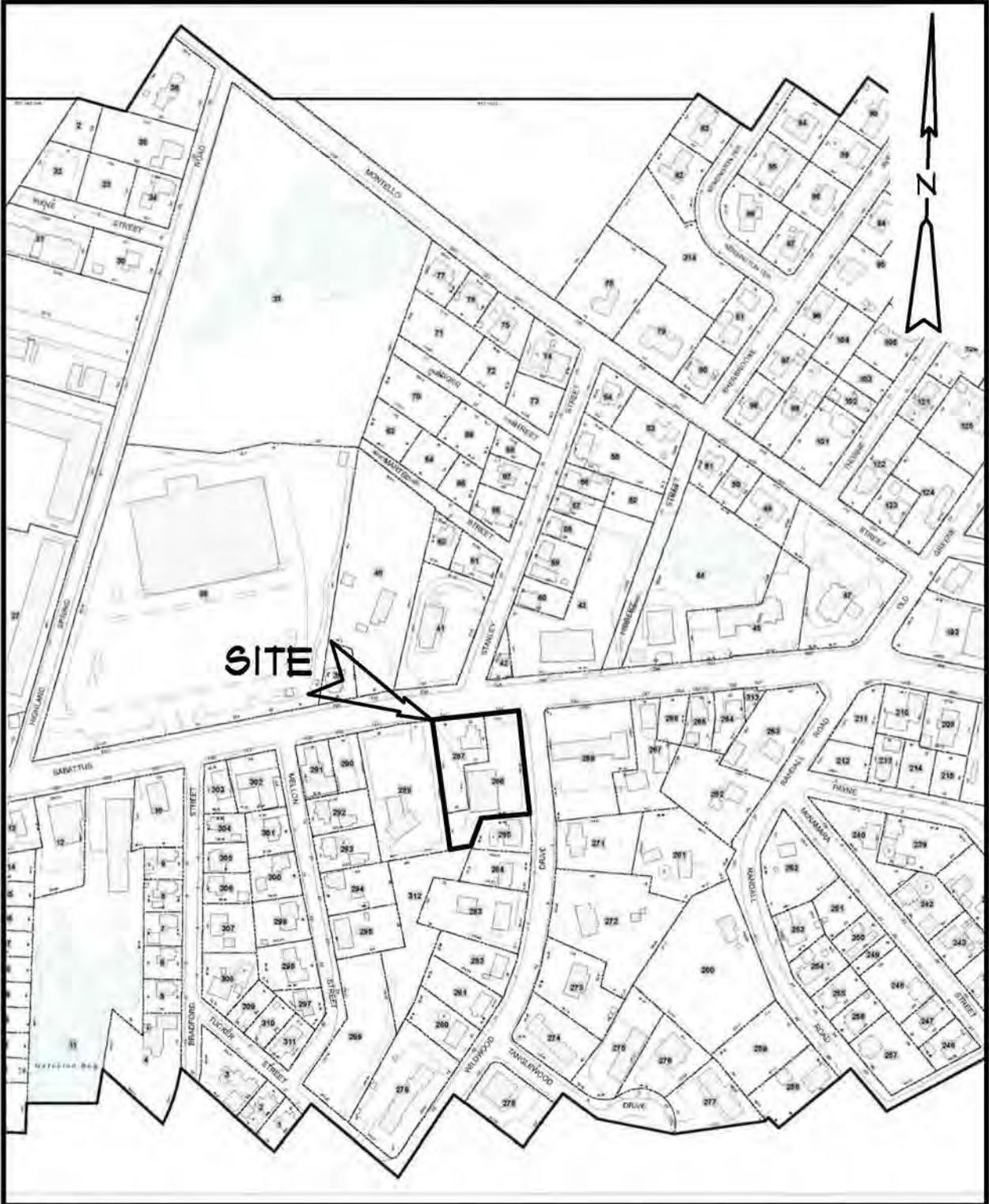
Project Maps



USGS LOCATION MAP

741-743 SABATTUS STREET, LEWISTON
 OWNER: KP REALTY, LLC
 SCALE: 1" = 2,000'
 DATE OF GRAPHIC: OCTOBER 11, 2012
 SOURCE: MAINE OFFICE OF GIS
 ORIGINAL PUBLICATION DATE: 1967

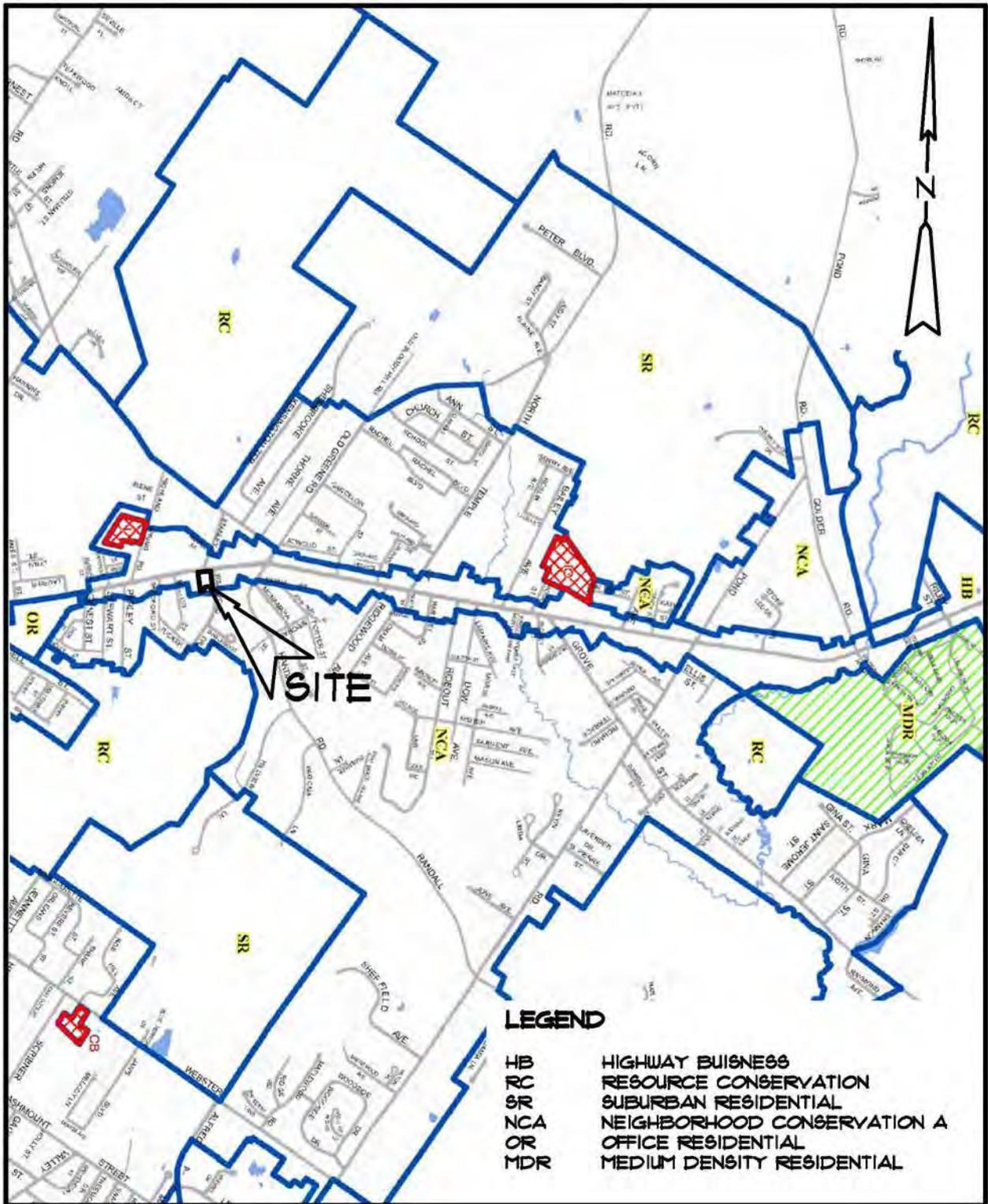
Stoneybrook
 Consultants, Inc.



TAX MAP

741-743 SABATTUS STREET, LEWISTON
 OWNER: KP REALTY, LLC
 SCALE: 1" = 300'±
 DATE OF GRAPHIC: OCTOBER 11, 2012
 SOURCE: CITY OF LEWISTON TAX MAP 153
 PUBLICATION DATE: APRIL 1, 2011

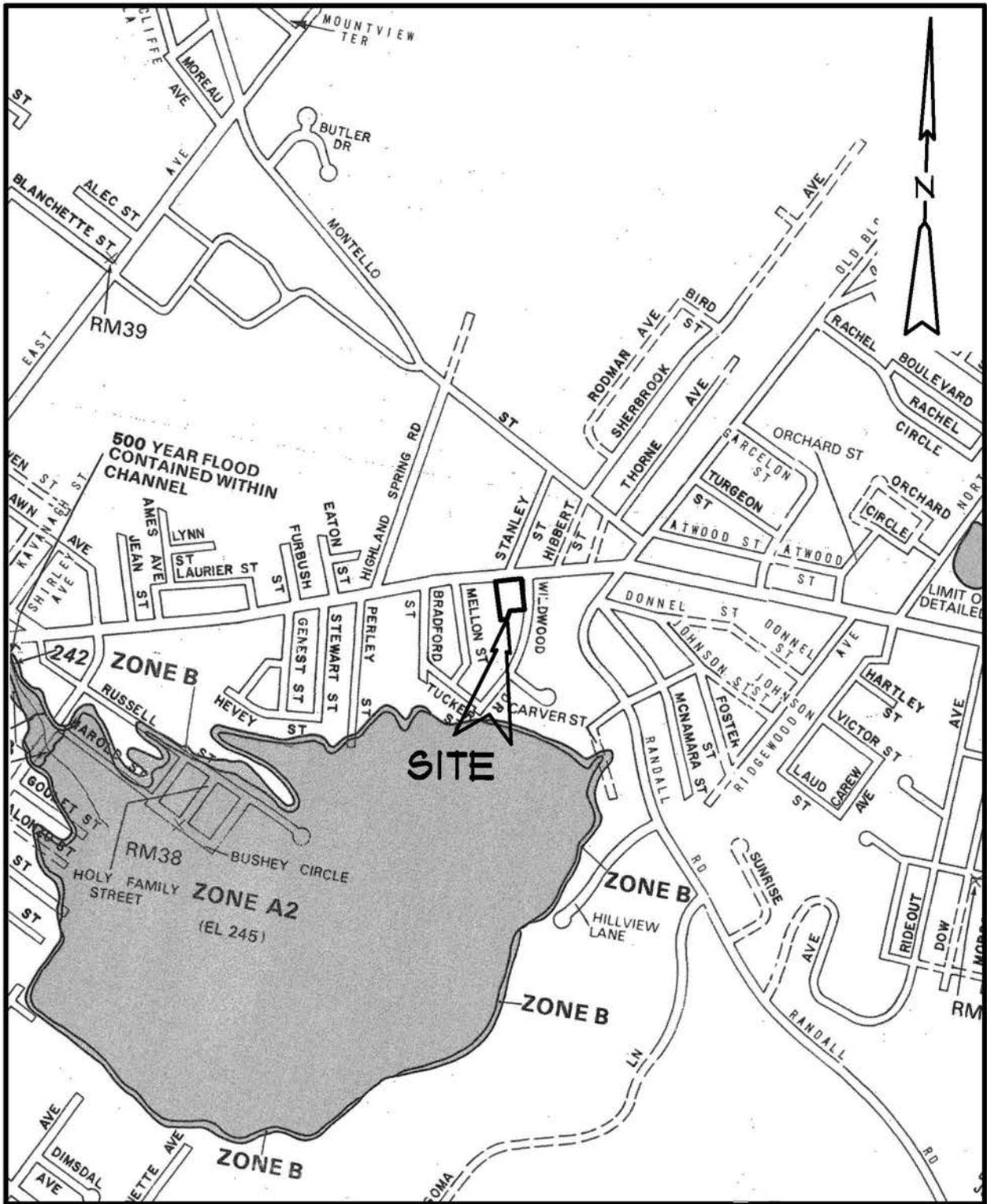
Stoneybrook
Consultants, Inc.



ZONING MAP

741-743 SABATTUS STREET, LEWISTON
 OWNER: KP REALTY, LLC
 SCALE: 1" = 2,000'±
 DATE OF GRAPHIC: OCTOBER 11, 2012
 SOURCE: CITY OF LEWISTON ZONING MAP
 PUBLICATION DATE: FEBRUARY 2011

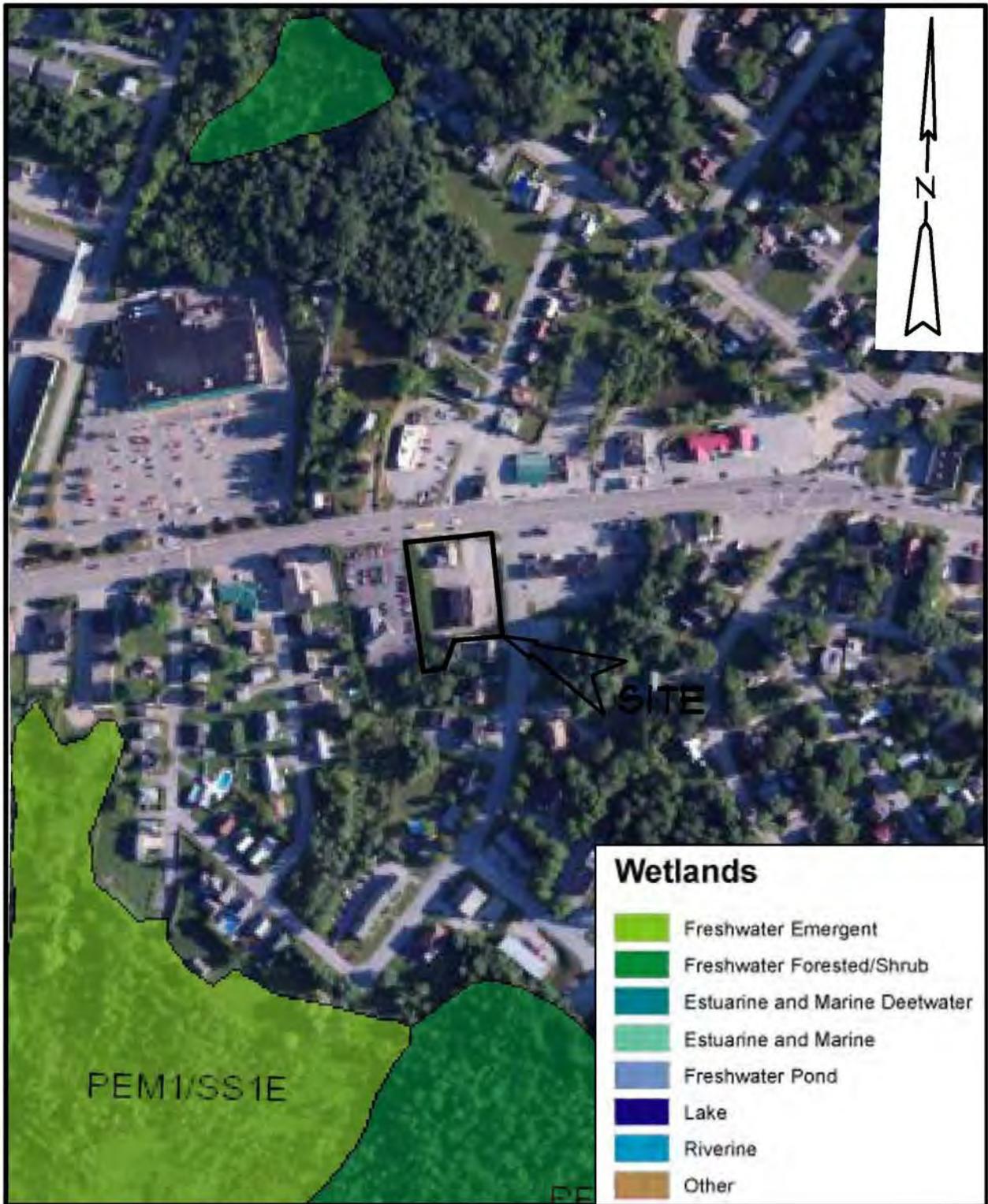
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 Consultants, Inc.



FLOOD MAP

741-743 SABATTUS STREET, LEWISTON
 OWNER: KP REALTY, LLC
 SCALE: 1" = 1,000'±
 DATE OF GRAPHIC: OCTOBER 11, 2012
 SOURCE: FEMA FLOOD INSURANCE RATE MAP
 PUBLICATION DATE: SEPTEMBER 28, 1979

Stoneybrook
 Consultants, Inc.



WETLANDS MAP

741-743 SABATTUS STREET, LEWISTON
 OWNER: KP REALTY, LLC
 SCALE: 1" = 300'±
 DATE OF GRAPHIC: OCTOBER 11, 2012
 SOURCE: US FISH AND WILDLIFE SERVICE
 PUBLICATION DATE: JANUARY 22, 2010

*Stoneybrook
 Consultants, Inc.*



SOILS LEGEND

BuB2	BUXTON SILT LOAM, 0 TO 8 PERCENT
HrC	HOLLIS FINE SANDY LOAM, 8 TO 15 PERCENT SLOPES
Pa	PEAT AND MUCK
ScA	SCANTIC SILT LOAM, 0 TO 3 PERCENT SLOPES
SyB	SUTTON VERY STONY LOAM, 0 TO 8 PERCENT SLOPES
MeB	MELROSE FINE SANDY LOAM, 0 TO 8 PERCENT SLOPES
ChD	CHARLTON VERY STONY FINE SANDY LOAM, 15 TO 25 PERCENT SLOPES

SOILS MAP

741-743 SABATTUS STREET, LEWISTON
 OWNER: KP REALTY, LLC
 SCALE: 1" = 500'±
 DATE OF GRAPHIC: OCTOBER 11, 2012
 SOURCE: USDA NRCS
 PUBLICATION DATE: JULY 27, 2009

Stoneybrook
 Consultants, Inc.

Section 3

Right, Title, Interest

WARRANTY DEED
(741 Sabattus Street, Lewiston)

APPLE MOUNTAIN LLC, a Maine limited liability company with an address of 319 Ridge Road, Wales, Maine, for consideration paid, grants to **K. P. REALTY LLC**, a Maine limited liability company with an address of 1385 Maine Street, Unit 1, Poland, Maine, 04274, with **WARRANTY COVENANTS**, certain lots or parcels of land, with any buildings thereon, situated in **LEWISTON**, County of **ANDROSCOGGIN**, and State of **MAINE**, bounded and described as follows:

PARCEL ONE:

Beginning at a point in the southerly line of Sabattus Street at the northwesterly corner of land owned by Edward Hunt; thence westerly by the southerly line of Sabattus Street one hundred thirty-six (136) feet, ten (10) inches to land formerly of I. J. Newton; thence south four (4) degrees thirty (30) minutes east two hundred fifty-nine (259) feet by said Newton land to land of said Edward Hunt; thence north thirty-four (34) degrees east by land of said Hunt to Sabattus Street and the point of beginning.

Being the same premises described in the deed from Irene Woodhead to Edmond Parent and Emma Parent dated November 9, 1948 and recorded in the Androscoggin County Registry of Deeds in Book 620, Page 308, the interest of Edmond Parent having been acquired by Emma Parent as surviving joint tenant upon Edmond's death on November 8, 1987.

PARCEL TWO:

Beginning at a point in the southerly line of Sabattus Street at the northeasterly corner of land previously conveyed to Sarah J. Newton by John Hibbert; thence South 4° 30' East sixteen rods; thence at a right angle easterly about forty (40) feet to land now or formerly of Edward Hunt; thence northeasterly by land of said Hunt about five (5) feet to a point that is forty-two (42) feet easterly from land now or formerly of Sarah J. Newton, measured at right angles; thence North 4° 30' West two hundred and fifty-one (251) feet to Sabattus Street; thence westerly by the southerly line of said Sabattus Street forty-two (42) feet to the point of beginning.

Being the same premises as the first parcel described in the deed from Sainte Famille Federal Credit Union to Edmond Parent and Emma Parent dated

NO MAINE R.E.
TRANSFER TAX PAID

November 17, 1954 and recorded in Book 700, Page 499, the interest of Edmond Parent having been acquired by Emma Parent as surviving joint tenant upon Edmond's death on November 8, 1987.

EXCEPTING FROM THE FOREGOING PARCELS ONE AND TWO SO MUCH THEREOF AS WAS PREVIOUSLY CONVEYED BY EDMOND PARENT AND EMMA PARENT BY THE FOLLOWING DEEDS:

(1) Deed to Reliable Aluminum Products Co., Inc. of Maine dated September 25, 1962 and recorded in Book 879, Page 158;

(2) Deed to Normand R. Fournier and Lorraine E. Fournier dated January 28, 1986 and recorded in Book 1901, Page 29; and

(3) Deed to Normand R. Fournier and Lorraine E. Fournier dated November 21, 1985 and recorded in Book 1901, Page 30.

ALSO EXCEPTED ARE THE FOLLOWING EASEMENTS:

(A) to Central Maine Power Company dated May 5, 1961 and recorded in Book 866, Page 96;

(B) to the City of Lewiston dated May 12, 1949 and recorded in Book 619, Page 502 for a sewer main;

(C) to Central Maine Power Company and New England Telephone and Telegraph Company dated October 8, 1985 and recorded November 5, 1985 in Book 1870, Page 295; and

(D) to Central Maine Power Company and Northern New England Telephone Operations, LLC dated November 14, 2008 and recorded in Book 7590, Page 244.

Being the same premises described in the Corrective Deed from Mildred Auger, Personal Representative of the Estate of Emma Parent, to Apple Mountain LLC dated October 16, 2008 and recorded in Book 7553, Page 297.

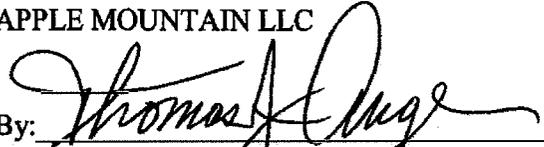
Also included herewith is the easement for use of a detention pond more particularly described in the deed from Thomas J. Auger to Apple Mountain LLC dated September 26, 2008 and recorded in Book 7539, Page 107.

IN WITNESS WHEREOF, the Grantor has executed this instrument on this 9th day of October, 2012.



Witness

APPLE MOUNTAIN LLC

By: 

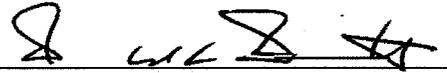
Thomas J. Auger, Sole Member

STATE OF MAINE
ANDROSCOGGIN, SS.

October 9, 2012

Then personally appeared the above-named Thomas J. Auger and acknowledged the foregoing instrument to be his free act and deed in said capacity and the free act and deed of Apple Mountain LLC.

Before me,



Ronald L. Bissonnette, Attorney at Law

df/F:\Darlene\CLIENTS\Pacheco\K. P. Realty LLC\Sabattus Street (741 & 743) Lewiston Purchase\WARRANTY DEED (741 Sabattus St).docx

ANDROSCOGGIN COUNTY
TINA H CHOUINARD
REGISTER OF DEEDS

**WARRANTY DEED
(743 Sabattus Street, Lewiston)**

THOMAS J. AUGER, of Wales, County of Kennebec, and State of Maine, with a mailing address of 319 Ridge Road, Wales, Maine, 04280, for consideration paid, grants to **K.P. REALTY LLC**, a Maine limited liability company with a mailing address of 1385 Maine Street, Unit 1, Poland, Maine, 04274, with **WARRANTY COVENANTS**, certain lots or parcels of land, with any buildings thereon, situated at 743 Sabattus Street in **LEWISTON**, County of **ANDROSCOGGIN**, and State of **MAINE**, bounded and described as follows:

PARCEL ONE: Beginning on the southerly side of Sabattus Street at an iron pipe driven into the ground, said pipe being located on the division line between land now or formerly of Edmond and Emma Parent and land now or formerly of Lugin C. Denis and Muguette Denis: thence in a westerly direction along the line of Sabattus Street a distance of seventy-eight and ten hundredths (78.10) feet, more or less, to a point; thence in a southerly direction ninety (90) feet to a point; thence in a westerly direction and parallel with said Sabattus Street a distance of fifty-one (51) feet to a point; thence at an angle and in a southerly direction a distance of one hundred seventy (170) feet more or less, to a point; thence at an angle and in a northeasterly direction to a point, said point being two hundred one and ninety-four hundredths (201.94) feet from the southerly side of Sabattus Street and ninety-five and forty-four hundredths (95.44) feet westerly from the easterly boundary of land now or formerly of said Parent; thence at an angle and in an easterly direction ninety-five and forty-four hundredths (95.44) feet to a point, said point being on the easterly boundary of property now or formerly of said Parent; thence at an angle and in a northerly direction two hundred one and ninety-four hundredths (201.94) feet to the southerly side of Sabattus Street and the point of beginning.

EXCEPTING AND RESERVING herefrom a one foot (1') strip of land as described in a deed from Reliable Aluminum Products Co. of Maine, Inc. to Jerry Blanchette & Son, Inc. dated September 15, 1971 and recorded in the Androscoggin County Registry of Deeds in Book 1040, Page 347.

Also included is all of Grantor's rights in a right-of-way of ten (10) feet being located at a point ninety (90) feet from said Sabattus Street and forty-three (43) feet from the westerly line of land now or formerly of said Parent, said point being the westerly boundary of the property conveyed as described above and extending a uniform width of ten (10) feet on said Parent property from said point in a southerly direction along the westerly boundary of the property described

above one hundred seventy (170) feet, more or less, to the southerly point of the parcel described above.

Being the same premises described in the Deed from Pierre V. and Jeannine Levesque to Thomas J. Auger dated April 26, 1999 and recorded in the Androscoggin County Registry of Deeds in Book 4220, Page 302.

SUBJECT TO the following easements:

(1) In Book 619, Page 502 there is an Easement dated May 12, 1949 and recorded May 20, 1949 from Edmond Parent and Emma Parent to the City of Lewiston for the right to maintain a sewer pipe.

(2) In Book 866, Page 96 there is an Easement dated May 5, 1961 and recorded March 2, 1962 from Edmond J. Parent to Central Maine Power Company for poles and wires.

PARCEL TWO: Beginning at the southwesterly corner of Lot 1 as depicted on the Plan of "Wildwood Village" recorded in Plan Book 21, Page 41 (being the southwesterly corner of land described in the deed from Jerry Blanchette & Son, Inc. to Maurice Beaudry and Patricia Beaudry dated November 8, 1972 and recorded in Book 1064, Page 147);

thence northeasterly along the northwesterly line of said Lot 1 a distance of ten (10) feet, more or less, to land conveyed by deed from Pierre V. Levesque and Jeannine Levesque to Thomas J. Auger dated April 26, 1999 and recorded in Book 4220, Page 302;

thence northwesterly along said land conveyed to Thomas J. Auger by deed recorded in said Registry in Book 4220, Page 302, a distance of seventy (70) feet, more or less, to land formerly of Emma Parent, now of Apple Mountain LLC (see deed of sale from the Estate of Emma Parent to Apple Mountain LLC recorded in Book 5593, Page 214 and corrective deed of sale from said Estate recorded in Book 7553, Page 297);

thence southwesterly along said land of Apple Mountain LLC a distance of forty-two (42) feet, more or less, to land conveyed by deed from Peoples Heritage Savings Bank to Thomas J. Auger dated April 30, 1999 and recorded in Book 4220, Page 304;

thence southeasterly along said land conveyed to Auger by deed recorded in Book 4220, Page 304 a distance of seventy-four (74) feet, more or less to the southeasterly corner of said land conveyed to Auger by said deed;

thence easterly a distance of forty (40) feet, more or less, to the point of beginning.

Being a part of the premises conveyed by deed from Ronald C. Blanchette to Thomas Auger dated July 12, 1999 and recorded in Book 4300, Page 124. See also Confirmatory Release Deed from Sabattus Street Associates to Thomas J. Auger dated August 11, 1999 and recorded in Book 4300, Page 128 and Affidavit of Ronald C. Blanchette dated August 10, 1999 and recorded in Book 4300, Page 122. The above described parcel is the same as "Excepted Parcel D" reserved in the deed from Thomas J. Auger to LAACP, LLC dated October 1, 2008 and recorded in Book 7541, Page 117.

SUBJECT TO the following exceptions to the extent they encumber the above described parcel:

- (1) Easement from Edmond J. Parent and Emma D. Parent to Central Maine Power Company and New England Telephone and Telegraph Company dated November 5, 1985 and recorded in Book 1870, Page 295;
- (2) Easement from Edmond J. Parent and Emma D. Parent to Central Maine Power Company dated May 5, 1961 and recorded in Book 866, Page 96;
- (3) Easement from Edmond Parent and Emma Parent to the City of Lewiston dated May 12, 1949 and recorded in Book 619, Page 502;
- (4) Easement for a right of way ten feet in width granted in the deed from Edmond Parent and Emma Parent to Reliable Aluminum Products Company, Inc. of Maine in the deed dated September 25, 1962 and recorded in Book 879, Page 158 which is the same easement included with and benefiting Parcel One hereinabove.

PARCEL THREE: Beginning at a point on the northwesterly corner of the building now or formerly owned or occupied by Normand R. Fournier and Lorraine E. Fournier which building is situated on land conveyed to the said Fourniers by Rodney Janelle, by deed dated February 19, 1984 and recorded in the Androscoggin County Registry of Deeds in Book 1807, Page 45, now owned by Thomas J. Auger (see deed from Pierre and Jeannine Levesque dated April 26, 1999 and recorded in Book 4220, Page 304); thence in a northerly direction on the extension of the westerly face of said building fourteen (14) feet more or less to a point on the southerly line of land formerly of Edmond J. and Emma D. Parent. Said point being ninety (90) feet southerly of the apparent southerly sideline of Sabattus Street; thence in a westerly direction perpendicular to the last described line five (5) feet to a point; thence in a southerly direction perpendicular to the last described line and running parallel to and five (5) feet westerly of the westerly face of said building ninety-five (95) feet, more or less, to the northerly point of a parcel more particularly described in a deed from the said Parents to the said Fourniers dated November 21, 1985 and recorded in the Androscoggin County Registry of Deeds in Book 1901, Page 30; thence in an easterly direction along the extension of the southerly face of the said building to

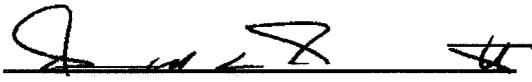
the southwesterly corner of said building; thence in a northerly direction along the westerly face of the said building eighty-one (81) feet more or less to the point of beginning.

Said parcel contains 450 square feet, more or less.

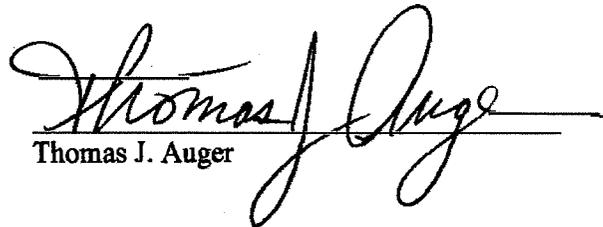
Being the same premises described in the deed from Ronald C. Blanchette to Thomas J. Auger dated September 5, 2008 and recorded in Book 7525, Page 193.

ALSO INCLUDED HEREWITH for the benefit of Parcel One, Parcel Two and Parcel Three is the easement reserved in paragraph (5) of the "EXCEPTIONS AND RESERVATIONS" in the deed to LAACP, LLC dated October 1, 2008 and recorded in Book 7541, Page 117, to allow storm water runoff to drain onto Parcel Three described in said deed recorded in Book 7541, Page 117 and to connect to any storm water drainage system, detention pond and other storm water detention systems (collectively the "Storm Water Systems") constructed or to be constructed on said Parcel Three. In addition, the owner of the land benefitted by this easement shall have the right to enter upon said Parcel Three from time to time to make such further improvements to the Storm Water Systems as the owner of the benefitted land may desire but at the sole expense of the owner of the benefitted land. This easement will run with the land.

IN WITNESS WHEREOF, the Grantor has executed this instrument on this 9th day of October, 2012.



Witness



Thomas J. Auger

STATE OF MAINE
ANDROSCOGGIN, SS.

October 9, 2012

Then personally appeared the above-named Thomas J. Auger and acknowledged the foregoing instrument to be his free act and deed.

Before me,



Ronald L. Bissonnette, Attorney at Law

d:\F:\Darlene\CLIENTS\Pacheco\K. P. Realty LLC\Sabattus Street (741 & 743) Lewiston Purchase\WARRANTY DEED (743 Sabattus St).docx

Section 4

Project Plans

N/F
GUY B. DOSTIE
MICHA ST. PIERRE
119-40
2004/193

N/F
RANCOURT ASSOCIATES
119-41
4583/57

N/F
KENNETH DAIGLE
119-42
4100/328
RESTAURANT

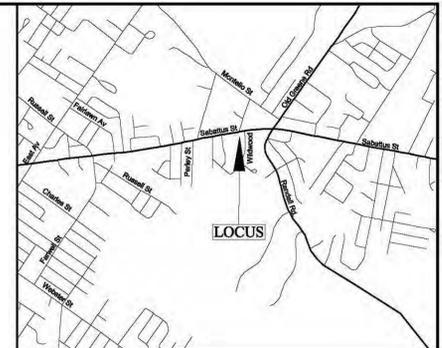
N/F
CHELSEA MARKET REALTY LLC
119-43
7899/304
PAVED AREA

N/F
LAACP, LLC
119-281
7541/117

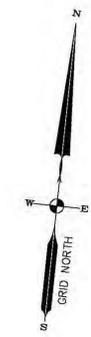
N/F
KATHLEEN A. LACOURSE
119-285
8357/231
LOT 1
PLAN REF. A

N/F
CHARLES R. PELLETIER
119-284
1745/8

N/F
LAACP, LLC
119-312
7541/117



LOCATION PLAN



LEGEND

- SUBJECT BOUNDARY LINE
- ABUTTER OR RIGHT OF WAY LINE
- TREELINE
- NOW OR FORMERLY
- DEED BOOK PAGE REFERENCE
- TAX MAP AND LOT NUMBER
- DECIDUOUS / CONIFEROUS TREE
- CONCRETE MONUMENT FOUND
- IRON PIPE/ROD/REBAR FOUND
- DRILL HOLE FOUND
- 5/8" REBAR W/ CAP TO BE SET
- UTILITY POLE
- SEWER MANHOLE
- DRAIN MANHOLE
- CATCH BASINS

NOTES:

1. RECORD OWNER:
LOCUS 1: THOMAS J. AUGER
LOCUS 2: APPLE MOUNTAIN, LLC
2. PARCEL DEED REFERENCE: LOCUS 1, SEE DEED FROM PIERRE V. AND JEANNINE LEVESQUE TO THOMAS J. AUGER DATED APRIL 26, 1999, RECORDED AT THE ANDROSCOGGIN COUNTY REGISTRY OF DEEDS IN BOOK 4220, PAGE 302. LOCUS 2, SEE DEED FROM MILDRED AUGER TO APPLE MOUNTAIN LLC DATED OCTOBER 16, 2008, RECORDED AT THE ANDROSCOGGIN COUNTY REGISTRY OF DEEDS IN BOOK 7553, PAGE 297.
3. ALL BOOK AND PAGE REFERENCES REFER TO THE ANDROSCOGGIN COUNTY REGISTRY OF DEEDS.
4. PARCEL TAX MAP REFERENCE:
LOCUS 1: CITY OF LEWISTON, MAP 119, LOT 286
LOCUS 2: CITY OF LEWISTON, MAP 119, LOT 287
5. TOTAL AREA OF PARCEL 38,045 SQUARE FEET (INCLUDES LOCUS PARCEL 1&2).
6. ALL BEARINGS ARE REFERENCED TO MAINE STATE PLANE WEST ZONE NAD83 GRID NORTH.
7. ELEVATIONS SHOWN ARE TIED TO NAVD 88 BY LOCATION OF MDOOT DISK 177-11.
8. ALL UNDERGROUND UTILITIES DEPICTED HEREON ARE BASED ON AVAILABLE EVIDENCE AND PLANS AND SHOULD BE CONSIDERED APPROXIMATE. ALL SUCH UTILITIES SHOULD BE CONFIRMED BY DIG-SAFE OR SIMILAR ENTITY BEFORE ANY CONSTRUCTION TAKES PLACE.
9. PLAN REFERENCES:
A. "REVISION NO.2 WILDWOOD VILLAGE, LEWISTON, MAINE, JERRY BLANCHETTE & SON DEVELOPER, 6 WILDWOOD DRIVE, LEWISTON, MAINE," DATED NOVEMBER 17, 1978, RECORDED IN SAID REGISTRY OF DEEDS IN PLAN BOOK 25, PAGE 116.
B. "PROPERTY OF EMMA PARENT, 741 SABATTUS STREET, LEWISTON, MAINE," DATED 1/19/1988, PREPARED BY A.L. & H. SURVEYORS & ENGINEERS, INC., RECORDED AT SAID REGISTRY OF DEEDS IN PLAN BOOK 34, PAGE 110.
C. "MAINE DEPARTMENT OF TRANSPORTATION RIGHT OF WAY MAP, STATE HIGHWAY "153", LEWISTON, ANDROSCOGGIN COUNTY," SHEET 38-4, DOT FILE NO. 1-203, DATED MARCH 1991, RECORDED AT SAID REGISTRY OF DEEDS IN PLAN BOOK 38, PAGES 60 & 61.
D. "LAND TITLE & TOPOGRAPHIC SURVEY FUTURE TIM HORTONS, 736 SABATTUS STREET, LEWISTON, MAINE," DATED JULY 2005, PREPARED BY TECHNICAL SERVICES, INC.
E. "EXISTING CONDITIONS PLAN, CUMBERLAND FARMS, 753-793 SABATTUS STREET, LEWISTON, MAINE," DATED 02/01/12, PREPARED BY SEBAGO TECHNICS.

REVISIONS			
NO.	DATE	DESCRIPTION	BY
01	4/22/13	ADDED AREA NORTH OF SABATTUS STREET	MH
02	5/2/13	REV. SEWER LINE PER CITY OF LEWISTON LOCATION	MH

EXISTING CONDITIONS PLAN
741 & 743 SABATTUS STREET
LEWISTON, MAINE

PREPARED FOR: K. P. REALTY LLC
1385 MAINE STREET, UNIT 1
POLAND, ME 04274

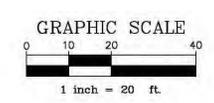
PREPARED BY:
JONES ASSOCIATES INC.
Foresters, Surveyors And Environmental Consultants
63 TUCKER LANE, POLAND SPRING, MAINE 04274
(207) 958-3242

PLAN DATE:
10/1/12

FIELD WORK DATE:
9/26/12

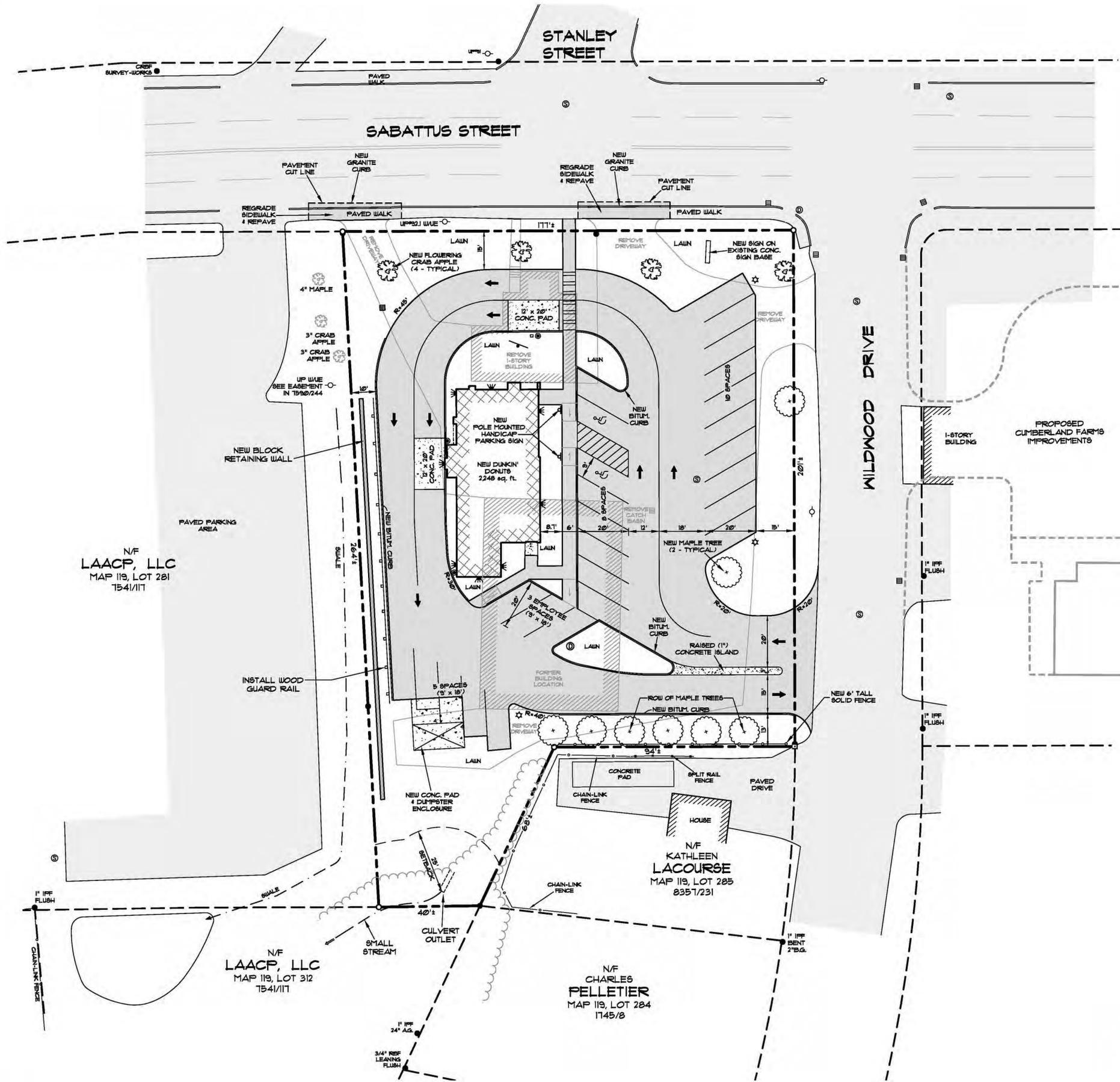
SCALE: 1"=20'

PROJ. #: 12-061LE



CERTIFICATION:
I HEREBY CERTIFY THAT THIS BOUNDARY SURVEY CONFORMS TO THE STATE OF MAINE BOARD OF LICENSURE FOR PROFESSIONAL LAND SURVEYORS, STANDARDS OF PRACTICE (CHAPTER 90, SECTIONS 1 & 2).
Michael A. Hartman
MICHAEL A. HARTMAN, P.L.S. #2433
FOR JONES ASSOCIATES INC.





NOTES

- EXISTING CONDITIONS INFORMATION WAS TAKEN FROM A PLAN ENTITLED "EXISTING CONDITIONS PLAN - 141-143 SABATTUS STREET" PREPARED BY JONES ASSOCIATES, INC. DATED OCTOBER 1, 2012.
- ALL BOOK AND PAGE NUMBERS REFER TO THE ANDROSCOGGIN COUNTY REGISTRY OF DEEDS.
- OWNER OF RECORD - KP REALTY LLC
 141 SABATTUS STREET - TAX MAP 119, LOT 281
 DEED REFERENCE - BOOK 0510, PAGE 25
 143 SABATTUS STREET - TAX MAP 119, LOT 286
 DEED REFERENCE - BOOK 0510, PAGE 28
- THE PARCEL IS LOCATED IN THE HIGHWAY BUSINESS ZONING DISTRICT.
- 26 TOTAL PARKING SPACES INCLUDING 2 HANDICAP ACCESSIBLE SPACES AND 24 STANDARD SPACES. SPACES ARE 10' X 20' EXCEPT WHERE NOTED.
- THIS PLAN WAS PREPARED TO SHOW A FULL SERVICE DUNKIN' DONUTS WITH SECOND FLOOR OFFICE SPACE TO FACILITATE DISCUSSIONS OF THIS SITE'S DEVELOPMENT POTENTIAL.

LEGEND

- BOUNDARY LINE (SUBJECT PARCEL)
- BOUNDARY LINE (OTHER)
- IRON ROD OR PIPE FOUND
- N/F NOW OR FORMERLY
- 2356/83 BOOK AND PAGE NUMBER
- UTILITY POLE
- ⊙ EXISTING SEWER MANHOLE
- ⊞ EXISTING CATCH BASIN
- ⊞ NEW FIELD INLET (CATCH BASIN)
- ⊞ EXISTING STORM DRAIN MANHOLE
- ⊞ NEW STORM DRAIN MANHOLE
- ★ NEW POLE MOUNTED EXTERIOR LIGHT
- ★ NEW WALL MOUNTED EXTERIOR LIGHT
- ▨ EXISTING BUILDING
- ▨ NEW BUILDING
- ▨ EXISTING PAVEMENT
- ▨ NEW PAVEMENT
- ⊙ NEW FLOWERING CRAB APPLE (4) - 2 1/2" CALIFER AT PLANTING
- ⊙ NEW MAPLE TREE (2) - 2 1/2" CALIFER AT PLANTING

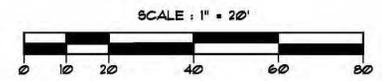
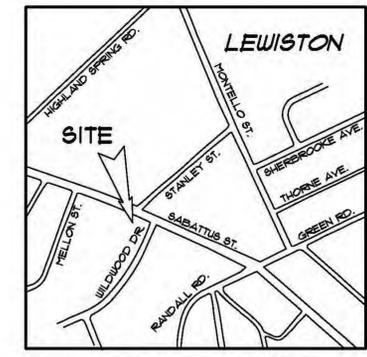
SITE TABULATIONS

EXISTING IMPERVIOUS AREA = 22,515 sq. ft.
 PROPOSED IMPERVIOUS AREA = 22,512 sq. ft.
 DRIVE-THRU LANE STACKING = 16 VEHICLES
 PROPOSED PARKING = 26 SPACES

APPROVAL

APPROVED BY THE CITY OF LEWISTON PLANNING BOARD

CHAIRMAN _____ DATE _____
 IF DEVELOPMENT HAS NOT OCCURRED AS DEFINED WITHIN THE SCOPE OF THE CITY OF LEWISTON CODE OF ORDINANCES, WITHIN 24 MONTHS OF THE DATE OF APPROVAL, DEVELOPMENT REVIEW APPROVAL SHALL EXPIRE. THE APPLICANT MAY NOT BEGIN CONSTRUCTION OR OPERATION OF THE DEVELOPMENT UNTIL A NEW APPROVAL IS GRANTED (ARTICLE XIII, SECTION II).



REVISED: MAY 3, 2013 - ADD PARKING SPACE
 REVISED: APRIL 18, 2013 - ADD LIGHTS, TREES, ADJUST PYMT.
 REVISED: APRIL 15, 2013 - 15' SETBACK ON SABATTUS

SHEET TITLE: **SITE PLAN**
 PROJECT: **DUNKIN' DONUTS**
 141-143 SABATTUS STREET - LEWISTON, ME
 CLIENT: **KP REALTY, LLC**
 1385 MAINE STREET #1 - FOLAND, MAINE 04274
 SCALE: 1" = 20'
 DATE: MARCH 2013
 DRAWN BY: KRF
 CHECKED BY: WMP

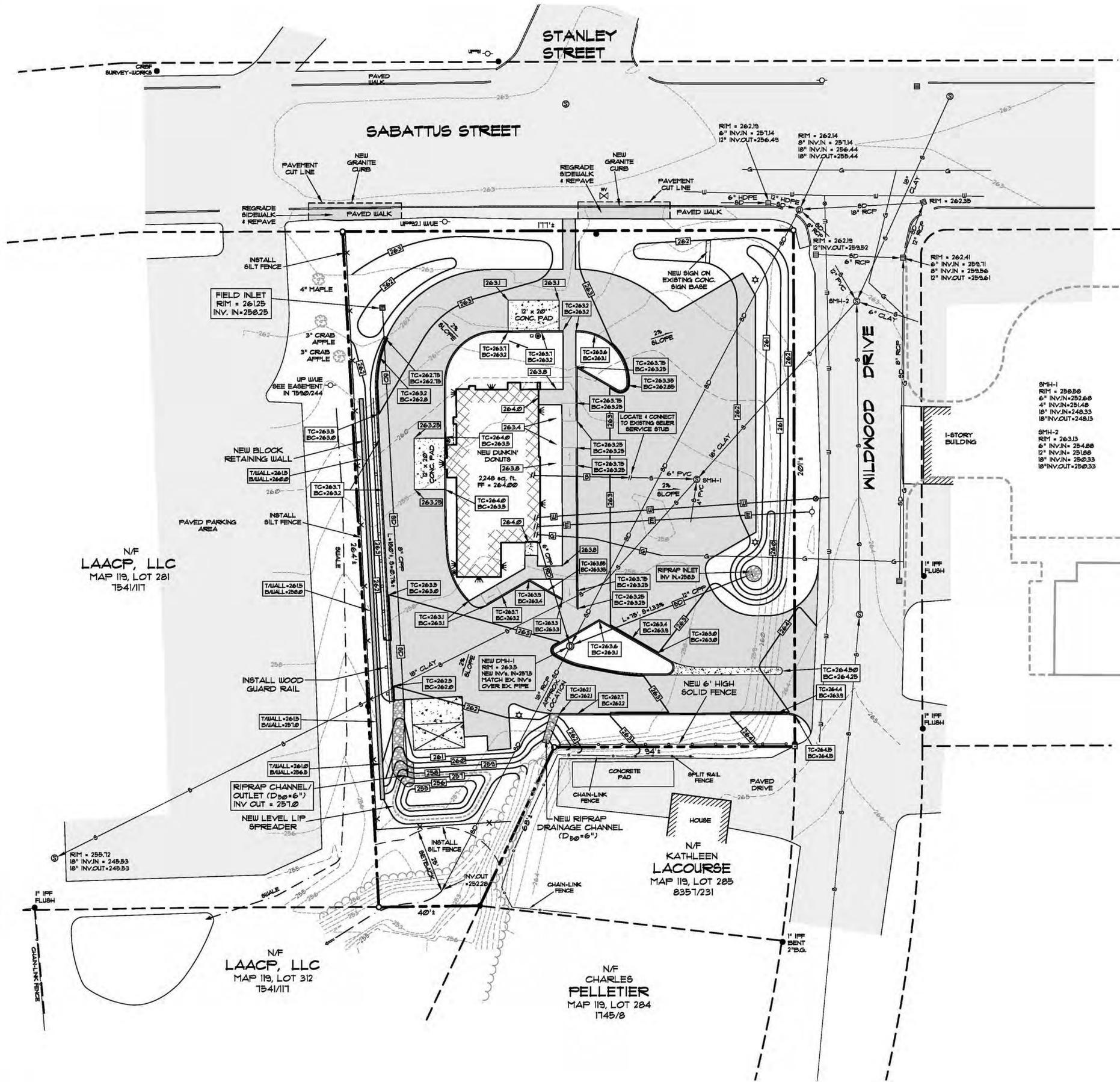
PROJECT: **Stoneybrook Consultants, Inc.**
 456 Buckfield Road - Turner, Maine 04282
 (207) 514-7491 Voice / (207) 514-7492 Fax
 Tel: (207) 516-3319
 Fax: (207) 755-6128
 www.stoneybrook.com

SUMMIT
 GEOENGINEERING SERVICES
 Tel: (207) 516-3319
 Fax: (207) 755-6128
 www.stoneybrook.com

640 MAIN ST.
 LEWISTON, ME 04240



JOB NO. - 13043
 SHEET |

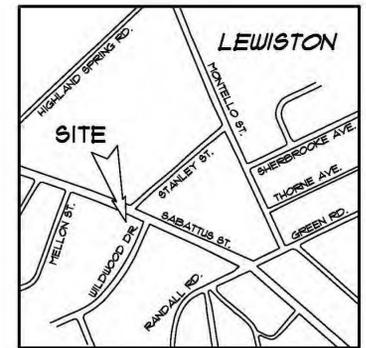


NOTES

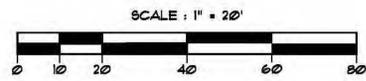
- EXISTING CONDITIONS INFORMATION WAS TAKEN FROM A PLAN ENTITLED "EXISTING CONDITIONS PLAN - 741-743 SABATTUS STREET" PREPARED BY JONES ASSOCIATES, INC. DATED OCTOBER 1, 2012.
- ALL BOOK AND PAGE NUMBERS REFER TO THE ANDROSCOGGIN COUNTY REGISTRY OF DEEDS.
- OWNER OF RECORD - KP REALTY LLC
 741 SABATTUS STREET - TAX MAP 119, LOT 281
 743 SABATTUS STREET - TAX MAP 119, LOT 286
 DEED REFERENCE - BOOK 8510, PAGE 25
 DEED REFERENCE - BOOK 8510, PAGE 28
- THE PARCEL IS LOCATED IN THE HIGHWAY BUSINESS ZONING DISTRICT.
- THIS PLAN WAS PREPARED TO SHOW A FULL SERVICE DUNKIN' DONUTS WITH SECOND FLOOR OFFICE SPACE TO FACILITATE DISCUSSIONS OF THIS SITE'S DEVELOPMENT POTENTIAL.
- THE LOCATION, DEPTH AND SIZE OF ALL UNDERGROUND UTILITY LINES, TANKS AND/OR STRUCTURES WAS NOT VERIFIED. CONTRACTOR SHALL CONTACT DIGSAFE/ON-TARGET PRIOR TO EXCAVATION TO HAVE ALL PUBLIC AND PRIVATE UTILITY LOCATIONS IN THE PROJECT AREA DETERMINED.
- CONNECT ROOF DRAIN AND FOUNDATION DRAIN LINES TO NEW DMH-1. LAYOUT, LOCATION, TYPE AND SIZE TO BE DETERMINED BY ARCHITECT, OWNER AND CONTRACTOR PRIOR TO INSTALLATION.
- NEW UTILITY CONNECTIONS (WATER, SEWER, GAS) SHALL USE EXISTING SERVICE LINES WITHIN THE PROJECT AREA WHERE POSSIBLE. SAID CONNECTIONS SHALL COMPLY WITH THE STANDARDS OF EACH RESPECTIVE UTILITY COMPANY.

LEGEND

---	BOUNDARY LINE (SUBJECT PARCEL)
---	BOUNDARY LINE (OTHER)
●	IRON ROD OR PIPE FOUND
○	NOW OR FORMERLY
2356/893	BOOK AND PAGE NUMBER
○	UTILITY POLE
○	EXISTING SEWER MANHOLE
■	EXISTING CATCH BASIN
■	NEW FIELD INLET (CATCH BASIN)
○	EXISTING STORM DRAIN MANHOLE
○	NEW STORM DRAIN MANHOLE
○	NEW BOLLARD
---	EXISTING CONTOUR
263	NEW CONTOUR
SD	EXISTING STORM DRAIN
SD	NEW STORM DRAIN
RD	NEW ROOF DRAIN
S	EXISTING SEWER LINE
S	NEW SEWER LINE
G	EXISTING GAS LINE
G	NEW GAS LINE
W	EXISTING WATER LINE
W	NEW WATER LINE
E	NEW UNDERGROUND ELECTRIC LINE
263.4	NEW SPOT GRADE
TC	TOP OF CURB
BC	BOTTOM OF CURB
TWALL	TOP OF WALL
BWALL	BOTTOM OF WALL
[Hatched Box]	EXISTING BUILDING
[Cross-hatched Box]	NEW BUILDING
[Dotted Box]	EXISTING PAVEMENT
[Solid Box]	NEW PAVEMENT



LOCATION MAP



REVISED: 5-3-13 - ADD ROOF DRAIN & STORM DRAIN LINES
 REVISED: 4-18-13 - ADD LIGHT POLES, ADJUST PAVEMENT
 REVISED: 4-16-13 - RECONFIGURE CURBS & SPOT GRADES
 REVISED: 4-12-13 - RECONFIGURE CURBS & SPOT GRADES
 SHEET TITLE:

GRADING & UTILITY PLAN

PROJECT: **DUNKIN' DONUTS**
 741-743 SABATTUS STREET - LEWISTON, ME

CLIENT: **KP REALTY, LLC**
 1385 MAINE STREET #1 - FOLAND, MAINE 04274

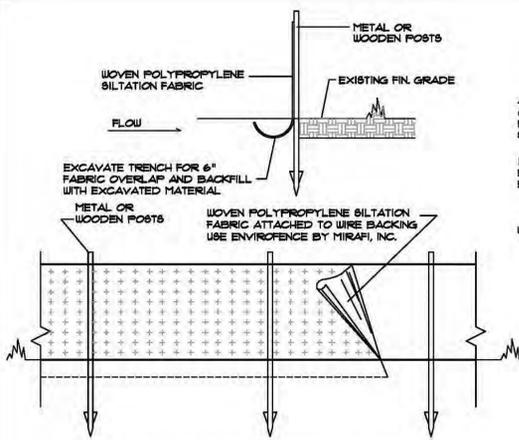
SCALE: 1" = 20'
 DATE: MARCH 2013
 DRAWN BY: KRF
 CHECKED BY: WMP

Stoneybrook Consultants, Inc.
 456 Buckfield Road - Turner, Maine 04282
 (207) 514-7491 Voice / (207) 514-7492 Fax

Tel: (207) 516-3319
 Fax: (207) 755-6128
 www.summitgeo.com

SUMMIT
 GEOENGINEERING SERVICES

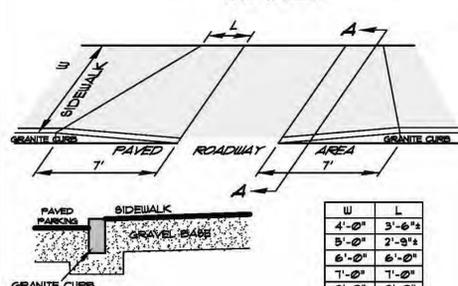
STATE OF MAINE
 WILLIAM M. PETERLEIN
 5787
 LICENSED PROFESSIONAL ENGINEER



SILT FENCE DETAIL
NOT TO SCALE

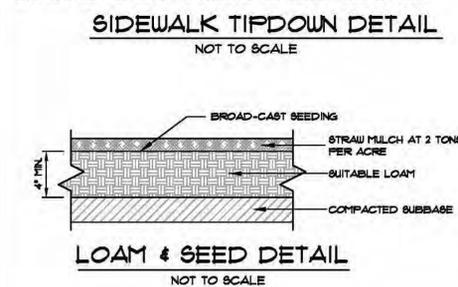
NOTE: REFERENCE IS MADE TO THE BEST MANAGEMENT PRACTICE FOR EROSION AND SEDIMENT CONTROL. B-1 SEDIMENT BARRIERS.

SILTATION FABRIC WITH INTEGRAL MESH AND POSTS MAY BE USED.

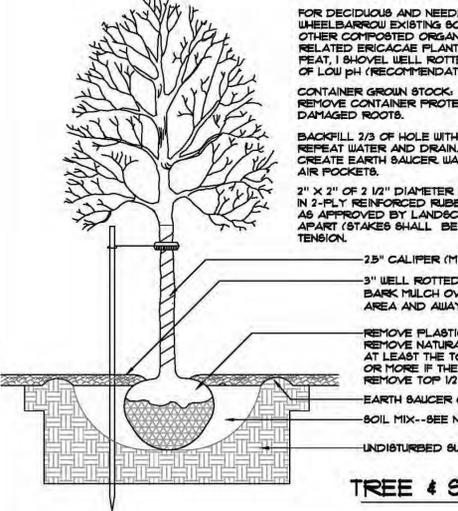


SECTION A-A
NOT TO SCALE

NOTES:
1) THE DIMENSIONS SHOWN AT ROADWAY EDGE ARE FIXED DISTANCES.
2) RAHP CROSS SECTION TO BE 6"± AS ADJACENT SIDEWALK (DEPTH OF SURFACE AND FOUNDATION).
3) IN NO CASE ARE THE RAHPs TO BE PLACED BEHIND THE STOP LINE.



LOAM & SEED DETAIL
NOT TO SCALE



TREE & SHRUB INSTALLATION DETAIL
NOT TO SCALE

DIG HOLE AT LEAST 2 TIMES THE WIDTH OF ROOT BALL AND AS DEEP AS THE ROOT BALL (NO DEEPER); SET ROOT BALL CENTERED, WITH TOP AT GROUND LEVEL OR SLIGHTLY HIGHER; CORRECT HOLE DEPTH AS NEEDED.

FOR DECIDUOUS AND NEEDLED EVERGREEN TREES AND SHRUBS: 1 FULL WHEELBARROW EXISTING SOIL, 2 SHOVELS PEAT, 1 SHOVEL WELL ROTTED MANURE OR OTHER COMPOSTED ORGANIC MATERIAL FOR BROADLEAVED EVERGREENS AND RELATED ERICACEAE PLANTS, 1 FULL WHEELBARROW EXISTING SOIL, 3-4 SHOVELS PEAT, 1 SHOVEL WELL ROTTED MANURE, OR OTHER COMPOSTED ORGANIC MATERIAL OF LOW PH (RECOMMENDATIONS ARE FOR ACCEPTABLE PLANTING AREAS).

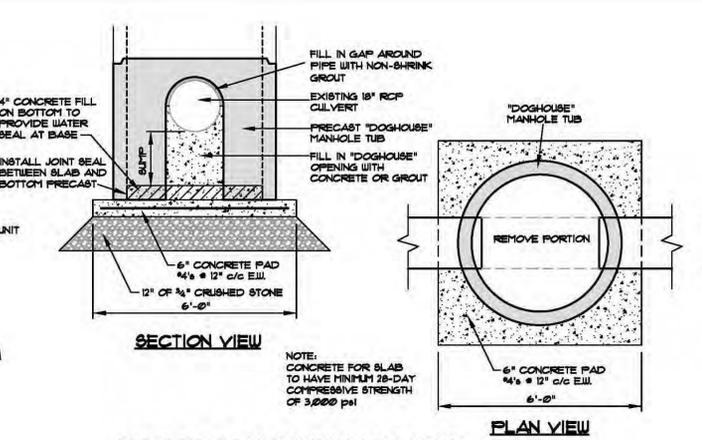
CONTAINER GROWN STOCK: REMOVE CONTAINER PROTECTING ROOT BALL. GENTLY COMB OUT ROOTS, PRUNE DAMAGED ROOTS.

BACKFILL 2/3 OF HOLE WITH AMENDED SOIL THEN FILL HOLE WITH WATER. LET DRAIN. REPEAT WATER AND DRAIN. BACK FILL TO FINISH GRADE. TAMP GENTLY, AND CREATE EARTH SAUCER. WATER THOROUGHLY ONCE AGAIN TO REMOVE REMAINING AIR POCKETS.

2" x 2" OF 2 1/2" DIAMETER STAKES WITH 2 #12 GALV. WIRES ENCASED IN 2-PLY REINFORCED RUBBER HOSE 1/2" DIAMETER OR CHAINLOCK AS APPROVED BY LANDSCAPE ARCHITECT. 2 STAKES LOCATED 180° APART (STAKES SHALL BE FIRMLY SET TO PROVIDE NECESSARY TENSION).

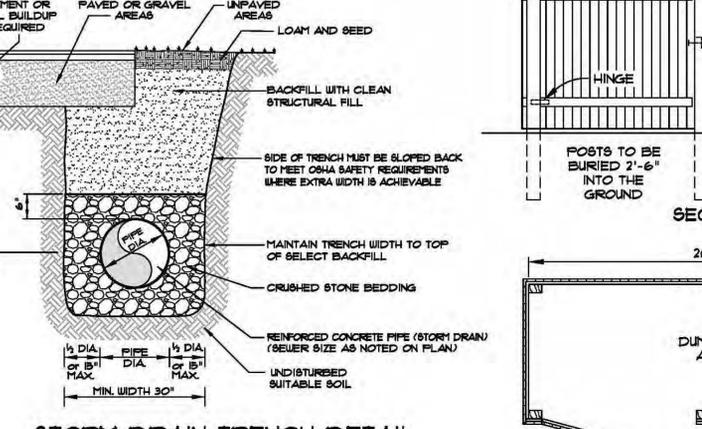
2 1/2" GALV. (MIN.)
3" WELL ROTTED BLACK FINE BARK MULCH OVER PLANTING AREA AND AWAY FROM TRUNK.

REMOVE PLASTIC BURLAP.
REMOVE NATURAL FIBER BURLAP FROM AT LEAST THE TOP 1/3 OF ROOTBALL OR MORE IF THE ROOTBALL IS FIRMLY SET.
EARTH SAUCER (3" HT.)
SOIL MIX--SEE NOTE ABOVE
UNDISTURBED SUBGRADE

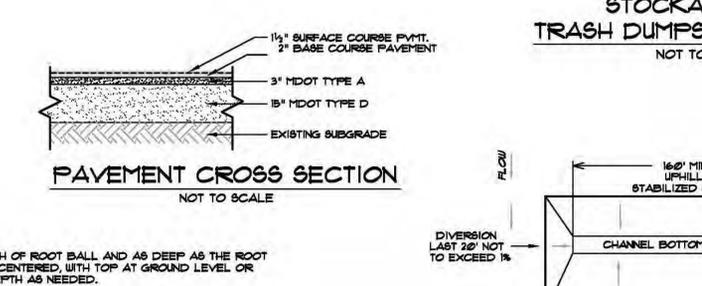


NEW "DOGHOUSE" MANHOLE CONNECTION TO EXISTING STORM DRAIN
NOT TO SCALE

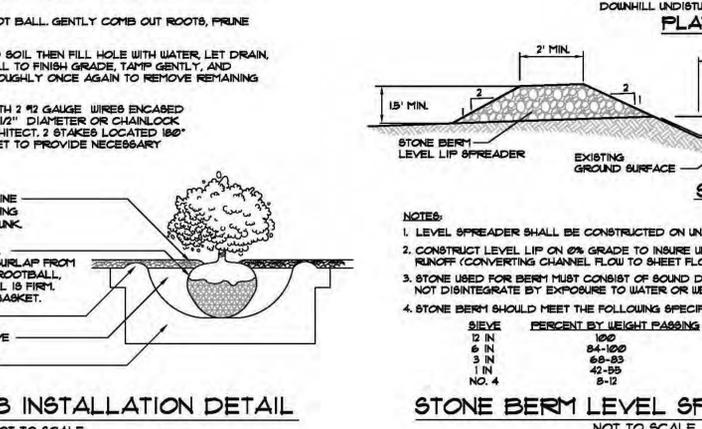
NOTE: CONCRETE FOR SLAB TO HAVE MINIMUM 28-DAY COMPRESSIVE STRENGTH OF 3,000 PSI.



STORM DRAIN TRENCH DETAIL
NOT TO SCALE



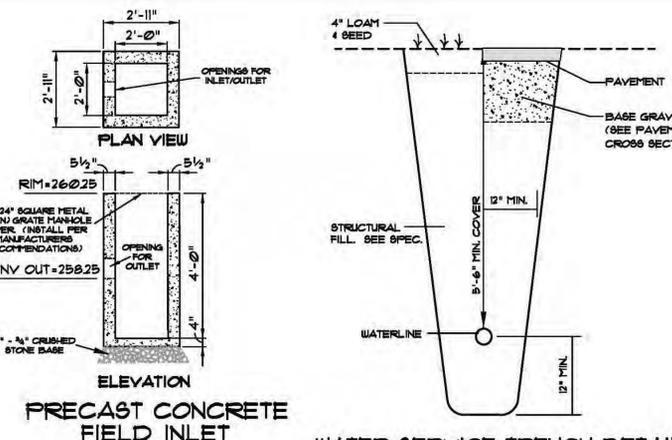
PAVEMENT CROSS SECTION
NOT TO SCALE



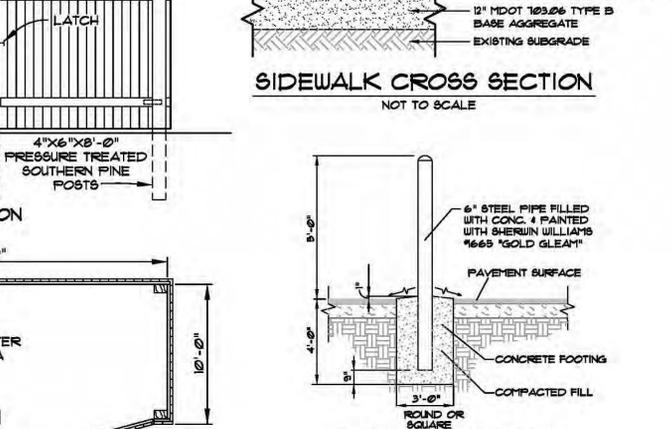
STONE BERM LEVEL SPREADER DETAIL
NOT TO SCALE

NOTES:
1. LEVEL SPREADER SHALL BE CONSTRUCTED ON UNDISTURBED SOIL (NOT ON FILL).
2. CONSTRUCT LEVEL LIP ON 0% GRADE TO INSURE UNIFORM SPREADING OF SEDIMENT-FREE RUNOFF (CONVERTING CHANNEL FLOW TO SHEET FLOW).
3. STONE USED FOR BERM MUST CONSIST OF SOUND DURABLE ROCK THAT WILL NOT DISINTEGRATE BY EXPOSURE TO WATER OR WEATHER.
4. STONE BERM SHOULD MEET THE FOLLOWING SPECIFICATIONS:

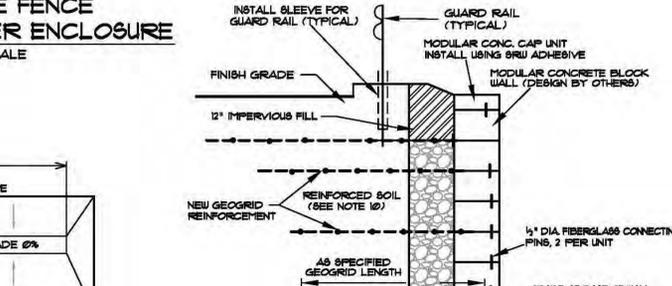
SIZE	PERCENT BY WEIGHT PASSING
12 IN	100
6 IN	100
3 IN	84-100
1 1/2 IN	68-85
3/4 IN	42-55
NO. 4	8-12



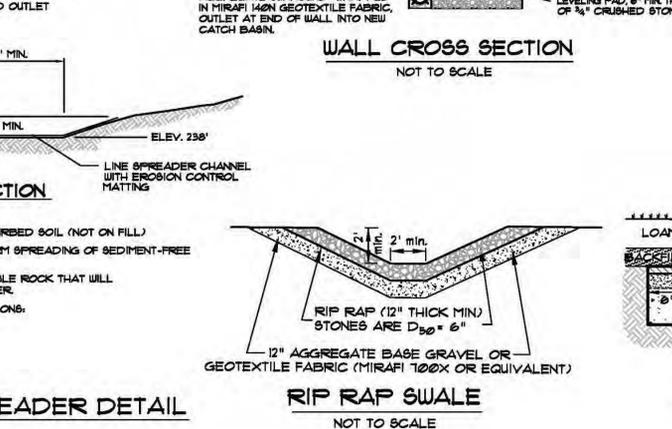
PRECAST CONCRETE FIELD INLET
NOT TO SCALE



STOCKADE FENCE TRASH DUMPSTER ENCLOSURE
NOT TO SCALE



WALL CROSS SECTION
NOT TO SCALE



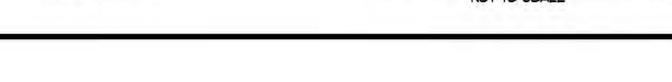
RIP RAP SWALE
NOT TO SCALE

GENERAL CONSTRUCTION NOTES

- 1) THE CONTRACT WORK TO BE PERFORMED ON THIS PROJECT CONSISTS OF FURNISHING ALL REQUIRED LABOR, MATERIALS, EQUIPMENT, PARTS AND SUPPLIES NECESSARY FOR OR APPURTENANT TO THE INSTALLATION OF CONSTRUCTION IMPROVEMENTS IN ACCORDANCE WITH THESE DRAWINGS AND AS FURTHER ELABORATED IN ANY ACCOMPANYING SPECIFICATIONS.
- 2) THE WORK SHALL BE PERFORMED IN A THOROUGH WORKMANLIKE MANNER ALL CONTRACTORS TO CONFORM TO ALL APPLICABLE OR ANY SPECIFICATION OR DESIGNATION OF THE AMERICAN SOCIETY FOR TESTING MATERIALS, FEDERAL SPECIFICATIONS, OR OTHER STANDARDS, CODES OR ORDERS, REFERS TO THE MOST RECENT OR LATEST SPECIFICATION OR DESIGNATION.
- 3) ALL CONSTRUCTION WITHIN THE CITY OF LEWISTON AND/OR STATE OF MAINE RIGHT OF WAY SHALL COMPLY WITH CITY PUBLIC WORKS AND/OR MDOT STANDARDS.
- 4) THE OWNER IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS REQUIRED BY THE CITY OF LEWISTON PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS FROM THE CITY OF LEWISTON REQUIRED TO PERFORM ALL THE WORK (STREET OPENINGS, BUILDING PERMIT, ETC.). THE CONTRACTOR SHALL POST ALL BONDS AS REQUIRED, PAY ALL FEES, PROVIDE PROOF OF INSURANCE AND PROVIDE TRAFFIC CONTROL, NECESSARY FOR THIS WORK.
- 5) PRIOR TO CONSTRUCTION, THE SITE CONTRACTOR IS TO INFORM ALL AREA UTILITY COMPANIES AND GOVERNMENTAL AGENCIES OF PLANNED CONSTRUCTION. THE CONTRACTOR IS REQUIRED TO CONTACT DIG-SAFE (1-800-225-4371) AT LEAST 3 BUSINESS DAYS PRIOR TO ANY EXCAVATION TO VERIFY ALL UNDERGROUND AND OVERHEAD UTILITY LOCATIONS.
- 6) THE PROJECT DRAWINGS ARE GENERALLY SCHEMATIC AND INDICATE THE POSSIBLE LOCATION OF EXISTING UNDERGROUND UTILITIES. INFORMATION ON EXISTING UTILITIES HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING MUNICIPAL RECORD MAPS, AND FIELD SURVEY. IT IS NOT GUARANTEED TO BE CORRECT OR COMPLETE. UTILITIES ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF THE EXISTING UTILITIES AS APPROPRIATE. THE CONTRACTOR IS TO PROVIDE ADEQUATE MEANS OF SUPPORT AND PROTECTION DURING THE EXCAVATING AND BACKFILLING OPERATIONS. SHOULD ANY UNCHARTED OR INCORRECTLY CHARTED UTILITIES BE FOUND, THE CONTRACTOR SHALL CONTACT THE DESIGN ENGINEER IMMEDIATELY FOR DIRECTIONS BEFORE PROCEEDING FURTHER WITH THE WORK IN THIS AREA.
- 7) OSHA REGULATIONS MAKE IT UNLAWFUL TO OPERATE CRANES, BOOMS, HOISTS, ETC. WITHIN TEN FEET (10') OF ANY ELECTRIC LINE. IF THE CONTRACTOR MUST OPERATE CLOSER THAN 10', THE CONTRACTOR MUST CONTACT THE POWER COMPANY TO MAKE ARRANGEMENTS FOR PROPER SAFEGUARDS BEFORE ENCRDACHING ON THIS REQUIREMENT.
- 8) IT IS THE CONTRACTOR'S RESPONSIBILITY TO EXAMINE ALL PLANS, APPROVALS, AND DETAILS FOR ADDITIONAL INFORMATION. THE CONTRACTOR SHALL VERIFY ALL THE SITE CONDITIONS IN THE FIELD AND CONTACT THE DESIGN ENGINEER IF THERE ARE ANY DISCREPANCIES REGARDING THE CONSTRUCTION DOCUMENTS AND/OR FIELD CONDITIONS SO THAT AN APPROPRIATE REVISION CAN BE MADE PRIOR TO BIDDING.
- 9) ALTERNATIVE METHODS AND PRODUCTS OTHER THAN THOSE SPECIFIED MAY BE USED IF REVIEWED AND APPROVED IN WRITING BY THE OWNER, DESIGN ENGINEER, AND APPROPRIATE GOVERNMENTAL AGENCY PRIOR TO INSTALLATION.
- 10) ALL EXCAVATION SHALL BE BACKFILLED TO EXISTING GRADE BEFORE THE END OF THE DAY OR ADEQUATELY PROTECTED FROM DANGER TO HUMANS AND ANIMALS.
- 11) THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ALL FIELD LAYOUT. THE OWNER WILL PROVIDE A BENCH MARK AT THE CONSTRUCTION SITE FROM WHICH TO BEGIN LAYOUT.
- 12) THE CONTRACTOR SHALL FURNISH ELECTRICAL, POWER, WATER, AND SANITARY FACILITIES FOR HIS EXCLUSIVE USE AT THE CONSTRUCTION SITE SHOULD THE CONTRACTOR DEEM THIS ESSENTIAL FOR THE PROPER PERFORMANCE OF THE CONTRACT.
- 13) WORK MAY PROCEED MONDAY THROUGH FRIDAY 7:00 AM TO 1:00 PM. WORK AT OTHER TIMES MAY PROCEED UPON WRITTEN APPROVAL BY THE OWNER AND THE CITY OF LEWISTON.
- 14) THE CONTRACTOR SHALL GUARANTEE THE FAITHFUL REMEDY OF ANY DEFECTS DUE TO FAULTY MATERIALS OR WORKMANSHIP AND GUARANTEES PATENT FOR ANY RESULTING DAMAGE WHICH SHALL APPEAR WITHIN A PERIOD OF ONE (1) YEAR FROM THE DATE OF SUBSTANTIAL COMPLETION OF THE PROJECT.
- 15) THE CONTRACTOR SHALL PROVIDE AS-BUILT RECORDS OF ALL CONSTRUCTION (INCLUDING UNDERGROUND UTILITIES) TO THE OWNER AT THE END OF CONSTRUCTION.
- 16) PROPER IMPLEMENTATION AND MAINTENANCE OF EROSION CONTROL MEASURES ARE OF PARAMOUNT IMPORTANCE FOR THIS PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH ALL EROSION CONTROL MEASURES SHOWN ON THE PLANS. ADDITIONAL EROSION CONTROL MEASURES SHALL BE INSTALLED IF DETERMINED NECESSARY BY ON-SITE INSPECTIONS OF THE OWNER, THEIR REPRESENTATIVES, OR STATE/LOCAL/FEDERAL INSPECTORS AT NO ADDITIONAL COST TO THE OWNER.

EROSION & SEDIMENTATION CONTROL NOTES

- 1) THE CONTRACTOR SHALL INSPECT EROSION & SEDIMENT CONTROL MEASURES WEEKLY AND AFTER HEAVY RAINFALLS THROUGHOUT THE DURATION OF THE PROJECT INCLUDING WEEKENDS AND HOLIDAYS. ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED.
- 2) THE CONTRACTOR IS RESPONSIBLE FOR CONSTRUCTING THE EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH THE MAINE HANDBOOK FOR CONSTRUCTION BEST MANAGEMENT PRACTICES, DEPARTMENT OF ENVIRONMENTAL PROTECTION, DATED MARCH 2003 (DEPLW 808). ADDITIONAL MEASURES SHALL BE INSTALLED IF DETERMINED NECESSARY DURING ON-SITE INSPECTIONS BY THE OWNER, THEIR REPRESENTATIVES OR STATE/LOCAL/FEDERAL INSPECTORS AT NO ADDITIONAL COST TO THE OWNER.
- 3) AREAS DISTURBED DURING CONSTRUCTION SHALL BE MINIMIZED. AREAS SHALL BE TEMPORARILY STABILIZED WITH MULCH OR NON-ERODIBLE COVER IF EXPOSED SOILS WILL NOT BE WORKED FOR MORE THAN 1 DAY. STABILIZE AREAS WITHIN 15' OF THE WETLAND AREA WITHIN 48 HOURS OF THE INITIAL DISTURBANCE OR PRIOR TO ANY STORM EVENT. PERMANENT SEEDINGS SHALL TAKE PLACE WITHIN 7 DAYS OF FINAL GRADING.
- 4) PRIOR TO CONSTRUCTION, PROPERLY INSTALL SEDIMENT BARRIERS AT THE DOWN GRADIENT EDGE OF THE DISTURBED AREA AND ADJACENT TO DRAINAGE CHANNELS WITHIN THIS AREA.
- 5) SILT FENCE LOCATIONS SHOWN ARE APPROXIMATE. INSTALL WHERE APPROPRIATE TO CONTROL SEDIMENTATION ON AND OFF SITE. SILT FENCE SHALL BE REMOVED AFTER THE SITE IS STABILIZED WITH AT LEAST 30% VEGETATED GROWTH.
- 6) NO SLOPES, EITHER PERMANENT OR TEMPORARY, SHALL BE STEEPER THAN TWO TO ONE (2 TO 1).
- 7) IF FINAL SEEDING OF THE DISTURBED AREAS IS NOT COMPLETED 45 DAYS PRIOR TO THE FIRST KILLING FROST, USE TEMPORARY MULCHING (DOMINANT SEEDING MAY BE ATTEMPTED AS WELL) TO PROTECT THE SITE AND DELAY SEEDING UNTIL THE NEXT RECOMMENDED SEEDING PERIOD.
- 8) TEMPORARY SEEDING OF DISTURBED AREAS THAT HAVE NOT BEEN FINAL GRADED SHALL BE COMPLETED BY AUGUST 15th OR 45 DAYS PRIOR TO THE FIRST KILLING FROST (OCT. 1) TO PROTECT FROM SPRING RUNOFF PROBLEMS.
- 9) REVEGETATION MEASURES WILL COMMENCE UPON COMPLETION OF CONSTRUCTION EXCEPT AS NOTED ABOVE.
- 10) ALL DISTURBED AREAS NOT OTHERWISE STABILIZED WILL BE GRADED, SMOOTHED AND PREPARED FOR FINAL SEEDING AS FOLLOWS:
 - A) 4" OF LOAM WILL BE SPREAD OVER DISTURBED AREAS AND SMOOTHED TO A UNIFORM SURFACE.
 - B) APPLY LIME/STONE AND FERTILIZER ACCORDING TO SOIL TEST. IF SOIL TESTING IS NOT FEASIBLE ON SMALL OR VARIABLE SITES, OR WHERE TESTING IS CRITICAL, FERTILIZER MAY BE APPLIED AT THE RATE OF 200 POUNDS PER ACRE OR 20 POUNDS PER SQUARE FEET USING 10-10-2 (N-P2O5-K2O) OR EQUIVALENT. APPLY GROUND LIMESTONE (EQUIVALENT TO 30% CALCIUM PLUS MAGNESIUM OXIDE) AT A RATE OF 53 TONS PER ACRE (150 LBS. PER 1000 S.F.).
 - C) FOLLOWING SEED BED PREPARATION, DITCHES AND BARK SLOPES WILL BE SEED TO A MIXTURE OF 4% CREEPING RED FESCUE, 8% REDTOP AND 48% TALL FESCUE. THE LAWN AREAS WILL BE SEED TO A PREMIUM TURF MIXTURE OF 44% KENTUCKY BLUEGRASS, 44% CREEPING RED FESCUE AND 12% PERENNIAL RYEGRASS. SEEDING RATE IS ONE POUND PER 1000 S.F. LAWN QUALITY SOO MAY BE SUBSTITUTE FOR SEED. SEED MIX SHALL CONTAIN 1% ANTI-CORROSION FERTILIZER.
 - D) HAY MULCH AT THE RATE OF 10-30 LBS PER 1000 S.F. OR A HYDRO-APPLICATION OF ASPHALT, WOOD OR PAPER FIBER SHALL BE APPLIED FOLLOWING SEEDING. A SUITABLE BINDER SUCH AS CURASOL OR RFB PLUS WILL BE USED ON HAY MULCH FOR WIND CONTROL.



CONCRETE PAD DETAIL
NOT TO SCALE

REVISED: APRIL 19, 2013 - ADD BOLLARD DETAIL
SHEET TITLE: **DETAIL SHEET**

PROJECT: **DUNKIN' DONUTS** 141-143 SABATTUS STREET - LEWISTON, ME

CLIENT: **KP REALTY, LLC** 1385 MAINE STREET #1 - FOLAND, MAINE 04274

SCALE: AS NOTED
DATE: MARCH 28, 2013
DRAWN BY: KRF
CHECKED BY: WMP

Stoneybrook Consultants, Inc.
456 Buckfield Road - Turner, Maine 04262
(207) 514-7491 Voice / (207) 514-7492 Fax

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6400 MAIN ST.
LEWISTON, ME 04240

SUMMIT
GEOENGINEERING SERVICES

WILLIAM M. PETERLIN
LICENSED PROFESSIONAL ENGINEER
5787

JOB NO. - 13043
SHEET 3

Section 5

Response



Stoneybrook Consultants, Inc.

456 Buckfield Road
Turner, Maine 04282
(207) 514-7491 voice
(207) 514-7492 fax

May 3, 2013

David Hediger, City Planner
Department of Planning & Code Enforcement
City of Lewiston
27 Pine Street
Lewiston, ME 04240-7201

Re: Dunkin' Donuts Shop
741 - 743 Sabattus Street

Dear David:

On behalf of K.P. Realty, LLC, we are pleased to submit this response to comments for the Dunkin' Donuts shop to be located on two lots located at 741 - 743 Sabattus Street. The comments have been repeated below followed by our responses.

Planning Staff comments:

- 1. A retaining wall is proposed on site. Please confirm height. Must be designed by a PE if it exceeds 6' in height. Also confirm construction type (i.e. precast, pour concrete, etc.)*

Information has been added to the Grading and Utility Plan which shows that the wall will not exceed 4.5' in height. The wall will be a standard precast concrete block wall. The actual block style or color has not yet been determined.

- 2. Proposed sign location will require a waiver from DOT given the width and number of travel lanes on Sabattus Street. No sign permits will be issued for its proposed location until a waiver is received from DOT.*

We have made contact with DOT and the waiver request forms will be submitted within a few days.

- 3. Please clarify what the square footage of the proposed office space on the second floor shall be.*

1,505 square feet.

- 4. Article XII, Section 17 (d) references the parking requirements for a restaurant, drive-in, and office. Please reference these provisions and how they are applied to this project.*

Under the City code requirements, the office area would require 5 parking spaces (1/300 S.F.). The restaurant area would typically add 10 parking spaces (1/3 seats), however, under your code, a restaurant with a drive-thru, as proposed at this site, falls under the definition of a "Drive-in Restaurant". The Drive-in Restaurant standards call for 10 spaces plus 1 space for every 100 square feet or 33 parking spaces. Your code appears to require 38 parking spaces (33 + 5) where normal requirements would be 15 parking spaces (10 + 5). We believe that this site does not need 38 parking spaces, but that the lower requirement of 15 parking spaces would not be sufficient to serve this use. We would ask the Planning Board to accept 26 spaces as sufficient space to accommodate the normal parking demands for this project. When considering this request, we would hope that you also consider that this site also provides 16 spaces for vehicle stacking. This means that the site can accommodate a total of 42 vehicles on the property.

- 5. The eight parking spaces adjacent the building will be difficult to access during peak hours due to being blocked by drive-thru traffic. Please speak to stacking of vehicles on site and the availability of parking.*

I am fortunate to have had an opportunity to work on many Dunkin' projects. Several of those projects were to correct the site circulation at existing shops because it was not working. With a Dunkin', traffic circulation is the most important site consideration. Most customers make their purchase at the

drive-thru window. Those few customers that want to order in the building need parking spaces that they can access without interfering with the drive-thru traffic during peak AM hours. Width of parking stalls and drive lanes are important for these movements to work. You are correct that, at this location, the spaces adjacent to the building will occasionally get blocked during peak AM use. Walk in customers will typically avoid these spaces during those periods, but they will get used by some customers. This typically occurs for about 2-3 hours per day. During the rest of the day, these spaces will be very accessible and useful for the customers.

6. *Where appropriate, consideration should be given to reducing the amount of impervious area by narrowing parking spaces and aisle widths to the minimum requirements of Article XII, Section 17.*

Original layouts for this project showed substantial increases in impervious areas. As we worked through the design phase, we made every change with consideration of reducing impervious areas to the point the project is now about equal with the current conditions. We are now showing all parking spaces at the minimum size required under Section 17 because of staff comments. That change, however, has not reduced impervious area, but has resulted in one additional parking space. My preferred aisle width for a Dunkin' project, given the circulation noted above, is to have a 20' aisle with a 12' wide drive-thru lane around the building. At this site, we have shown a 12' wide drive-thru lane but have reduced the aisle to 18' wide in the parking areas and to a varying width of 12' to 15' (the minimum required for truck movements) in other areas. We have made every effort to minimize impervious area for stormwater control reasons on this project.

7. *Article XIII, Section 4(b) requires that the planning board shall determine that any traffic increase attributable to the proposed development will not result in unreasonable congestions or unsafe conditions. In making this determination, the board shall consider the analysis and recommendation provided by the applicant, DOT and the City. While there has been a great deal of discussion regarding the traffic improvement needed to support this project, additional information is need, including but not limited to, a site plan showing all proposed on and off site improvements including signals, signs, striping,*

pedestrian crossings, etc.; a summary traffic analysis provided by the projects traffic engineer; a draft traffic movement permit. Reference should also be made the DOT's proposed improvements to the Sabattus Street corridor and how this project shall tie into said improvements. All of this information must be reviewed to the satisfaction of the City and MDOT prior to any permits being issued for construction activity. In addition, upon receiving approval from the Planning Board, staff is recommending as a condition of approval that all offsite improvements must be completed in accordance with the approved TMP and said plans prior to a certificate of occupancy being issued for construction activity. Staff is also recommending that the applicant traffic engineer provided a stamped statement verifying that all on and off site traffic related improvements have been completed in accordance with the approved plans and TMP. If the applicant believes the site will be completed prior to DOT's corridor improvements, then a condition of approval should be drafted for staff and the Board's consideration as to how the site may operate with traffic improvements remaining to be completed.

From our first meeting with City Staff at the site in September of last year, we have understood that traffic issues were key to the approval of this project. We had two meetings in November of last year with staff to discuss traffic options and how we could work together to make this site work. In December, we prepared a Traffic Assessment report and held a Traffic Movement pre-application meeting with City staff, your peer review consultant, HNTB, ATRC staff and MDOT. At that time, it was suggested that we should pursue installation of a traffic signal at the Sabattus Street/Wildwood Drive/Stanley Street intersections. That signal would need to be tied to an MDOT project for traffic signal modifications for eight intersections along Sabattus Street. This MDOT project was being designed by Vanasse Hangen Brustlin, Inc. (VHB) to coordinate traffic flow at eight existing intersections along the Sabattus Street corridor. We prepared a Traffic Signal Warrant Assessment report, which led to a follow up meeting with the same group of officials in March. In March, we were told that a signal could be installed at this location and that VHB had determined on a very preliminary basis that the signal could work with their corridor signalization project. We were also told that we would need to provide the information to support this finding and provide standard engineering plans for these improvements. We then prepared

a Traffic Scoping Application and held the Traffic Scoping meeting with all parties interested on April 9th (copies of all of these reports are included in Section 6). Since that time, we have completed field surveys for the offsite improvements and last week obtained important design information from VHB for their signal coordination project. We are working on the preliminary intersection design and a draft traffic movement permit which will be submitted under separate cover in a few days. All of this data will also be summarized in the final Traffic Study. We also plan to review all of this information again with Steve Landry of MDOT on May 14th.

While you review this data and work to issue a Traffic Movement Permit, we will complete the final intersection design and coordinate the work with the MDOT, VHB and their project contractor while seeking final approval of the intersection design. It is hoped that we will be able to work with the MDOT project contractor to install the signals before the MDOT project is completed this August or September.

This has been a long process, but every step of the way we have tried to make sure the design will work and meet final approvals as we do with every project. The details will take time, but everything we have and all of the reports or studies completed to this point indicate that we will receive final approvals from the City and MDOT to get this project done. We will continue to add information for your review as soon as we have it available and we look forward to September when the new store opens.

8. *The application notes a Permit By Rule from DEP will be required for proposed soil disturbance within 75' of a stream. No building permits or site activity may occur until this activity is approved by DEP.*

A copy of the permit by rule is included in Section 7 of this application. This information has been sent to DEP and will be valid within 14 days.

9. *Given the sites proximity to a natural resource, the amount of fill proposed for the site, and grading of the site toward the natural resource, staff is recommending that upon construction activity commencing, weekly erosion*

control reports are provided to city on the status of the project and any issues or corrections needed or made with respect to erosion control.

No response required.

- 10. The applicant must provide evidence to demonstrate financial capacity to complete the project.*

We have included a letter from TD Bank in Section 8 indicating that the applicant has demonstrated the capacity to obtain financing for this project.

- 11. Staff asks that the sidewalk accessing the site from Sabattus Street be possibly relocated to align with proposed crosswalks on Sabattus Street.*

We have considered this change, but have decided to leave the sidewalk in the location we have proposed. A sidewalk serving our project from Sabattus Street will have nothing to do with the crosswalk crossing Sabattus Street at the traffic signal proposed for this project. Pedestrians will likely be coming from both directions along Sabattus Street. We have selected our walk location based upon the location of the drive-thru order board. We want the crossing of the drive-thru lane behind the vehicle at the order board, not in front of that vehicle. We believe this will be the safest location onsite for the walkway. This location also reduces the impervious area needed for construction of a walk connection to the street.

- 12. You have indicated that the amount of impervious area on site shall remain the same. However, based upon the existing and proposed grading, it appears more of the site shall drain into an existing catch basin and city stormwater line on site. This may be allowed upon Public Works approval. Specifically, Article XIII, Section 4(f)(1)(b) allows for a connection to a city storm water system if the applicant has adequately demonstrated that it has the capacity to accommodate increases in flow. A slight increase may be allowed if it is determined that the increase cannot be avoided and does not significantly impact abutting properties or city property. More information is necessary for the city to make a determination on the proposed stormwater improvements.*

As noted, we have made every effort to minimize stormwater impacts in the design of this project. In Section 9, we have provided pre- and post-condition watershed maps and a chart showing the changes in these conditions. As you review the chart, you will see that changes from existing conditions to proposed conditions are very minor. We do not believe that stormwater calculations for review of this project will be useful or necessary given the proposed design. We hope that this information will be helpful and resolve these questions.

- 13. There appears to be a grading typo behind the dumpster with the 258' elevation.*

The contours in this area have been adjusted on the revised plans.

- 14. Detail on the dumpster screen should be provided.*

A dumpster enclosure detail has been provided.

- 15. Sheet 1 legend must be revised with respect to proposed landscaping. Please confirm plantings must be 2.5" caliper upon planting.*

The legend has been revised to match the graphics and trees proposed for this project.

- 16. Details need on proposed lighting. Lighting must be cut off fixtures. Pole lighting height should not exceed 20'.*

All exterior lighting proposed for this project will be full cut off fixtures and pole heights will not exceed 20'.

Engineering Staff Comments:

Application

- 1. The application letter states that the percentage of impervious areas are remaining the same and no stormwater treatment is required. The proposed work changes the layout of the site redirecting water to the existing*

stormwater pond located on the adjacent parcel, therefore stormwater calculations will be required to show that the additional flow to the stormwater pond will not cause adverse impacts to the downstream properties.

The project plans have been revised to keep the stormwater away from the abutter's stormwater pond. We have also shown a decrease in total green space area from our site draining towards this pond. Stormwater calculations should not be required with the revised site design.

- 2. A cost estimate for the onsite and offsite work was not included with this application. Both cost estimates and Financial Capacity will need to be submitted.*

The prior cover letter indicated that the cost of onsite improvements will be \$550,000. Offsite improvements are still being designed, but are expected to add \$150,000 to \$175,000 to these costs. The applicant has provided a letter from TD Bank as noted above to demonstrate financial capacity.

- 3. A Traffic impact study and Signal plan were not included in the application and will need to be submitted for review. As previously stated at meets between the applicant and staff he signal plan shall include, video detection for all approaches, preemption, APS Equipment, connection to the MaineDOT Coordinated system, and rehabbing of all impacted pedestrian ramps. Additional comments regarding the offsite improvements and signal plan will be provided after a plan has been submitted for review.*

We are aware of the traffic requirements for this project and are working with DOT officials to prepare that design and coordinate those improvements with the Signal Project on Sabattus Street. More details on that work will be provided under separate cover before the Planning Board meeting. It is expected that that this work effort will have full engineering design review after the Planning Board approval and issuance of the traffic movement permit, but before construction of those offsite improvements begin as is typical with all Traffic Movement Permits.

Plans

1. *The applicant should consider relocating the walkway from Sabattus Street to the site to be on the opposite side of the order board so that the walkway can be aligned with the crosswalk crossing Sabattus Street.*

See comments in item #11 above.

2. *The applicant should consider reducing the parking spaces to be the minimum City dimensions to further reduce impervious area and run off from the site. The applicant may be able to add additional parking to meet the parking demand by reducing the stall width.*

All parking spaces shown on the revised plans have been reduced to the minimum allowed by City code. That change has resulted in an increase of 1 parking space with no change in impervious area.

3. *The site grading will add approximately 6,500 square feet of area to the stormwater pond on the adjacent parcel, the applicant shall provide calculations that show that this will not cause adverse impacts to the downstream properties.*

The site design has been changed to reduce the area draining towards the abutter. With a reduction in area draining towards the abutter, stormwater calculations should not be necessary.

4. *The proposed drainage plan will provide a new manhole to be constructed on the existing 18" stormdrain, the applicant shall provide calculations showing that the 18" stormdrain can handle this increase in flow.*

We have provided additional information with pre- and post-watershed maps. That information shows that we have a reduction in impervious area flowing towards this stormdrain, but an increase in green space. We have also increased the length of the stormwater flow path which will slow the rate of flow. We hope that this additional information will be considered acceptable and will eliminate the need for stormwater calculations.

5. Level lip spreaders or other means of dissipation should be added to the end of the rip rap downspouts to prevent erosion adjacent to the stream due to the concentration of flows and increase in slopes as a result of this project.

A level spreader has been provided at this location.

6. Please provide grading information for the proposed wall and the curb along the top of the wall.

Grades have been provided.

7. The plans include a detail for a Precast Concrete Catchbasin, however, none are proposed, if a catchbasin is installed a catchbasin hood such as a Snout will be required instead of the 90° elbow.

We have removed the catch basin detail and a field inlet detail has been added.

8. A detail for a manhole should be added to the detail sheet. The applicant should consider utilizing a doghouse structure and maintaining the bottom of the existing pipe as the channel and construct the shelf around the pipe instead of trying to make connections using short sections of pipe that could leak over time.

A detail has been provided.

9. A detail for the retaining wall including type of wall and reinforcement if necessary should be added to the detail sheet.

A detail has been provided.

10. A detail for a rip-rap downspout including non-woven geotextile and stone sizing should be added to the detail sheet.

A detail has been provided.

May 3, 2013
David Hediger
RE: 741-743 Sabattus Street
Page 11

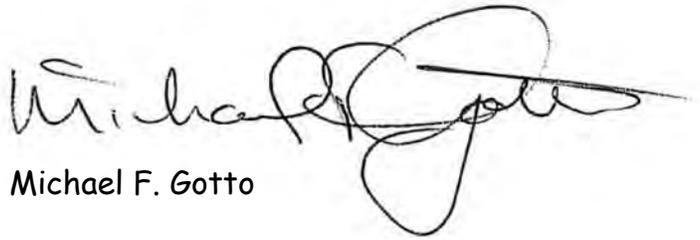
11. Erosion & Sedimentation Control Note #1 makes reference to a Stormwater Permit, the application states that a Stormwater Permit is not required, the note should be revised to indicate the appropriate measures necessary since a permit will not be provided to the contractor.

The note has been revised.

We hope you find this application complete and we will plan to attend the Planning Board meeting to answer any questions the Planning Board or staff may have.

Respectfully Yours

STONEBROOK CONSULTANTS, INC.

A handwritten signature in black ink, appearing to read "Michael F. Gotto". The signature is stylized with a large, looping flourish at the end.

Michael F. Gotto

cc: Kevin Pacheco

Section 6

Traffic

Traffic Assessment

William J. Bray, P.E.
235 Bancroft Street
Portland, Maine 04102
Phone (207) 774-3603
trafficsolutions@maine.rr.com

December 7, 2012

Traffic Assessment Proposed Dunkin Donuts Kiosk - Sabattus Street

INTRODUCTION

Kevin Pacheco is proposing to construct a Dunkin Donuts “kiosk” on a corner parcel of property located at the intersection of Sabattus Street (State Route 126) and Wildwood Drive. Primary access will be provided through a full-service driveway that intersects Wildwood Drive approximately 160 feet from the street line of Sabattus Street. The proposed Dunkin Donuts “kiosk” facility will only provide drive-through service.

This document presents existing peak hour traffic flows for the Sabattus Street (Route 126) corridor; estimates the peak hour trip generation of the proposed project during a typical weekday AM peak hour, and develops a traffic assignment of those trips to the area roadway system.

EXISTING CORRIDOR PEAK HOUR TRAFFIC

A manual turning movement count was performed at the intersection of Sabattus Street, Wildwood Drive, and Stanley Street on October 4, 2012 (A copy of the count summary sheet is attached as an appendix to the report). All traffic entering each approach of the intersection was recorded in 15-minute intervals between the hours of 6:30 and 8:30 AM. It was determined from a summary of the data that the peak hour of traffic for the intersection occurs between 7:15 and 8:15AM when a total of 1,606 vehicles were recorded entering the intersection. The peak hour data for the intersection was then adjusted to reflect “peak” annual travel conditions for the corridor. A seasonal adjustment of 1.06 was applied to the October data to reflect peak summer travel conditions at the intersection. The predominate directional flow was recorded on the westbound approach of Sabattus Street with a total of 1,161 vehicles in the peak hour; traffic traveling east on Sabattus Street was considerably less at 464 vehicles. The volume of traffic on Wildwood Drive was very small with a peak hour total of less than 20 vehicles. Figure 1 depicts the estimated peak hour traffic for the study intersection.

SITE TRIP GENERATION

The proposed Dunkin Donuts site, which will only serve drive-through customers, can be expected to generate significantly less vehicle trips than a full-service store. In an attempt to accurately estimate the peak hour trip generation of the project, existing weekday vehicle counts were conducted at two existing drive-through only Dunkin Donuts sites: 1) 828 Lisbon Street, Lewiston and 2) 67 Sewall Street, Augusta (*This approach for determining trip generation for*

a “kiosk” facility was discussed and approved by MaineDOT’s Assistant Traffic Engineer). Manual traffic counts were performed at each existing facility, with all vehicles entering and exiting the site being recorded in 15-minute intervals. Vehicle trips were recorded between 6:30 and 9:30 AM at the Lewiston site on September 20 and between the hours of 6:15 and 8:45 AM on October 2 at the Augusta site (Copies of the data summary sheets are attached). A summation of the data from both sites is presented in the following table:

Dunkin Donuts “kiosk” Vehicle Trips

<u>Time Period</u>	<u>828 Lisbon Street</u>			<u>67 Sewall Street</u>		
	<u>Entering Trip</u>	<u>Exiting Trip</u>	<u>Total Trips</u>	<u>Entering Trip</u>	<u>Exiting Trip</u>	<u>Total Trips</u>
6:15 - 6:30				14	13	27
6:30 - 6:45	7	7	14	24	18	32
6:45 - 7:00	9	6	15	19	25	41
7:00 - 7:15	13	14	27	21	19	40
7:15 - 7:30	16	15	31	22	21	43
7:30 - 7:45	16	16	32	28	20	48
7:45 - 8:00	18	16	34	26	27	53
8:00 - 8:15	16	17	33	19	22	41
8:15 - 8:30	11	11	22	22	24	44
8:30 - 8:45	20	12	32	16	19	35
8:45 - 9:00	12	12	24			
9:00 - 9:15	10	15	25			
9:15 - 9:30	6	9	15			
Peak Hour Trips	66	64	130	95	93	188

As noted in the preceding table, the volume of peak hour trips generated by a Dunkin Donuts “kiosk” is considerably less than trip generation found at a full-service store, with recorded peak hour trip rates in excess of 300 vehicle trips per hour.

An hourly trip rate of 180 vehicle trips was selected as a somewhat conservative volume for the proposed Sabattus Street site, with 90 vehicle trips entering the site and 90 trips exiting.

SITE TRIP ASSIGNMENT

Site Trip Composition: On May 21, 2012, on behalf of the regional office of Dunkin Donuts, Gorrill Palmer, Inc. published a very comprehensive study of existing vehicle trends occurring at existing Dunkin Donuts sites within the State of Maine. That study concluded that “70% of Dunkin Donuts traffic is pass-by traffic and the remaining trips are either a diverted trip of a new or primary trip to the roadway system”. This report has assumed that similar trip composition will occur at the proposed Sabattus Street site. Accordingly, approximately 126 of the projected 180 site trips will be “pass-by” vehicle trips already traveling along the Route 126 corridor and the remaining trips (54 trips) will be new or primary trips to the corridor.

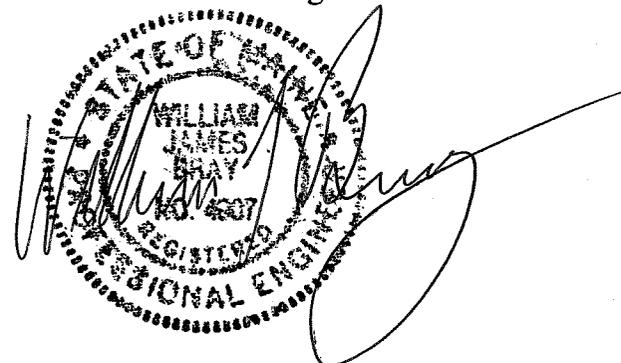
Site Trip Composition: Based upon information presented in the Gorrill-Palmer, Inc. study, approximately 50% of the site trips will enter and the remainder will exit the site in the AM peak hour. This trend is affirmed with the data collected at the existing Lewiston and Augusta sites.

Site Trip Assignment: Site trips generated by the proposed project were directionally assigned to the study intersection based upon existing vehicle splits recorded for the Route 126 corridor with a corrective adjustment applied to vehicle trips approaching the site from the east along Sabattus Street. An existing Dunkin Donuts site located on Route 126 in the neighboring Town of Sabattus (approximately 3.5 miles from the proposed Wildwood Drive site) will have a profound impact in reducing the volume of site trips with a travel desire from the east. A manual turning movement count was conducted at the existing Sabattus Dunkin Donuts site on November 2 (between the hours of 6:30 and 8:30 AM) to quantify the volume of site trips that enter the Dunkin Donuts site from the east on Route 126 (A copy of the count summary sheet is attached). Based upon a summary of the data, approximately 83% of the entering site trips (114 trips) enter the site from the east and a very small volume of trips (23 trips) from the west during the morning peak hour. During the same peak hour time period the Technician recorded the volume of through traffic passing the Sabattus site. As shown on the summary sheet, approximately 559 (seasonally adjusted) through vehicles were recorded traveling west on Route 126 towards the City of Lewiston. The Sabattus Street site is located on the north side of Route 126 enabling motorists traveling eastbound to enter the site via a right-turn movement. The report has assumed that the primary market area for the proposed Wildwood Drive Dunkin Donuts site will have an origin west of the site because a significant volume of motorists approaching from the east will continue patronizing the Sabattus Dunkin Donuts for two primary reasons:

- The Sabattus Store is located earlier in the trip path and,
- Entering the proposed Wildwood Drive site would require a left-turn across two lanes of traffic, whereas the Sabattus store allows for a much easier right-turn movement to the site.

The report has developed a traffic model that assumes the 559 vehicle trips recorded at the Sabattus Dunkin Donuts site continue traveling westerly on Route 126 and, ultimately, become a large percentage (approximately 50%) of the westbound through trips recorded on Sabattus Street at the Wildwood Drive intersection.

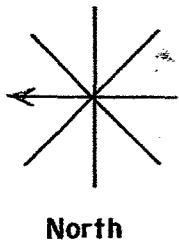
Accordingly, an adjustment is required to the vehicle splits found on Sabattus Street at the Wildwood Drive intersection before an assignment of site trips is developed. The westbound flow of traffic is reduced from the recorded volume of 1,161 vehicles by 559 vehicles to a revised total of 602 vehicles. The corrected vehicle split for through traffic on Sabattus Street at the proposed site shows 55% traveling westbound and 45% eastbound. Vehicle trips generated by the proposed “kiosk” site were assigned accordingly with a total 49 trips approaching the site from the east and 41 trips from the west. Departing site trips will follow a similar pattern with 43 trips traveling east and 47 trips west. Figure 2 illustratively presents the site traffic assignment.



VEHICLE VOLUME COUNT GRAPHIC SUMMARY SHEET

Intersection of SABATIUS ST. = WILDWOOD DR. Date _____

Weather _____ Road Surface Condition _____ Time _____ to _____



Name SABATIUS STREET

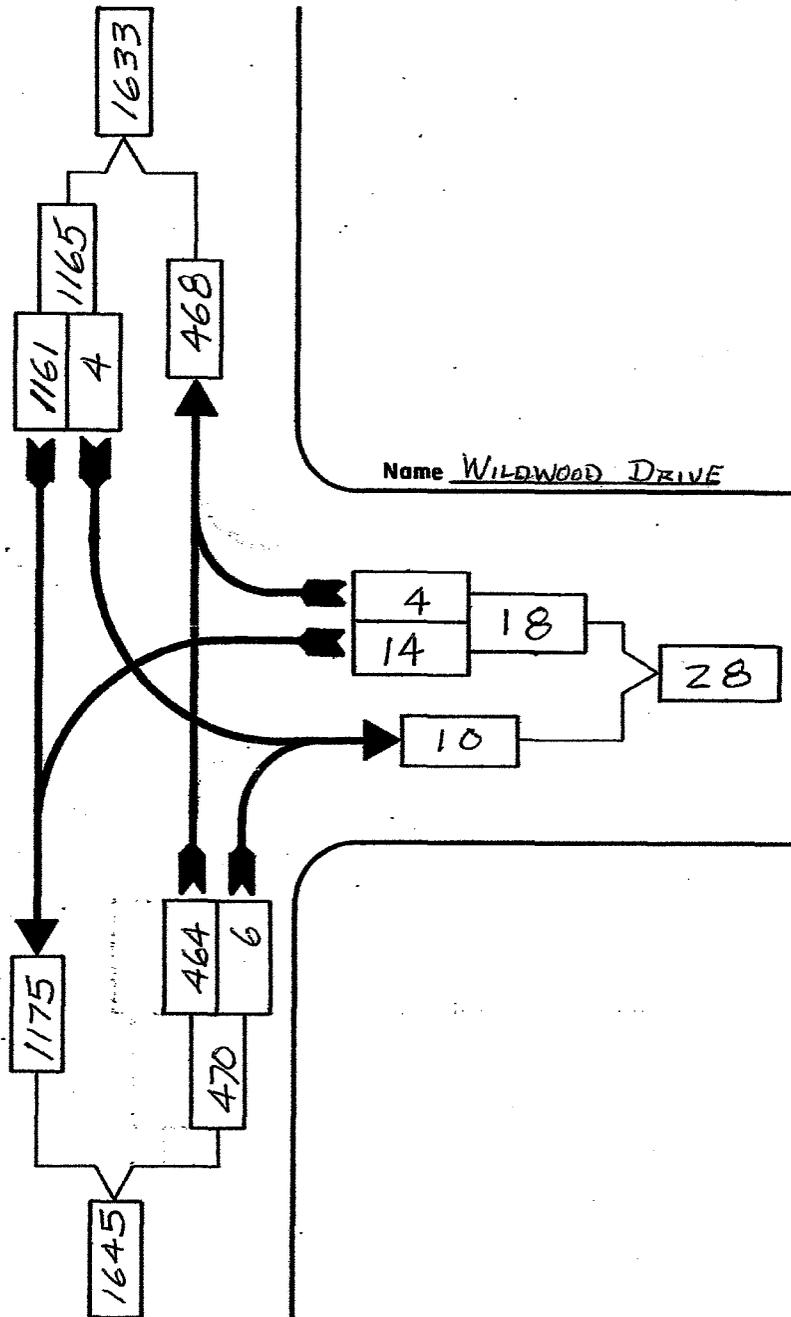


FIGURE 1: 2012 AM Peak Hour Traffic

VEHICLE VOLUME COUNT GRAPHIC SUMMARY SHEET

Intersection of SABOTTUS ST. & WILDWOOD DR. Date _____

Weather _____ Road Surface Condition _____ Time _____ to _____

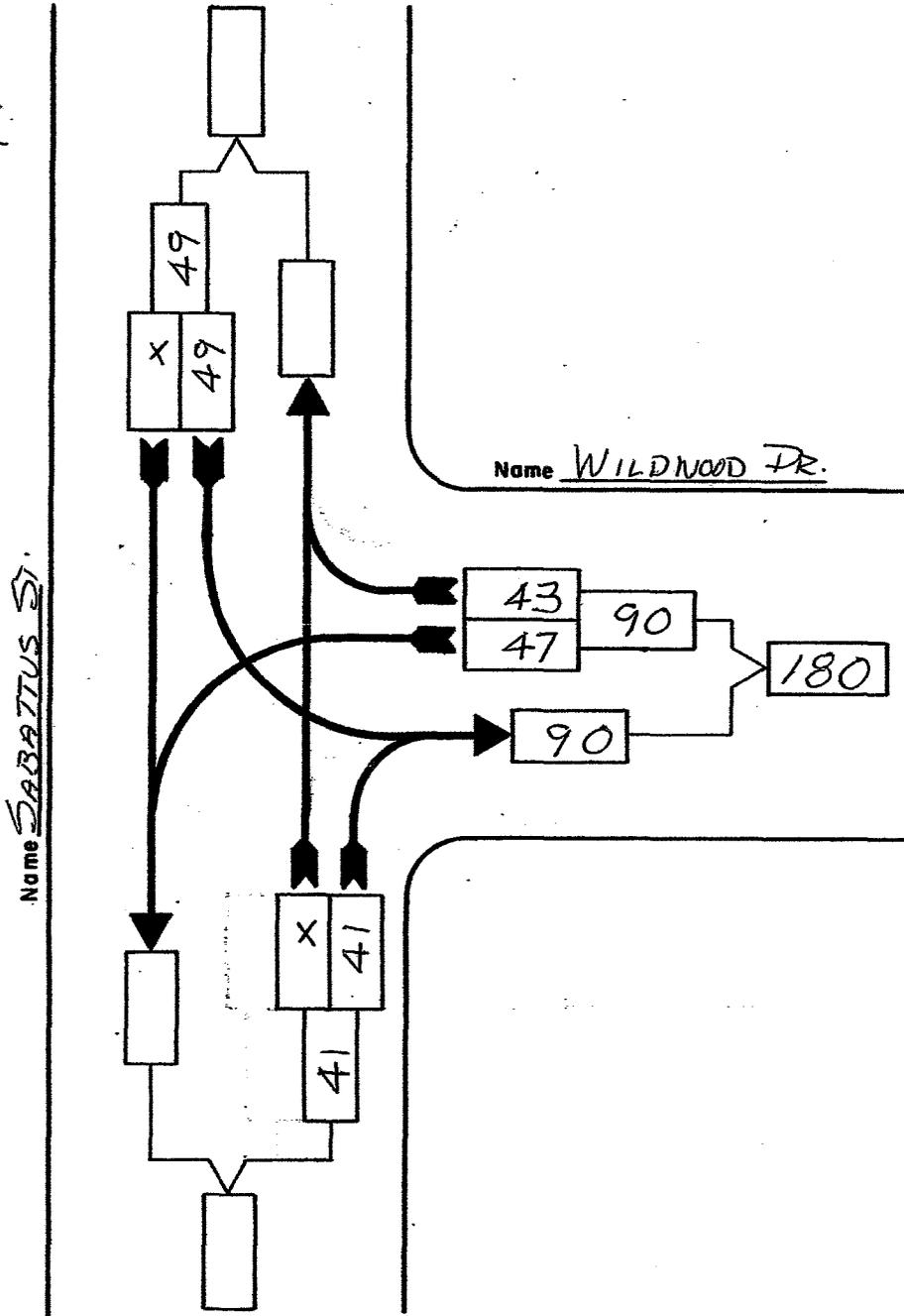
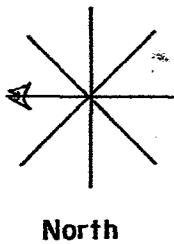


FIGURE 2: Site Traffic Assignment – AM Peak Hour

JOB NO. _____

**INTERSECTION PLAN
WITH NUMBERED MOVEMENTS:**

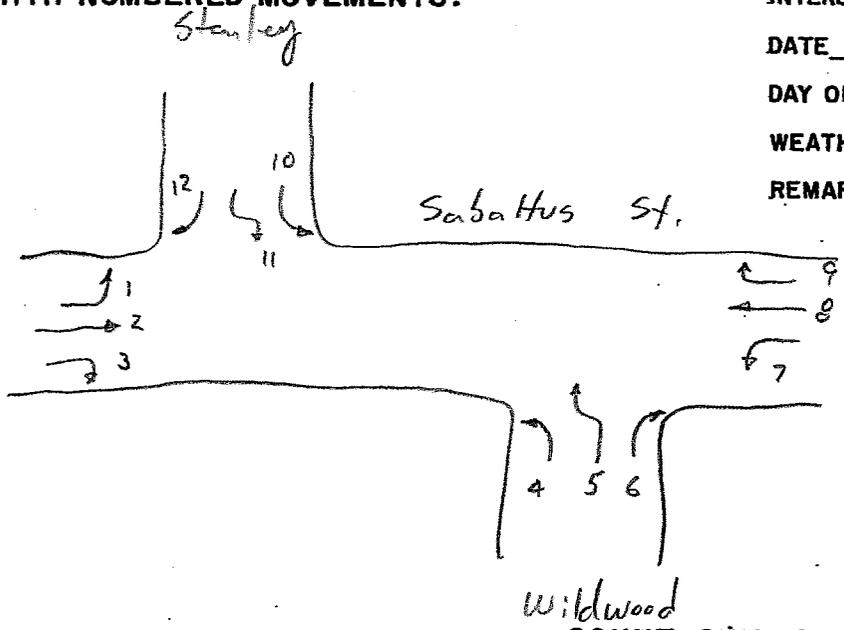
INTERSECTION Sabathus, Wildwood, Stanley

DATE 10-4-2012

DAY OF WEEK Thursday

WEATHER clear

REMARKS:



COUNT SUMMARY

	MOVEMENT												
	1	2	3	4	5	6	7	8	9	10	11	12	TOTAL
:00													
:15													
:30													
6:30													
6:45	5	64	0	1	0	1	0	166	8	1	0	1	247
6:45													
7:00	5	50	2	3	0	5	0	213	13	2	0	5	298
7:00													
7:15	6	86	0	5	0	3	1	186	22	5	0	1	315
7:15													
7:30	8	99	2	6	0	0	3	272	22	8	0	3	423
7:30													
7:45	8	97	0	2	0	2	0	311	17	7	0	7	451
7:45													
7:45	8	89	2	1	1	2	0	230	19	6	1	7	366
8:00													
8:00	5	128	2	2	1	0	1	201	18	4	0	4	366
8:15													
8:15	3	108	1	1	1	2	1	208	18	3	2	3	351
8:30													
:30													
:45													
:45													
:00													

PEAK HOUR COUNT

TIME: 7:15 TO: 8:15

29	413	6	11	2	4	4	1014	76	25	1	21	
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Conversion: 31 440 6 12 2 4 4 1090 81 27 1 23
 Annual Adj:

JOB NO. _____

**INTERSECTION PLAN
WITH NUMBERED MOVEMENTS:**

Sabbathus

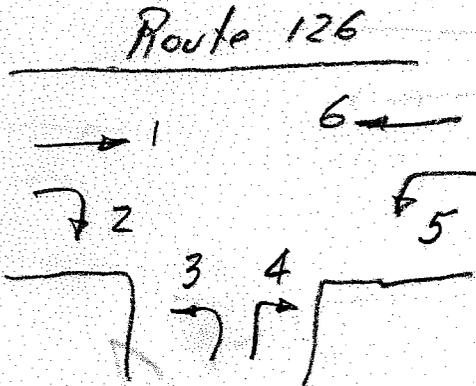
INTERSECTION ROUTE 126 = D:D

DATE 11/20/2012

DAY OF WEEK Tuesday

WEATHER Clear

REMARKS:



D:D

**COUNT SUMMARY
MOVEMENT**

	1	2	3	4	5	6	7	8	9	10	11	12	TOTAL
:00													
:15													
:30													
6:30													
6:45	114	24	13	17	6	41				60			
6:45													
7:00	90	20	14	9	2	32				45			
7:00													
7:15	91	32	15	16	5	58				68			
7:15													
7:30	126	32	13	21	5	49				71			
7:30													
7:45	158	27	15	16	5	41				63			
7:45													
8:00	129	23	16	17	8	64				64			
8:00													
8:15	80	20	16	8	5	46				49			
8:15													
8:30	85	19	12	15	9	38				55			
8:30													
8:45													
8:45													
9:00													

PEAK HOUR COUNT *D:D*

TIME: 7:00 TO: 8:00

504	114	59	70	23	212					266			
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Traffic Signal Warrant Assessment

William J. Bray, P.E.
 235 Bancroft Street
 Portland, Maine 04102
 Phone (207) 774-3603
trafficsolutions@maine.rr.com

February 8, 2013

Traffic Signal Warrant Assessment Route 126 (Sabattus Street) and Wildwood Drive Intersection

INTRODUCTION

KP Reality, LLC is proposing construction of a full service Dunkin’ Donuts store at the corner of Route 126 (Sabattus Street) and Wildwood Drive in the City of Lewiston. Access to the projected project is proposed exclusively through Wildwood Drive, a minor roadway intersecting Route 126 just east of Stanley Street. Most recently, the City and MaineDOT approved a Cumberland Farms store on the opposite corner of the intersection with a secondary access drive on Wildwood Drive. The City of Lewiston staff and traffic consultant, Executive Director of ATRC, and MaineDOT’s Region 3 Traffic Engineer in a preliminary review of the proposed Dunkin’ Donuts project have suggested full traffic signalization of the Route 126 (Sabattus Street) @ Wildwood Drive intersection and Stanley Street to create acceptable ingress/egress to both projects. The Stanley Street approach, a much lower volume minor street, operationally creates an “off-set” intersection; however, it’s inclusion in the assessment of signalization is essential because of its close proximity to the Wildwood Drive approach. The reviewing entities have required a detailed traffic signal warrant study for the noted intersection. The following assessment complies with that request.

EXISTING TRAFFIC CONDITIONS

ATRC at the request of KP Reality, LLC. conducted a 12-hour traffic volume count of the Route 126/Stanley Street intersection on Tuesday, January 15, 2013 between the hours of 6:00AM and 6:00PM. The recorded volume of hourly traffic was adjusted by a factor of 1.08, derived from MaineDOT’s weekly mean factors, to represent “average” travel conditions at the intersection. The adjusted traffic volumes are presented in the following Table 1:

**Table 1
Hourly Traffic Volumes
Route 126 (Sabattus Street) @ Stanley Street**

<u>Time</u>	<u>Stanley Left</u>	<u>Stanley Right</u>	<u>Rt. 126 WB Right</u>	<u>Rt. 126 WB Thru</u>	<u>Rt. 126 EB Left</u>	<u>Rt. 126 EB Thru</u>
6:00 - 7:00 AM	8	5	32	554	13	171
7:00 - 8:00	11	18	58	1100	22	368
8:00 - 9:00	8	8	43	793	26	396
9:00 - 10:00	12	6	22	649	22	408
10:00 - 11:00	6	7	23	613	20	497
11:00 - 12Noon	7	5	15	642	19	613
12Noon - 1:00 PM	14	12	21	1266	21	663
1:00 - 2:00	11	11	14	661	25	608
2:00 - 3:00	9	11	16	644	14	780
3:00 - 4:00	5	5	27	732	20	940
4:00 - 5:00	7	8	19	738	19	1073
5:00 - 6:00	5	5	20	588	17	942

PROPOSED DUNKIN' DONUTS HOULRY TRAFFIC

An estimate of hourly traffic that will patronize the proposed Dunkin' Donuts store was based upon an existing hourly sales activity report from a Dunkin' Donuts store located on Route 126 in the neighboring community of Sabattus. The sales activity report shows the number of sales per hour; conversion of this data to vehicle trips requires multiplication by two. Table 2 presents the volume of hourly trips generated by a similar sized Dunkin' Donuts store:

Table 2
Hourly Sales Activity
Route 126 Dunkin' Donuts – Town of Sabattus
(November 16, 2012)

<u>Time</u>	<u>Sales Count</u>	<u>Estimated Trips</u>
6:00 - 7:00 AM	105	210
7:00 - 8:00	175	350
8:00 - 9:00	127	254
9:00 - 10:00	80	160
10:00 - 11:00	59	118
11:00 - 12Noon	54	108
12Noon - 1:00 PM	41	82
1:00 - 2:00	38	76
2:00 - 3:00	51	102
3:00 - 4:00	45	90
4:00 - 5:00	43	86
5:00 - 6:00	28	56

PROPOSED CUMBERLAND FARMS STORE HOULRY TRAFFIC

Hourly traffic volumes were estimated for the proposed 4,513 square foot convenience store with 8 fueling positions with the conduct of a manual traffic count at the existing Cumberland Farms Store located on the opposite side of Sabattus Street, which will be razed with development of the proposed new store site. The hourly data collected at the existing store was compared to estimated peak hour trip generation prepared by Sebago Technics, Traffic Consultant for the Cumberland Farms project, and adjusted based on two separate factors. Each of the hourly traffic counts collected in the AM period was adjusted by a factor of 1.50 and the afternoon hour traffic data was adjusted by a factor of 1.83. Table 3, as follows, presents the adjusted hourly trip estimates for the proposed Cumberland Farms Store:

Table 2
Hourly Vehicle Trips Proposed
Cumberland Farms Store

<u>Time</u>	<u>Existing Cumberland Farms Traffic</u>	<u>Estimated Cumberland Farms Traffic⁽¹⁾</u>
6:00 - 7:00 AM	82	124
7:00 - 8:00	72	109
8:00 - 9:00	114	172
9:00 - 10:00	38	57
10:00 - 11:00	35	53
11:00 - 12Noon		
12Noon - 1:00 PM		
1:00 - 2:00		
2:00 - 3:00	88	161
3:00 - 4:00	99	181
4:00 - 5:00	114	209
5:00 - 6:00	117	214

- (1) Estimate of hourly trips were forecast by dividing AM peak traffic volume recorded at existing store by volume projected in Sebago Technics Traffic Movement Permit Application separately for the AM and PM peak hours. The AM peak hour volume recorded at the existing store was 114 trips. The estimated volume in Sebago Technics materials was 172 trips ($172 \div 114 = 1.50$). The PM peak hour for the existing volume was 117 with the corresponding Sebago Technics volume of 214 trips ($214 \div 117 = 1.83$).

TRAFFIC SIGNAL WARRANT ASSESSMENT

A detailed traffic signal warrant analysis, based upon forecast 2013 Post-Development travel conditions of both development projects (Dunkin' Donuts and Cumberland Farms) was completed for the Route 126 (Sabattus Street)/Wildwood Drive/Stanley Street intersection. The conduct of the traffic signal warrant assessment focused specifically on the hourly volume of traffic forecast for the Route 126/Wildwood Drive portion of the intersection, specifically; left-turn traffic entering the intersection from the Wildwood Drive approach, left-turn traffic from Route 126 onto Wildwood Drive, and through traffic projections along Route 126. Vehicle trips generated by the two proposed sites (Dunkin' Donuts and Cumberland Farms) were assigned directionally to the Route 126 corridor based upon current hourly splits recorded for Route 126. For the analysis, all site trips were assumed to be either "pass-by" or "diverted" trips with their directional origins/destinations directly influenced by the current vehicle splits found on Route 126. Finally, the volume of trips entering and exiting each proposed site was assumed to be equal for the hours studied.

The traffic signal warrant analysis follows the guidelines presented in the 2009 edition of the Manual on Uniform Traffic Control Devices (MUTCD) in Chapter 4C. The MUTCD provides nine separate traffic signal warrants, whereby, prevailing conditions at an intersection can be evaluated to determine if sequenced traffic signals are warranted. Each of the nine warrants is listed as follows:

- Warrant 1 – Eight Hour Vehicular Volume
- Warrant 2 – Four Hour Vehicular Volume
- Warrant 3 – Peak Hour
- Warrant 4 – Pedestrian Volume
- Warrant 5 – School Crossing
- Warrant 6 – Coordinated Signal System
- Warrant 7 – Crash Experience
- Warrant 8 – Roadway Network
- Warrant 9 – Intersection Near a Grade Crossing

The analysis was completed based upon the estimated 2013 Post-Development traffic values presented in the following Table 4:

Table 4
Estimated 2013 Post-Development Hourly Traffic
Route 126 @ Wildwood Drive

Time Period	Major Street Volume ⁽¹⁾			Directional Split	Cumberland Farms Traffic			Dunkin' Donuts Traffic			Total Left-Turn Entry	Total Left-Turn Exit
	Sabattus WB	Sabattus EB	Total		Total Trips ⁽²⁾	Left-Turn Entry ⁽³⁾	Left-Turn Exit ⁽³⁾	Total Trips ⁽⁴⁾	Left-Turn Entry ⁽³⁾	Left-Turn Exit ⁽³⁾		
6:00 - 7:00 AM	633	199	832	24% EB 76% WB	124	47	47	210	80	80	127	127
7:00 - 8:00	1,244	414	1,658	25% EB 75% WB	109	41	41	350	131	131	172	172
8:00 - 9:00	903	456	1,359	34% EB 66% WB	172	57	57	254	84	84	141	141
9:00 - 10:00	725	464	1,189	39% EB 61% WB	57	18	18	160	49	49	67	67
10:00 - 11:00	687	558	1,245	45% EB 55% WB	53	14	14	118	32	32	46	46
11:00 - 12 Noon	708	683	1,391	49% EB 51% WB				108	28	28		
12 Noon - 1:00 PM	697	739	1,436	51% EB 49% WB				82	20	20		
1:00 - 2:00	729	684	1,413	48% EB 52% WB				76	20	20		
2:00 - 3:00	713	858	1,571	55% EB 45% WB	161	36	36	102	23	23	87	87
3:00 - 4:00	820	1,037	1,857	56% EB 44% WB	181	40	40	90	20	20	85	85
4:00 - 5:00	818	1,179	1,997	59% EB 41% WB	209	42	42	86	18	18	86	86
5:00 - 6:00	657	1,031	1,688	61% EB 39% WB	214	42	42	56	11	11	70	70

Notes:

⁽¹⁾ Traffic Data obtained from January 15, 2013 count adjusted by 1.08 to equal "average" travel conditions.

⁽²⁾ Hourly Trips were based upon a traffic count conducted at existing Cumberland Farms on January 9, 2013, with data adjusted to size increase of new store. AM data adjusted by a factor of 1.50, PM data adjusted by a factor of 1.83.

⁽³⁾ Left-turn trips were calculated based upon directional distribution of through traffic on Sabattus Street (Values highlighted in blue).

⁽⁴⁾ Total Trips for Dunkin' Donuts reflect actual patron counts for November 16, 2013 at existing Dunkin' Donuts store on Route 126 in Town of Sabattus.

Each Warrant is briefly described below followed by a determination of whether forecast conditions meet or fail required conditions. Minor Street Approach assumes two separate conditions: Case I is the Wildwood Drive left-turn movement, and Case II applies to the Route 126 left-turn entry movement to Wildwood Drive.

Warrant 1A: Eight Hour Vehicular Volume - Condition A (Column “b”)

Requires 120 vehicles on minor street and 600 vehicles on major street with two travel lanes, both approaches (except when major street left-turn is minor approach, then only opposing approach volume of major street is applied) for 8 hours of an average day. Minor street volume, Case 1 or Case 2, met during three one-hour time periods only. Major street volume met during each hour time period. Warrant Not Satisfied

Warrant 1B: Eight Hour Vehicular Volume - Condition B

Requires 60 vehicles on minor street and 720 vehicles on major street, both approaches (except when major street left-turn is minor approach, then only opposing approach volume of major street is applied) for 8 hours of an average day. Minor street volume is met during 8 hours for both Case I and Case II minor street conditions. Major Street volume is met for all nine hours with Case I minor street requirements and for four hours under the Case II condition. Warrant Met Case I Condition

Warrant 2: Four Hour Vehicular Volume

If predicted travel conditions for any four hours of an average day representing the volume per hour on the major street and the corresponding vehicles per hour on the higher volume minor street approach all fall above the applicable curve in Figure 4C-1. The forecast hourly traffic volumes for the Case I Minor Street fall above the applicable curve during five of eight hours; in Case II the values fail during all hours to exceed the applicable curve. Warrant Met Under Case I Condition

Warrant 3: Peak Hour

If predicted travel conditions for one hour of an average day representing the volume per hour on the major street and the corresponding vehicles per hour on the higher volume minor street approach all fall above the applicable curve in Figure 4C-3. The forecast hourly traffic volumes for the Case I Minor Street fall above the applicable curve during one hour; in Case II the values fail during all hours to exceed the applicable curve. Warrant Met Under Case I Condition

Warrant 4: Pedestrian Volume - Warrant Not Applicable

Warrant 5: School Crossing - Warrant Not Applicable

Warrant 6: Coordinated Signal System - Warrant Not Applicable

Warrant 7: Crash Experience - Warrant not Considered

Warrant 8: Roadway Network - Warrant Not Applicable

Warrant 9: Intersection Near a Grade Crossing - Warrant Not Applicable

SUMMARY

The forecast 2013 Post-Development Traffic Conditions for the subject intersection were determined to fully meet Traffic Signal Warrants 1B, Warrant 2, and Warrant 3. If a portion of the right-turn traffic leaving the Wildwood Drive approach were considered in the analysis it is highly likely that additional time periods during the 12-hour study period would meet and exceed the threshold values for each respective time period. Favorable approval of the request for signalization of the Route 126 (Sabattus Street), Wildwood Drive, and Stanley Street intersection will greatly improve the ingress and egress traffic movements to both proposed development projects existing turning movements to/from Stanley Street. The Local and State reviewing personnel in attendance both suggested and encouraged signalization of the noted intersection as an appropriate means for citing the proposed Dunkin' Donuts store seamlessly into the existing roadway system. This report fully supports that recommendation and provides clear documentation that projecting traffic conditions can be expected to fully meet the required traffic signal warrants requirements.

**Traffic Permit Application
(partial)**

Provisionally Adopted 1/10/00

Department of Transportation
Traffic Engineering Division
16 State House Station
Augusta, Maine 04333
Telephone: 207-287-3775

FOR MDOT USE
ID # 1/2000

Total Fees:
Date Received:

**PERMIT APPLICATION – TRAFFIC
TRAFFIC MOVEMENT PERMIT, 23 M.R.S.A. § 704 – A**

Please type or print:

This application is for: Traffic 100-200 PCE's _____
Traffic 200 + PC-E's X

Name of Applicant: **KP Realty, LLC**

Address: **1385 Maine Street #1, Poland, Maine 04274** Telephone: **(207) 576-5359**

Name of local contact or agent: **Mike Gotto, Stoneybrook Consultants Inc. / William J. Bray, P.E., Traffic Solutions**

Address: **456 Buckfield Road, Turner, Maine 04282 / 235 Bancroft Street, Portland, Maine 04102**
Telephone: **(207) 514-7491/ 400-6890**

Name and type of development: **Dunkin' Donuts Restaurant**

Location of development including road, street, or nearest route number **743 Sabattus Street, Lewiston, Maine 04240**

City/Town/Plantation: **Lewiston** , County: **Androscoggin** , Tax 119__ Lot # **286&287**

Do you want a consolidated review with DEP pursuant to 23 M.R.S.A. § 704-A (7)? Yes ___ No **X**

Was this development started prior to obtaining a traffic permit? **No**

Is the project located in an area designated as a growth area (as defined in M.R.S.A. title 30 – A, chapter 187)?
Yes **X** No _____

Is this project located within a compact area of an urban compact municipality? Yes **X** No _____

Is this development or any portion of the site currently subject to state or municipal enforcement action? **No**

Existing DEP or MDOT permit number (if applicable): **N/A**

Name(s) of DOT staff person(s) contacted concerning this application: **Stephen Landry, P.E.**

Name(s) of DOT staff person(s) present at the scoping meeting for 200+ applications: _____

NOTICE OF INTENT TO FILE

Please take notice that:

**KP Realty, LLC
1385 Maine Street #1
Poland, Maine 04274**

Is intending to file a Traffic Movement Permit application with the Maine Department of Transportation pursuant to the provisions of 23 M.R.S.A. § 704 – A on or about:

March 20, 2013

The application is for:

Development of a 2,500 square foot full-service Dunkin' Donuts restaurant with a drive-through window. The proposed site is located on a 38,045 square foot parcel of property at 743 Sabattus Street, in the City of Lewiston. The proposed project is expected to generate the following peak hour trips:

Weekday AM Peak Hour of Street = 297 trips

At the following location: **743 Sabattus Street, Lewiston, Maine**

A request for a public hearing must be received by the Department, in writing, no later than 20 days after the application is found by the Department to be complete and is accepted for processing. Public comment on the application will be accepted throughout the processing of the application.

The application will be filed for public inspection at the City of Lewiston's Planning office, during normal working hours.

Written public comments may be sent to the Department of Transportation, Traffic Engineering Division, 16 State House Station, Augusta, Maine 04333.

1/2000

CERTIFICATION

The traffic engineer responsible for preparing this application and/or attaching pertinent site and traffic information hereto, by signing below, certifies that the application for traffic approval is complete and accurate to the best of his/her knowledge.

Signature: William J. Bray

Re/Cert/Lic No.: _____

Name (print): William Bray

Date: 3-15-2013

If the signature below is not the applicant's signature, attach letter of agent authorization signed by applicant.

"I certify under penalty of law that I have personally examined the information submitted in this document and all attachments thereto and that, based on my inquiry of those individuals immediately responsible for obtaining the information, I believe the information is true, accurate, and complete. I authorize the Department to enter the property that is the subject of this application, at reasonable hours, including buildings, structures or conveyances on the property, to determine the accuracy of any information provided herein. I am aware there are significant penalties for submitting false information, including the possibility of fine and imprisonment."

William J. Bray
Signature of applicant

3-15-2013
Date

SECTION 1

A. SITE PLAN

The proposed site, a 38,045 square foot parcel, is located at 743 Sabattus Street in the City of Lewiston. Access to the proposed site is provided with a full-service driveway connection to Wildwood Drive, a minor local street that intersects directly onto Sabattus Street.

SECTION 1

B. EXISTING AND PROPOSED SITE USES

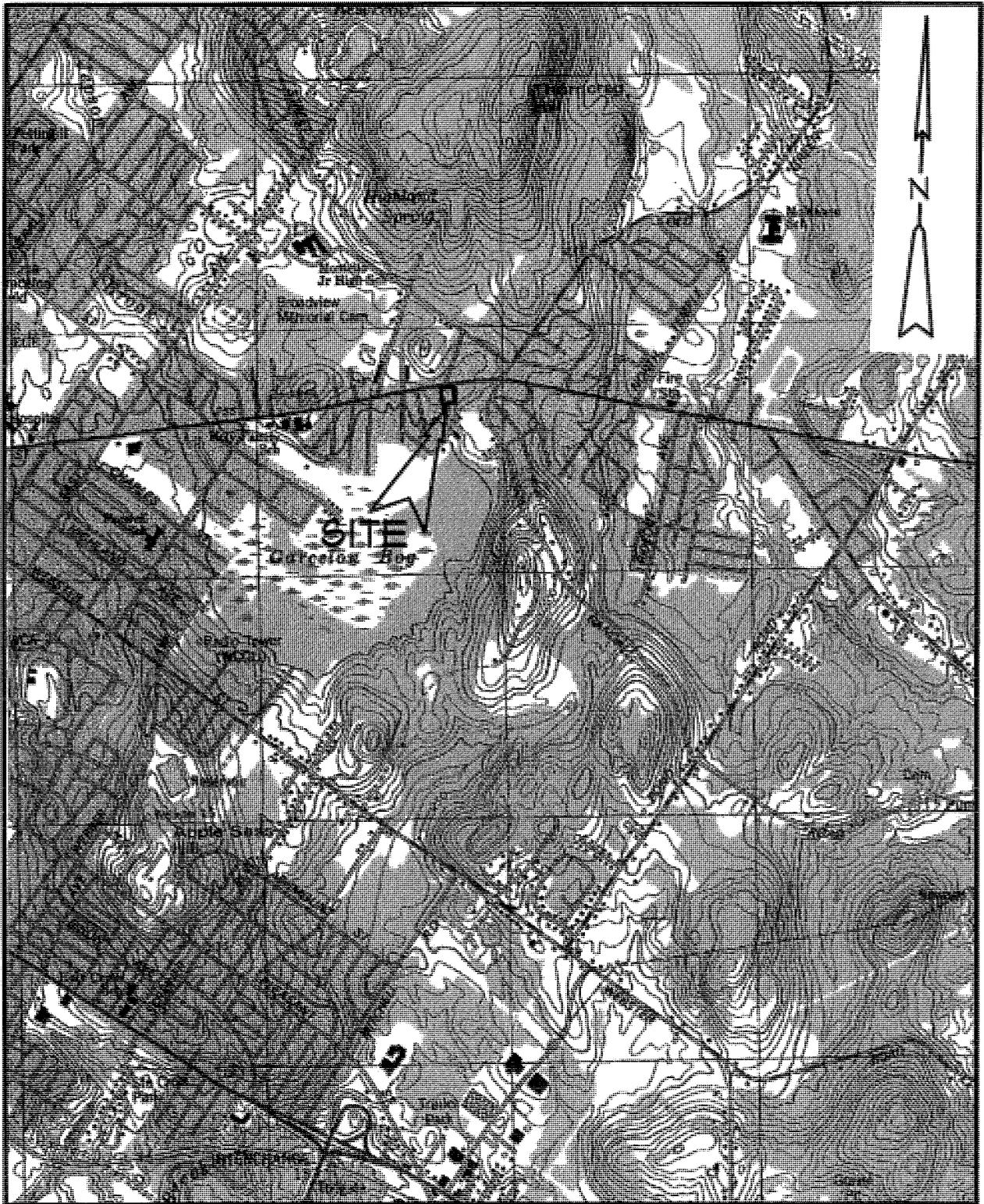
The existing site (38,045 square foot lot) is currently occupied by a single story vacant building.

The Applicant is proposing to construct a 2,500+/- square foot full-service Dunkin' Donuts restaurant with a drive-through window. The current site plan for the proposed project provides 24 parking spaces on site. Access to the proposed site is provided with a full-service driveway connection to Wildwood Drive onto Sabattus Street (Route 126).

SECTION 1

C. SITE AND VICINITY BOUNDARIES

The attached regional map depicts the proposed Dunkin' Donuts site.



USGS LOCATION MAP

741-743 SABATTUS STREET, LEWISTON

OWNER: KP REALTY, LLC

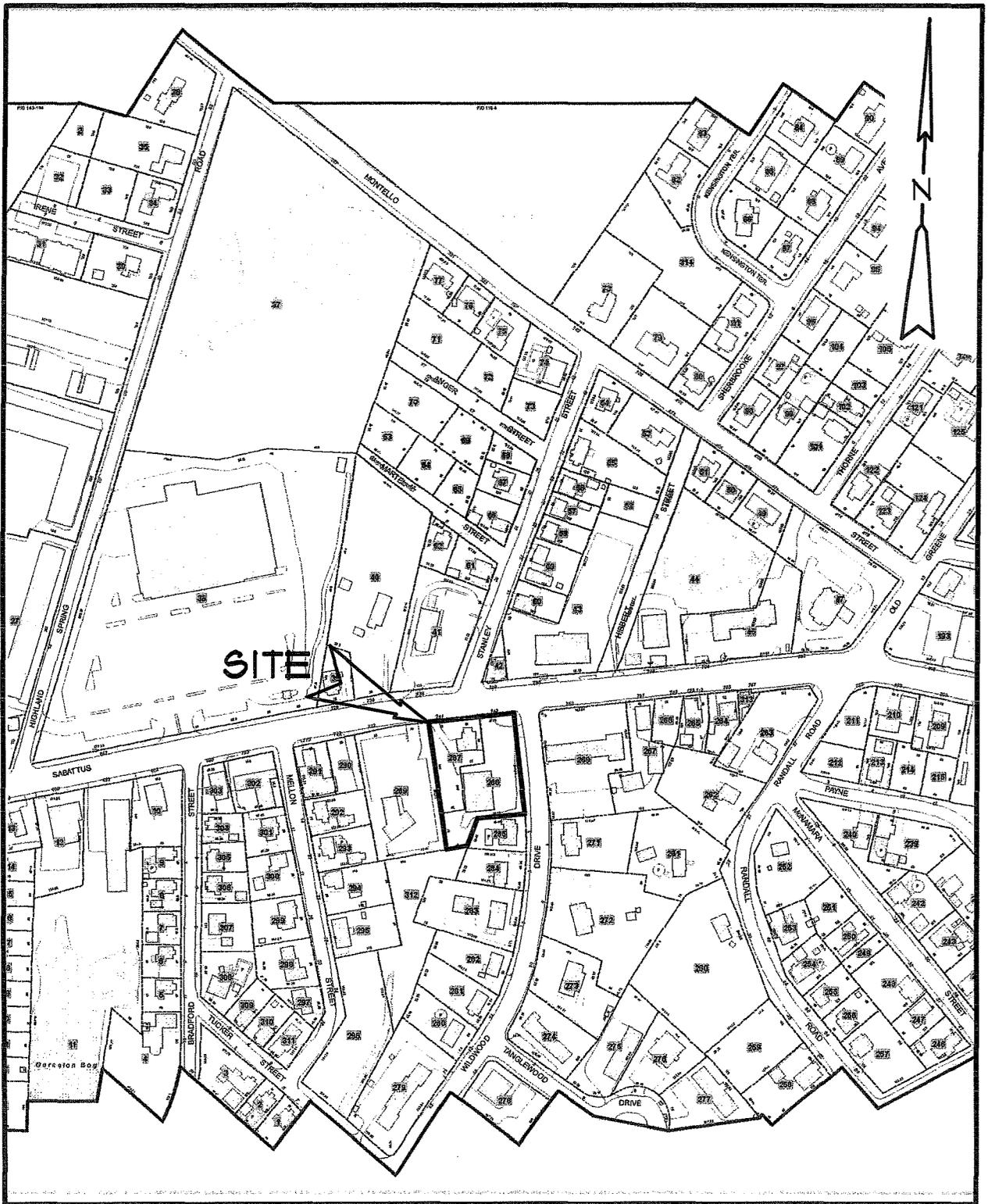
SCALE: 1" = 2000'

DATE OF GRAPHIC: OCTOBER 11, 2012

SOURCE: MAINE OFFICE OF GIS

ORIGINAL PUBLICATION DATE: 1967

Stoneybrook
Consultants, Inc.



TAX MAP

741-743 SABATTUS STREET, LEWISTON

OWNER: KP REALTY, LLC

SCALE: 1" = 300'±

DATE OF GRAPHIC: OCTOBER 11, 2012

SOURCE: CITY OF LEWISTON TAX MAP 153

PUBLICATION DATE: APRIL 1, 2011

Stoneybrook
Consultants, Inc.

SECTION 1

D. PROPOSED USES IN THE GENERAL VICINITY OF THE PROPOSED DEVELOPMENT

Performance of the required Traffic Impact Study will account for all projects whose trips potentially impact the roadway system adjacent to the proposed site.

SECTION 1

E. TRIP GENERATION

Trip generation for the proposed full-service Dunkin' Donuts restaurant was estimated based upon procedures developed in 2007 by Gorrill-Palmer, Inc. for the regional office of Dunkin' Donuts. MaineDOT conducted a thorough review of the report and have approved and adopted the following equation as the accepted procedure for calculating trip generation for a Dunkin' Donuts site:

$$Y = 0.0081(x) + 139.36$$

[Y = Number of trips generated during the AM peak hour; (x) represents the Average Annual Daily Traffic (AADT) passing by the site]

MaineDOT, in their annual traffic count book (see attached excerpt from report), have estimated the 2011 AADT for Route 126 (Sabattus Street) at 19,430 vehicles.

Accordingly, the proposed Dunkin' Donuts site can be expected to generate a total of 297 vehicle trips during the AM peak hour time period.

County

2011 Maine Transportatic Count Book

Androscoggin		STATION	ROAD	LOCATION	TYPE	GROUP	AADT07	AADT08	AADT09	AADT10	AADT11
TOWN											
01	LEWISTON	02802	0011X	SR 11/100/202 (MAIN)(NB) NE/O HAMMOND ST	T	I	.	16890	.	.	8630
01	LEWISTON	02806	0011X	SR 11/100/US 202 SW/O SR 126 NE/O CUT	T	I	.	.	.	.	18950
01	LEWISTON	02812	0011X	SR 11/100/202 (MAIN)(SB) NE/O HAMMOND ST	T	I	.	.	.	.	8020
01	LEWISTON	08806	0011X	SR 11/US 202(MAIN) SW/O RUSSELL ST RAMPS	C	I	19970	17460	.	.	17770
01	LEWISTON	09006	0011X	SR 11/100/US 202 (MAIN) SW/O MONTELLO ST	C	I	.	20640	.	.	.
01	LEWISTON	09205	0011X	SR 11/100/US 202 (MAIN) S/O NORTHWOOD RD	C	I	18340	19460	.	.	.
01	LEWISTON	09406	0011X	SR 11/100/US 202 (MAIN) SW/O STETSON RD	C	I	.	12950	.	.	.
01	LEWISTON	15801	0011X	SR 11/100/US 202(MAIN) N/O PETTINGILL ST	C	I	24640	23880	.	.	22710
01	LEWISTON	21505	0011X	SR 11/100/US 202 (MAIN) S/O LARRABEE RD	C	I	12090	11990	.	.	11390
01	LEWISTON	02804	0126X	SR 126(EB) SE/O SR11/100/US202(NW/O CUT)	T	I	.	.	.	.	1060
01	LEWISTON	02814	0126X	SR 126(WB) SE/O SR11/100/US202(NW/O CUT)	T	I	.	.	.	.	5730
01	LEWISTON	02903	0126X	SR 126 (SABATTUS ST) E/O OAK ST	C	I	11610	11640	.	.	.
01	LEWISTON	02907	0126X	SR 126 (SABATTUS ST) W/O OAK ST	C	I	.	12100	.	.	.
01	LEWISTON	05603	0126X	SR 126 (SABATTUS ST) E/O CENTRAL AVE	C	I	.	8130	.	.	.
01	LEWISTON	05703	0126X	SR 126 (SABATTUS ST) E/O COLLEGE ST	C	I	.	10510	.	.	.
01	LEWISTON	06103	0126X	SR 126 (SABATTUS ST) E/O EAST AVE	C	I	.	14040	.	.	.
01	LEWISTON	06107	0126X	SR 126 (SABATTUS ST) W/O EAST AVE	C	I	.	10660	.	.	10440
01	LEWISTON	06203	0126X	SR 126 (SABATTUS ST) E/O RUSSELL ST	T	I	.	21840	.	.	22110
01	LEWISTON	06207	0126X	SR 126 (SABATTUS ST) W/O RUSSELL ST	T	I	.	.	.	.	12980
01	LEWISTON	06503	0126X	SR 126 (SABATTUS ST) E/O OLD GREENE RD	T	I	.	.	.	.	17260
01	LEWISTON	06507	0126X	SR 126 (SABATTUS ST) W/O OLD GREEN RD	T	I	.	.	.	.	19430
01	LEWISTON	06803	0126X	SR 126 (SABATTUS ST) E/O GROVE ST	T	I	.	11300	.	.	10930
01	LEWISTON	06807	0126X	SR 126 (SABATTUS ST) W/O GROVE ST	T	I	.	.	.	.	15530
01	LEWISTON	07203	0126X	SR 126 (SABATTUS ST) E/O POND RD	T	I	.	.	.	.	11290
01	LEWISTON	07207	0126X	SR 126 (SABATTUS ST) W/O POND RD	T	I	.	.	.	.	10600
01	LEWISTON	07400	0126X	SR 126 (SABATTUS ST) @ SABATTUS TL	C	I	.	10120	.	.	9750
01	LEWISTON	21403	0126X	SR 126 (SABATTUS ST) E/O N TEMPLE ST	T	I	.	.	.	.	16130
01	LEWISTON	21407	0126X	SR 126 (SABATTUS ST) W/O N TEMPLE ST	T	I	.	.	.	.	16350
01	LEWISTON	25203	0126X	SR 126 (SABATTUS ST) E/O BRADFORD ST	T	I	.	.	.	.	20420
01	LEWISTON	25207	0126X	SR 126 (SABATTUS ST) W/O BRADFORD ST	T	I	.	.	.	.	19100
01	LEWISTON	00605	0196S	SR 196(CANAL)(OW) S/O SR11/100/202(MAIN)	C	I	.	.	.	.	4790
01	LEWISTON	01204	0196S	SR 196 (CANAL ST)(OW) SE/O CHESTNUT ST	C	I	.	.	.	.	5710
01	LEWISTON	01208	0196S	SR 196 (CANAL ST)(OW) NW/O CHESTNUT ST	C	I	.	.	.	.	6500
01	LEWISTON	01304	0196S	SR 196 (CANAL ST)(OW) SE/O CEDAR ST RMP	C	I	.	10550	.	.	10410

SECTION 1

F. TRIP COMPOSITION

The Gorrill-Palmer, Inc. 2007 study established that, on average, approximately 15% of the total trips generated by a Dunkin' Donuts site are "*primary*" or new trips to the area; 70% are "*pass-by*" trips or vehicle trips already on the roadway system and; 15% are "*diverted*" trips, existing trips on adjacent roadways that shift existing travel patterns.

Accordingly, the proposed Dunkin' Donuts site will generate: 208 "pass-by" trips, 45 "primary" trips and, 44 "diverted" trips.

SECTION 1

G. TRIP DISTRIBUTION

Vehicle trips generated during each of the noted peak time periods were assigned to/from the proposed site based upon a 50%/50% split, which generally conforms to patterns established in the 2007 Gorrill Palmer, Inc. study.

SECTION 1

H. TRIP ASSIGNMENT

Vehicle trip assignments were prepared for the AM peak hour based upon existing traffic patterns recently recorded for the section of Sabattus Street adjacent to the proposed site and the Consultant's knowledge of the area. It was assumed that approximately 75% of the trips would arrive from the east and depart to the west.

Figure 1 illustratively presents the AM peak hour traffic assignment for the proposed Dunkin' Donuts site.

VEHICLE VOLUME COUNT GRAPHIC SUMMARY SHEET

Intersection of ROUTE 126 & WILDWOOD DRIVE Date _____

Weather _____ Road Surface Condition _____ Time _____ to _____

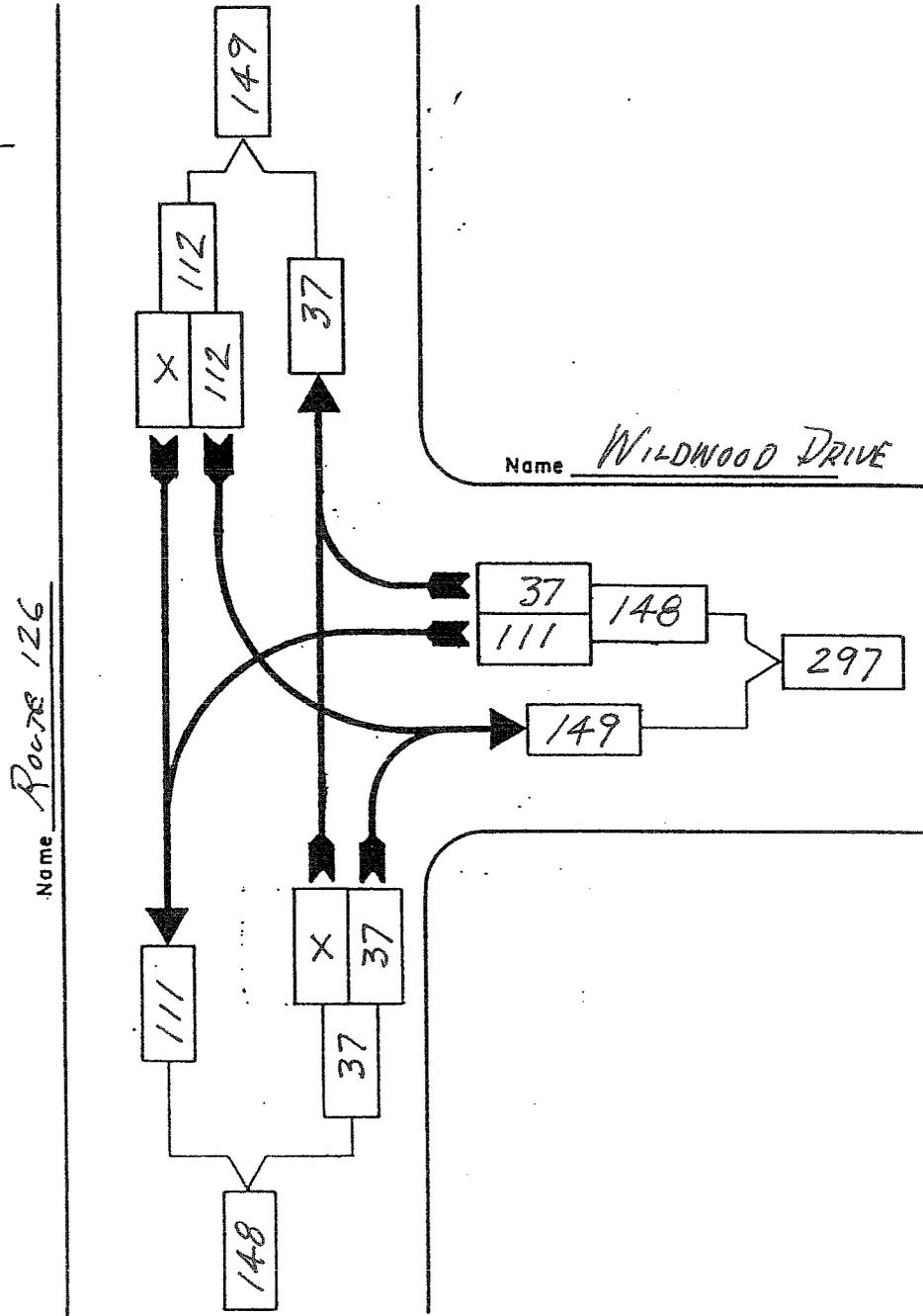
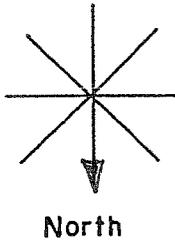


FIGURE 1: Site Traffic Assignment – AM Peak Hour

SECTION 2

TRAFFIC ACCIDENTS

The Maine Department of Transportation's (MaineDOT) Accident Records Section provided three-year (2009 through 2011) safety records for the 0.32 mile section of Route 126 (Sabattus Street) between the Old Greene Road and the Highland Spring Road intersections. MaineDOT's report is presented as follows:

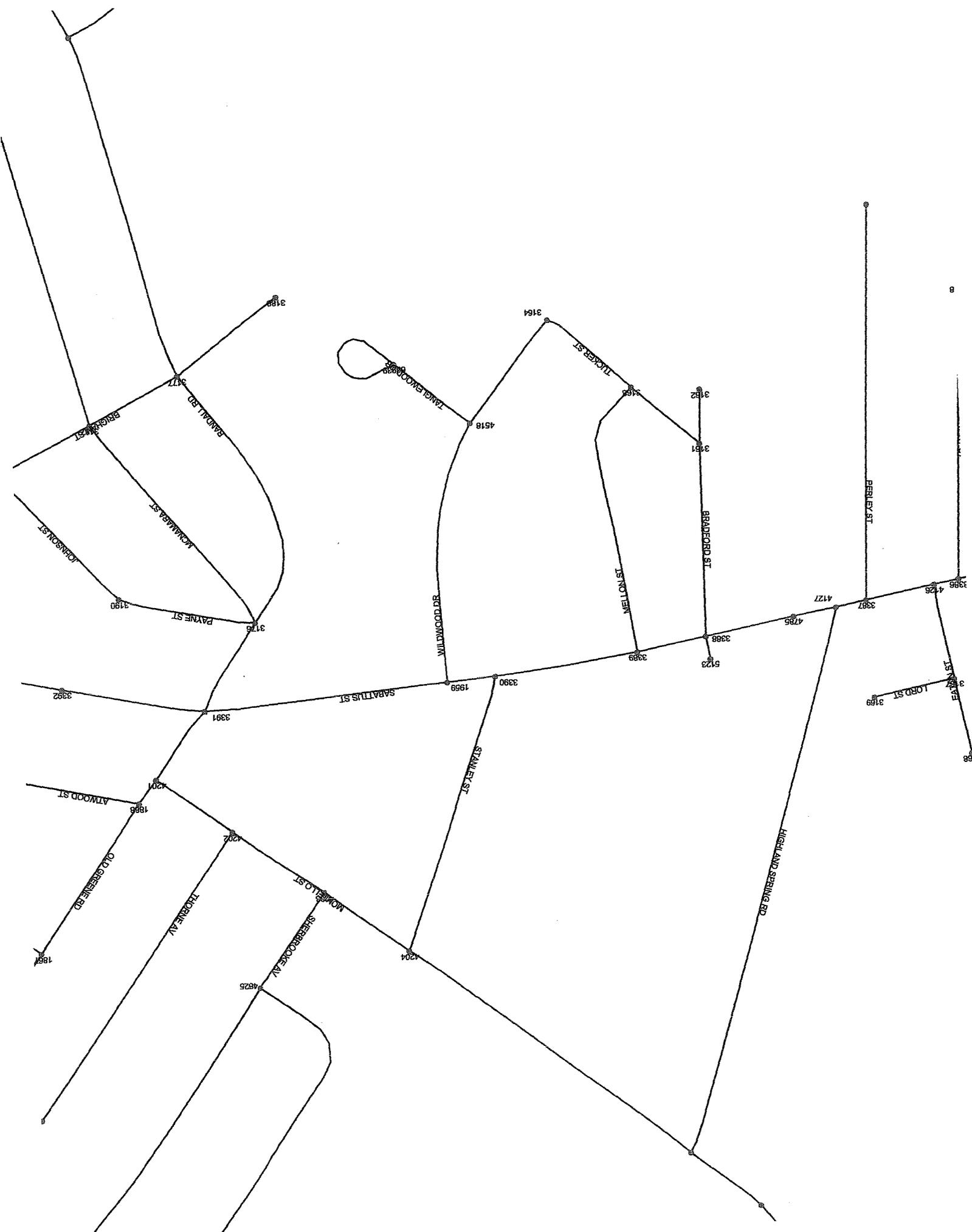
2009 - 2011 Accident Summary (Section of Route 126)

<u>Location</u>	<u>Number of Accidents</u>	<u>Critical Rate Factor</u>
1. Sabattus Street @ Bradford Street	19	0.78
2. Sabattus Street @ Mellon Street	1	0.13
3. Sabattus Street @ Stanley Street	4	0.62
4. Sabattus Street @ Wildwood Drive	3	0.48
5. Sabattus Street, Old Greene Road and Randall Road	30	1.16
6. Sabattus Street btw. Mid-block Node and Bradford Street	3	0.63
6. Sabattus Street btw. Bradford Street and Mellon Street	1	0.24
7. Sabattus Street btw. Mellon Street and Stanley Street	1	0.16
8. Sabattus Street btw. Wildwood Drive and Old Greene Road/Randall Road intersection	12	1.29

The MaineDOT considers any roadway segment or intersection a high crash location if both of the following criteria are met:

- *8 or more accidents*
- *A Critical Rate Factor greater than 1.00*

Two locations, which are highlighted in blue, have been identified by MaineDOT as high crash locations: 1) Sabattus Street, Old Greene Road & Randall Road intersection and, 2) Sabattus Street between Wildwood Drive and the Old Greene Road/Randall Road intersection. Detailed vehicle collision diagrams will be prepared with the performance of the required Traffic Impact Study and will include appropriate mitigation measures.



Crash Summary Report

Report Selections and Input Parameters

REPORT SELECTIONS

Crash Summary I Section Detail Crash Summary II 1320 Included 1320 & Driver Report Included

REPORT DESCRIPTION

Rte 126 from Highland Spring Rd to Old Green Rd in Lewiston

REPORT PARAMETERS

Year 2009, Start Month 1 through Year 2011 End Month: 12

Route: 0126X

Start Node: 4127

Start Offset: 0

Exclude First Node

End Node: 3391

End Offset: 0

Exclude Last Node

Maine Department Of Transportation - Traffic Engineering, Crash Records Section

Crash Summary I

Node	Route - MP	Node Description	U/R	Nodes							Percent Annual M Injury Ent-Veh	Crash Rate	Critical Rate	CRF
				Total Crashes	K	A	B	C	PD					
A4127	0126X - 1.76	Int of HIGHLAND SPRING RD SABATTUS ST	2	0	0	0	0	0	0	0.0	0.000	0.00	0.00	0.00
											Statewide Crash Rate:	0.12		
4785	0126X - 1.78	Non Int SABATTUS ST	2	0	0	0	0	0	0	0.0	3.486	0.00	0.35	0.00
											Statewide Crash Rate:	0.12		
3388	0126X - 1.83	Int of BRADFORD ST, ENT SHOP/SAVE, SABATTUS ST	9	19	0	0	2	4	13	31.6	7.913	0.80	1.02	0.00
											Statewide Crash Rate:	0.62		
3389	0126X - 1.87	Int of MELLON ST SABATTUS ST	2	1	0	0	1	0	0	100.0	7.462	0.04	0.29	0.00
											Statewide Crash Rate:	0.12		
3390	0126X - 1.94	Int of SABATTUS ST STANLEY ST	2	4	0	0	0	0	4	0.0	7.435	0.18	0.29	0.00
											Statewide Crash Rate:	0.12		
1959	0126X - 1.96	Int of SABATTUS ST, WILDWOOD DR	2	3	0	0	0	0	3	0.0	7.245	0.14	0.29	0.00
											Statewide Crash Rate:	0.12		
3391	0126X - 2.08	Int of OLD GREENE RD RANDALL RD SABATTUS ST	9	30	1	2	0	7	20	33.3	8.587	1.16	1.00	1.16
											Statewide Crash Rate:	0.62		
Study Years: 3.00			NODE TOTALS:	57	1	2	3	11	40	29.8	42.128	0.45	0.44	1.02

Maine Department Of Transportation - Traffic Engineering, Crash Records Section

Crash Summary I

Sections																	
Start Node	End Node	Element	Offset Begin - End	Route - MP	Section U/R Length	Total Crashes	K	Injury Crashes				Percent Injury	Annual HMVM	Crash Rate	Critical Rate	CRF	
								A	B	C	PD						
4127	4785	3104842	0 - 0.02	0126X - 1.76	0.02	2	0	0	0	0	0	0.0	0.00139	0.00	574.83	0.00	
Int of HIGHLAND SPRING RD SABATTUS ST				ST RTE 126													Statewide Crash Rate: 172.00
3388	4785	3117712	0 - 0.05	0126X - 1.78	0.05	2	3	0	0	0	1	2	33.3	0.00349	286.88	454.56	0.00
Int of BRADFORD ST, ENT SHOP/SAVE, SABATTUS ST				ST RTE 126													Statewide Crash Rate: 172.00
3388	3389	3103858	0 - 0.04	0126X - 1.83	0.04	2	1	0	0	0	0	1	0.0	0.00298	111.81	473.32	0.00
Int of BRADFORD ST, ENT SHOP/SAVE, SABATTUS ST				ST RTE 126													Statewide Crash Rate: 172.00
1959	3391	3104060	0 - 0.12	0126X - 1.84	0.12	2	12	0	1	2	1	8	33.3	0.00851	470.02	363.85	1.29
Int of SABATTUS ST, WILDWOOD DR				ST RTE 126													Statewide Crash Rate: 172.00
3389	3390	3103860	0 - 0.07	0126X - 1.87	0.07	2	1	0	0	0	1	0	100.0	0.00520	64.05	410.35	0.00
Int of MELLON ST SABATTUS ST				ST RTE 126													Statewide Crash Rate: 172.00
1959	3390	3130171	0 - 0.02	0126X - 1.94	0.02	2	0	0	0	0	0	0	0.0	0.00145	0.00	569.24	0.00
Int of SABATTUS ST, WILDWOOD DR				ST RTE 126													Statewide Crash Rate: 172.00
Study Years: 3.00					Section Totals:	0.32	17	0	1	2	3	11	35.3	0.02303	246.09	293.30	0.84
					Grand Totals:	0.32	74	1	3	5	14	51	31.1	0.02303	1071.22	418.12	2.56

SECTION 3

A. ENTRANCE/EXIT LOCATION

Access to the proposed site is provided with a full-service driveway connection to Wildwood Drive and onto Sabattus Street (Route 126).

SECTION 3

B. ENTRANCE AND EXIT PLAN VIEW

Sightline measurements of 200 feet (Sabattus Street) left and 250+/- right were observed from the proposed driveway intersection on Wildwood Drive. The posted speed limit on Wildwood Drive is assumed to be 25mph in the vicinity of the proposed project. MaineDOT's standard for a posted speed limit of 25mph is 200 feet.

SECTION 3

C. ENTRANCE/EXIT DESIGN

The proposed project, including all required off-site improvements, will be designed and constructed in accordance with appropriate MaineDOT and City of Lewiston standards.

Section 7

Permit By Rule Application

**DEPARTMENT OF ENVIRONMENTAL PROTECTION
PERMIT BY RULE NOTIFICATION FORM**
(For use with DEP Regulation, Chapter 305)

PLEASE TYPE OR PRINT IN **BLACK INK ONLY**

Name of Applicant: (owner)	KP Realty, LLC	Name of Agent:	Mike Gotto Stoneybrook Consultants, Inc.		
Applicant Mailing Address:	1385 Maine Street #1	Agent Phone # (include area code):	(207) 514-7491		
Town/City:	Poland, ME	PROJECT Information Name of Town/City:	Lewiston		
State and Zip code:	04024	Name of Wetland or Waterbody:	Unnamed Stream		
Daytime Phone # (include area code):	(207) 576-5359	Map #:	119	Lot #:	286 & 287
Detailed Directions to Site:	From Exit 80 in Lewiston, take Alfred A. Plourde Parkway north to the end. Turn left onto Webster Street. After about 1 mile turn right onto Farwell Street. Turn right onto Sabattus Street. The site is 741-743 on the right at the intersection of Wildwood Dr.				
Description of Project:	Grading approximately 25' from an unnamed stream and concrete/pavement placement approximately 55' from an unnamed stream as improvements associated with construction of a Dunkin' Donuts.				
Part of a larger project? (check one) →	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	After the Fact? (check one) →	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	Check one → This project <input type="checkbox"/> does (or) <input checked="" type="checkbox"/> does not involve work below mean low water (average low water).	

PERMIT BY RULE (PBR) SECTIONS: (Check at least one)

I am filing notice of my intent to carry out work which meets the requirements for Permit By Rule (PBR) under DEP Rules, Chapter 305. I and my agents, if any, **have read** and will comply with all of the standards in the Sections checked below.

- | | | |
|--|---|---|
| <input checked="" type="checkbox"/> Sec. (2) Act. Adj. to Protected Natural Res. | <input type="checkbox"/> Sec.(10) Stream Crossing | <input type="checkbox"/> Sec. (17) Transfers/Permit Extension |
| <input type="checkbox"/> Sec. (3) Intake Pipes | <input type="checkbox"/> Sec. (11) State Transportation Facil. | <input type="checkbox"/> Sec. (18) Maintenance Dredging |
| <input type="checkbox"/> Sec. (4) Replacement of Structures | <input type="checkbox"/> Sec. (12) Restoration of Natural Areas | <input type="checkbox"/> Sec. (19) Activities in/on/over significant vernal pool habitat |
| <input type="checkbox"/> Sec. (5) REPEALED | <input type="checkbox"/> Sec. (13) F&W Creation/Enhance/Water Quality Improvement | <input type="checkbox"/> Sec. (20) Activities in existing dev. areas located in/on/over high or moderate value inland waterfowl & wading bird habitat or shorebird nesting, feeding & staging areas |
| <input type="checkbox"/> Sec. (6) Movement of Rocks or Vegetation | <input type="checkbox"/> Sec. (14) REPEALED | |
| <input type="checkbox"/> Sec. (7) Outfall Pipes | <input type="checkbox"/> Sec. (15) Public Boat Ramps | |
| <input type="checkbox"/> Sec. (8) Shoreline stabilization | <input type="checkbox"/> Sec. (16) Coastal Sand Dune Projects | |
| <input type="checkbox"/> Sec. (9) Utility Crossing | | |

I have attached the following required submittals. **NOTIFICATION FORMS CANNOT BE ACCEPTED WITHOUT THE NECESSARY ATTACHMENTS:**

- Attach** a check for \$70 made payable to: "Treasurer, State of Maine".
- Attach** a U.S.G.S. topo map or Maine Atlas & Gazetteer map with the project site clearly marked.
- Attach Proof of Legal Name.** If applicant is **not** an individual or municipality, provide a copy of Secretary of State's registration information (available at <http://icrs.informe.org/nei-sos-icrs/ICRS?MainPage=x>)
- Attach photos of the proposed site where activity will take place as outlined in PBR Sections checked above.**
- Attach all other required submissions as outlined in the PBR Sections checked above.**

I authorize staff of the Departments of Environmental Protection, Inland Fisheries & Wildlife, and Marine Resources to access the project site for the purpose of determining compliance with the rules. I also understand that **this permit is not valid until approved by the Department or 14 days after receipt by the Department, whichever is less.**

By signing this Notification Form, I represent that the project meets all applicability requirements and standards in the rule and that the applicant has sufficient title, right, or interest in the property where the activity takes place.

Signature of Agent or Applicant:		Date:	5/3/13
----------------------------------	--	-------	--------

Keep a copy as a record of permit. Send the form with attachments via certified mail or hand deliver to the Maine Dept. of Environmental Protection at the appropriate regional office listed below. The DEP will send a copy to the Town Office as evidence of the DEP's receipt of notification. No further authorization by DEP will be issued after receipt of notice. Permits are valid for two years. **Work carried out in violation of any standard is subject to enforcement action.**

AUGUSTA DEP
17 STATE HOUSE STATION
AUGUSTA, ME 04333-0017
(207)287-3901

PORTLAND DEP
312 CANCO ROAD
PORTLAND, ME 04103
(207)822-6300

BANGOR DEP
106 HOGAN ROAD
BANGOR, ME 04401
(207)941-4570

PRESQUE ISLE DEP
1235 CENTRAL DRIVE
PRESQUE ISLE, ME 04769
(207)764-0477

OFFICE USE ONLY	Ck.#	Date	Staff	Staff	After Photos
PBR #	FP	Date	Acc. Date	Def. Date	

Activity Description

K.P. Realty, LLC

741 - 743 Sabattus Street, Lewiston

A permit-by-rule application for this property is being filed for construction activity that will take place within 75' of an unnamed stream. An existing State or municipal storm drain line from Sabattus Street crosses this property and outlets near the rear property line. The outlet area has previously been determined to be a stream by MDEP personnel. The stream does not have a Shoreland Zoned area or Stream Protection zone under the City zoning requirements, but is subject to the Natural Resource Protection Act setbacks.

This property has frontage on State Route 126, also known as Sabattus Street, and is located at the intersection of Wildwood Drive. Sabattus Street in this area is developed commercially and this site is fully developed. Current site improvements have a maintained lawn to within 10' of the stream. A prior building (removed in 2010 or 2011) was located about 70' from the stream and gravel driveway improvements for that building still exist about 45' from the stream.

The applicant is seeking local approvals to redevelop the property with a drive-thru Dunkin Donuts Shop. To support this use, the rear portion of the site will be raised about 5' in elevation. Parking improvements will be set back about 55' from the stream, but grading will extend to within 25' of the stream. The existing lawn and trees that fall within 25' of the stream will remain undisturbed.

We have attached reduced copies of the current site plans and pictures of existing conditions. We expect to secure approvals from the City Planning Board in May and plan to begin construction as soon as building permits can be obtained. Occupancy is expected by Fall.

Activities under this submittal are for disturbance adjacent to an unnamed stream for construction of the proposed improvements.



1. Existing house looking from rear of site toward Sabattus Street.



2. Existing paved area looking from Sabattus Street toward rear of site.

SITE PHOTOS

741-743 SABATTUS STREET

LEWISTON, MAINE

KP REALTY, LLC

PICTURES TAKEN SEPTEMBER 13, 2012

& NOVEMBER 7, 2012 BY MFG

Stoneybrook
Consultants, Inc.



3. Wooded area at rear of site surrounding unnamed stream.

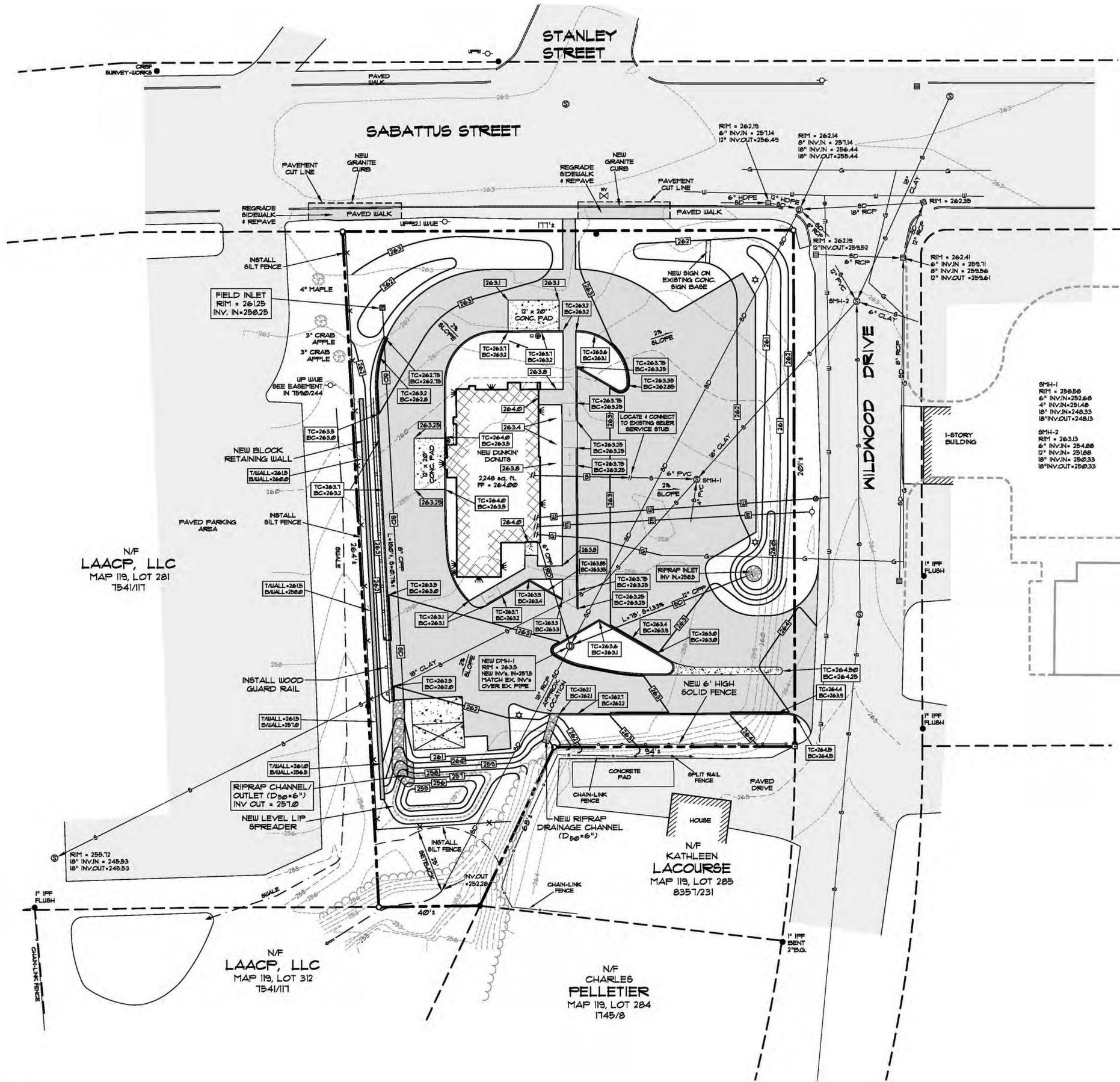


4. Stream channel at rear of site.

SITE PHOTOS

741-743 SABATTUS STREET
LEWISTON, MAINE
KP REALTY, LLC
PICTURES TAKEN SEPTEMBER 13, 2012
& NOVEMBER 7, 2012 BY MFG

Stoneybrook
Consultants, Inc.

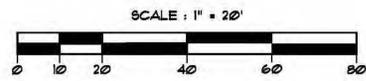
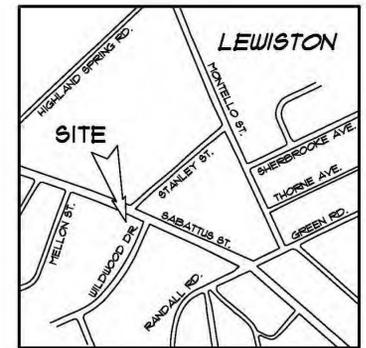


NOTES

- EXISTING CONDITIONS INFORMATION WAS TAKEN FROM A PLAN ENTITLED "EXISTING CONDITIONS PLAN - 741-743 SABATTUS STREET" PREPARED BY JONES ASSOCIATES, INC. DATED OCTOBER 1, 2012.
- ALL BOOK AND PAGE NUMBERS REFER TO THE ANDROSCOGGIN COUNTY REGISTRY OF DEEDS.
- OWNER OF RECORD - KP REALTY LLC
741 SABATTUS STREET - TAX MAP 119, LOT 281
743 SABATTUS STREET - TAX MAP 119, LOT 286
DEED REFERENCE - BOOK 8510, PAGE 25
DEED REFERENCE - BOOK 8510, PAGE 28
- THE PARCEL IS LOCATED IN THE HIGHWAY BUSINESS ZONING DISTRICT.
- THIS PLAN WAS PREPARED TO SHOW A FULL SERVICE DUNKIN' DONUTS WITH SECOND FLOOR OFFICE SPACE TO FACILITATE DISCUSSIONS OF THIS SITE'S DEVELOPMENT POTENTIAL.
- THE LOCATION, DEPTH AND SIZE OF ALL UNDERGROUND UTILITY LINES, TANKS AND/OR STRUCTURES WAS NOT VERIFIED. CONTRACTOR SHALL CONTACT DIGSAFE/ON-TARGET PRIOR TO EXCAVATION TO HAVE ALL PUBLIC AND PRIVATE UTILITY LOCATIONS IN THE PROJECT AREA DETERMINED.
- CONNECT ROOF DRAIN AND FOUNDATION DRAIN LINES TO NEW DMH-1. LAYOUT, LOCATION, TYPE AND SIZE TO BE DETERMINED BY ARCHITECT, OWNER AND CONTRACTOR PRIOR TO INSTALLATION.
- NEW UTILITY CONNECTIONS (WATER, SEWER, GAS) SHALL USE EXISTING SERVICE LINES WITHIN THE PROJECT AREA WHERE POSSIBLE. SAID CONNECTIONS SHALL COMPLY WITH THE STANDARDS OF EACH RESPECTIVE UTILITY COMPANY.

LEGEND

---	BOUNDARY LINE (SUBJECT PARCEL)
---	BOUNDARY LINE (OTHER)
●	IRON ROD OR PIPE FOUND
○	NOW OR FORMERLY
2356/893	BOOK AND PAGE NUMBER
○	UTILITY POLE
○	EXISTING SEWER MANHOLE
■	EXISTING CATCH BASIN
■	NEW FIELD INLET (CATCH BASIN)
○	EXISTING STORM DRAIN MANHOLE
○	NEW STORM DRAIN MANHOLE
○	NEW BOLLARD
---	EXISTING CONTOUR
263	NEW CONTOUR
SD	EXISTING STORM DRAIN
SD	NEW STORM DRAIN
RD	NEW ROOF DRAIN
S	EXISTING SEWER LINE
S	NEW SEWER LINE
G	EXISTING GAS LINE
G	NEW GAS LINE
W	EXISTING WATER LINE
W	NEW WATER LINE
E	NEW UNDERGROUND ELECTRIC LINE
263.4	NEW SPOT GRADE
TC	TOP OF CURB
BC	BOTTOM OF CURB
TWALL	TOP OF WALL
BWALL	BOTTOM OF WALL
[Hatched Box]	EXISTING BUILDING
[Cross-hatched Box]	NEW BUILDING
[Dotted Box]	EXISTING PAVEMENT
[Solid Box]	NEW PAVEMENT



REVISED: 5-3-13 - ADD ROOF DRAIN & STORM DRAIN LINES
REVISED: 4-18-13 - ADD LIGHT POLES, ADJUST PAVEMENT
REVISED: 4-16-13 - RECONFIGURE CURBS & SPOT GRADES
REVISED: 4-12-13 - RECONFIGURE CURBS & SPOT GRADES
SHEET TITLE:

GRADING & UTILITY PLAN

DUNKIN' DONUTS
741-743 SABATTUS STREET - LEWISTON, ME

Stoneybrook Consultants, Inc.
456 Buckfield Road - Turner, Maine 04282
(207) 514-7491 Voice / (207) 514-7492 Fax
www.stoneybrook.com

640 MAIN ST.
LEWISTON, ME 04240
Tel: (207) 516-3313
Fax: (207) 755-6128
www.summitgeo.com



JOB NO. - 13043
SHEET **2**

DRAWN BY: KRF
CHECKED BY: WMP
SCALE: 1" = 20'
DATE: MARCH 2013

KP REALTY, LLC
1385 MAINE STREET # - FOLAND, MAINE 04714

Summit
GEOENGINEERING SERVICES

Section 8

Financial Capacity



Bank

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10 Great Falls Plaza
Auburn, ME 04210
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April 30, 2013

Mr. Kevin Pacheco
c/o K.P. Donuts, Inc.
1385 Maine St. #1
Poland, ME. 04274

Dear Kevin:

This letter shall serve to confirm that we have discussed your planned development of a new Dunkin Donuts restaurant facility to be located on Sabattus Street in Lewiston, and that we have provided you with a proposal for financing for this project. I would also like to confirm that we consider you to be a valued client in good standing and that I believe you have demonstrated the capacity to obtain the necessary financing.

Please do not hesitate to call if you would like me to provide further assurances to the City of Lewiston regarding your ability to obtain financing. I'd be happy to do so.

Sincerely,

Christopher Rugullies
Vice President

Section 9

Stormwater

Pre and Post Watershed Comparisons
Dunkin' Donuts - 741-743 Sabattus Street, Lewiston

Watershed #1

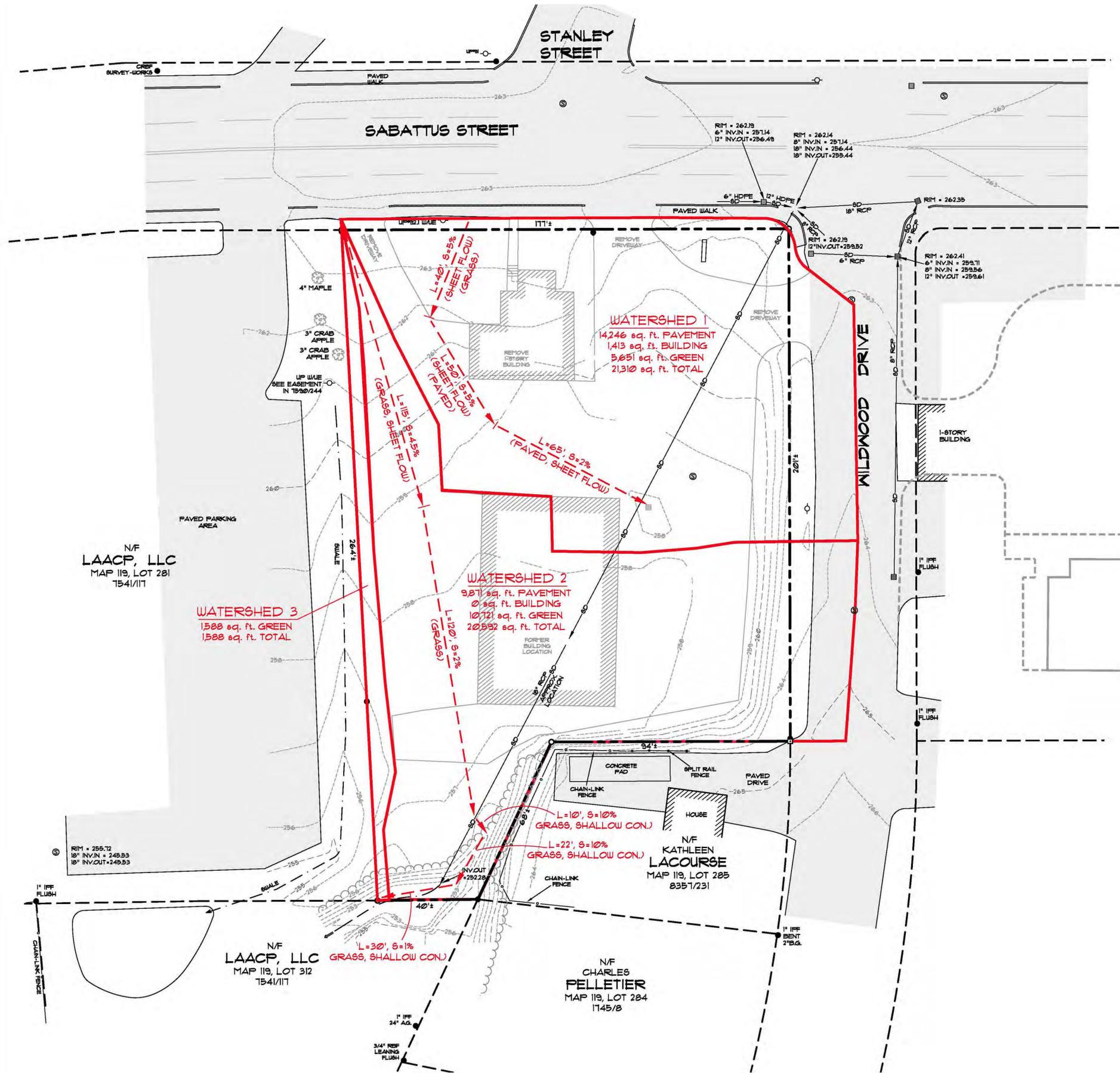
	Building/Pavement	Green	Total
Pre	15,659 s.f.	5,651 s.f.	21,310 s.f.
Post (1)	13,314 s.f.	7,585 s.f.	20,899 s.f.
Post (1A)	2,248 s.f.	0 s.f.	2,248 s.f.
Post (Total)	15,562 s.f.	7,585 s.f.	23,147 s.f.
Change	-97 s.f.	1,934 s.f.	1,837 s.f.

Watershed #2

	Building/Pavement	Green	Total
Pre	9,871 s.f.	10,721 s.f.	20,592 s.f.
Post	9,794 s.f.	9,037 s.f.	18,831 s.f.
Change	-77 s.f.	-1,684 s.f.	-1,761 s.f.

Watershed #3

	Building/Pavement	Green	Total
Pre	0 s.f.	1,588 s.f.	1,588 s.f.
Post	0 s.f.	1,490 s.f.	1,490 s.f.
Change	0 s.f.	-98 s.f.	-98 s.f.



NOTES

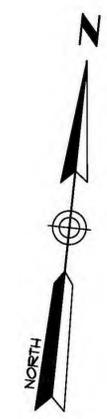
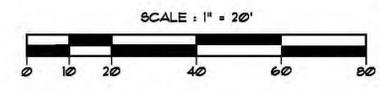
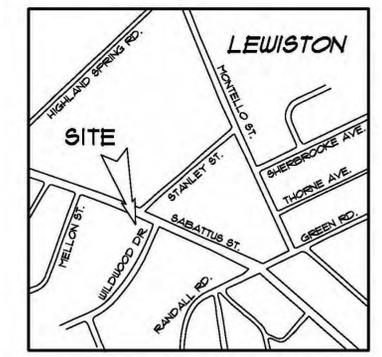
- EXISTING CONDITIONS INFORMATION WAS TAKEN FROM A PLAN ENTITLED "EXISTING CONDITIONS PLAN - 141-143 SABATTUS STREET" PREPARED BY JONES ASSOCIATES, INC. DATED OCTOBER 1, 2012.
- ALL BOOK AND PAGE NUMBERS REFER TO THE ANDROSCOGGIN COUNTY REGISTRY OF DEEDS.
- OWNER OF RECORD - KP REALTY LLC
 141 SABATTUS STREET - TAX MAP 119, LOT 281
 DEED REFERENCE - BOOK 8510, PAGE 25
 143 SABATTUS STREET - TAX MAP 119, LOT 286
 DEED REFERENCE - BOOK 8510, PAGE 28
- THE PARCEL IS LOCATED IN THE HIGHWAY BUSINESS ZONING DISTRICT.

LEGEND

	BOUNDARY LINE (SUBJECT PARCEL)
	BOUNDARY LINE (OTHER)
	IRON ROD OR PIPE FOUND
	NOT OR FORMERLY
	BOOK AND PAGE NUMBER
	EXISTING SEWER MANHOLE
	EXISTING CATCH BASIN
	EXISTING STORM DRAIN MANHOLE
	NEW POLE MOUNTED EXTERIOR LIGHT
	NEW WALL MOUNTED EXTERIOR LIGHT
	EXISTING BUILDING
	NEW BUILDING
	EXISTING PAVEMENT
	NEW PAVEMENT

SITE TABULATIONS

EXISTING IMPERVIOUS AREA = 22,575 sq. ft.
 PROPOSED IMPERVIOUS AREA = 22,572 sq. ft.
 DRIVE-THRU LANE STACKING = 16 VEHICLES
 PROPOSED PARKING = 25 SPACES



<p>PROJECT:</p> <p>DUNKIN' DONUTS</p> <p>141-143 SABATTUS STREET - LEWISTON, ME</p> <p>CLIENT:</p> <p>KP REALTY, LLC</p> <p>1385 MAINE STREET # - POLAND, MAINE 04274</p>	<p>SHEET TITLE:</p> <p>PRE-DEVELOPMENT WATERSHED PLAN</p>	<p>SCALE: 1" = 20'</p> <p>DATE: MAY 3, 2013</p>	<p>DRAWN BY: KRF</p> <p>CHECKED BY: WMP</p>
<p>Stoneybrook Consultants, Inc.</p> <p>456 Duckfield Road - Turner, Maine 04282</p> <p>(207) 514-7491 Voice / (207) 514-7492 Fax</p> <p>Tel: (207) 516-3313 Fax: (207) 195-6128 www.summitgeog.com</p>			
<p>SUMMIT</p> <p>ENGINEERING SERVICES</p>			
<p>JOB NO. - 13043</p> <p>SHEET 2-1</p>			



CITY OF LEWISTON



Department of Planning & Code Enforcement

TO: Planning Board
FROM: David Hediger, City Planner
DATE: May 10 2013
RE: May 13, 2013 Planning Board Agenda Item IV(b)

Request by the Lewiston School Department for a capital expenditure not included in Lewiston's annual capital program to make baseline safety improvements for the school entrance of Montello Elementary School.

As the Board will recall, a favorable recommendation was provided to the City Council in February to adopt to FY2014 Lewiston Capital Improvement Plan (LCIP). This recommendation is required by Charter and the Zoning and Land Use Code.

The Lewiston School Department is now making a request for a capital expenditure that was not included in the adopted LCIP. Article VII, Section 4(f) of the Zoning and Land Use Code requires that the Board review and make a recommendation to the City Council with regard to all capital expenditures costing \$100,000 or more which are not included in the LCIP. This is the case with respect to the School Department's request to make baseline safety improvements for the school entrance of Montello Elementary School. Reference should be made the School Department's letter dated May 6, 2013.

At this time, the Board is being asked to provide a favorable recommendation to the City Council for the Lewiston School Department's capital expenditure not included in Lewiston's annual capital program to make baseline safety improvements for the school entrance of Montello Elementary School.

ACTIONS NECESSARY:

1. Make a motion to consider a request by the Lewiston School Department for a capital expenditure not included in Lewiston's annual capital program.
2. Obtain input on the proposed amendment.
3. Make a motion pursuant to Article VII, Section 4(f) of the Zoning and Land Use Code to send a favorable recommendation for the City Council's consideration the Lewiston School Department's capital expenditure not included in Lewiston's annual capital program to make baseline safety improvements for the school entrance of Montello Elementary School.

LEWISTON PUBLIC SCHOOLS



36 Oak Street
Lewiston, ME 04240
www.lewistonpublicschools.org
TDD: 207-795-4100
FAX: 207-795-4177

Administration: 207-795-4100
Business: 207-795-4104
Curriculum: 207-795-4103
ELL Program: 207-795-4105
Maintenance: 207-795-4107
Nutrition: 207-795-4106
Payroll: 207-795-4109
Special Education: 207-795-4108
Technology: 207-753-6416

May 6, 2013

David Hediger
City Planner/Deputy Director Planning and Code Enforcement
City of Lewiston
27 Pine Street
Lewiston, ME 04240-7201

Dear David:

The School Department is seeking approval to fund a capital project that will establish baseline safety features for the entrance at Montello Elementary. We understand a Planning Board recommendation for the City Council's consideration is required for capital improvements of \$100,000 or more not included in the LCIP.

A general project overview follows:

Today, entering the school building requires passing through an open, dimly lit space and walking through a second door to ascend the stairway before seeing the school secretary. This entrance prevents any ability to monitor or contain visitors in the lobby, away from most students. The project will renovate the main entrance of Montello Elementary, creating an entrance similar to those at Geiger and Farwell schools. The design will raise the existing roof of the entrance area, enabling installation of a ramp that meets current codes for ADA and a new set of stairs from the Main Office level to the lower entrance level. This design relocates the school secretary and assistant to the main level, where they will be able to greet and monitor the safe movement of students, staff, parents and visitors.

Along with the above changes, this renovation will allow us to widen an existing ramp from approximately 4' wide to roughly 9'. Currently the ramp creates a pedestrian bottleneck in the school, undermining safety in a setting with over 700 children passing through at least 3 times each school day. There will be no change in the building footprint or square footage.

We have obtained a construction estimate of \$275,000 for the project. In order to meet this budget without added soft costs, we will manage it as a design-build project using both internal and external resources. We propose the following funding sources for approval by City Council:

David Hediger
City Planner/Deputy Director Planning and Code Enforcement
May 6, 2013
Page Two

Transfer from New School Bond:	\$20,000	(710-7107101)
Transfer unused Longley bond funds:	\$22,842	(710-7107301)
Transfer unused contingency McMahon*:	\$126,667	(713-7137201*)
Re-purpose Montello Locker bond funds:	<u>\$105,491</u>	(713-7137301)
TOTAL	<u>\$275,000</u>	

* This contingency amount includes residual McMahon construction funds from the previous phase, originating in Bond 722-7227201.

The School Committee approved this request at its meeting held April 22, 2013.
We thank you and the Planning Board for consideration of this request.

Sincerely,



William Webster, Jr.
Superintendent of Schools

c: Elaine Runyon, Controller
Joe Perryman, Facilities Director